



**muswellbrook  
shire council**

**Muswellbrook Shire Council**  
**ORDINARY COUNCIL MEETING**

**BUSINESS PAPER**  
**28 JULY 2020**



**ORDINARY COUNCIL MEETING, 28 JULY 2020**

MUSWELLBROOK SHIRE COUNCIL

P.O Box 122  
MUSWELLBROOK  
24 July, 2020

Councillors,

You are hereby requested to attend the Ordinary Council Meeting to be held in the CHAMBERS, Administration Centre, Muswellbrook on **28 July, 2020** commencing at 6.00pm.

Fiona Plesman  
**GENERAL MANAGER**

# Order of Business

ITEM	SUBJECT	PAGE NO
1	ACKNOWLEDGEMENT OF COUNTRY	6
2	CIVIC PRAYER	6
3	APOLOGIES AND LEAVE OF ABSENCE	6
4	CONFIRMATION OF MINUTES OF PREVIOUS MEETING	6
5	DISCLOSURE OF ANY PECUNIARY AND NON-PECUNIARY INTEREST	6
6	PUBLIC PARTICIPATION	7
7	MAYORAL MINUTES	7
8	GENERAL BUSINESS	7
9	BUSINESS ARISING	7
	Nil	
10	ENVIRONMENTAL SERVICES	8
10.1	PP016 - REZONING - IRONBARK RIDGE	8
10.2	UPDATE ON NEW SHIRE WIDE LEP AND DCP	114
10.3	NATIVE BIRD PUBLIC ART DENMAN	125
11	COMMUNITY INFRASTRUCTURE	130
11.1	EDDERTON ROAD STAGE 1 PRACTICAL COMPLETION	130
11.2	PROPOSED COUNCIL PUBLIC ROAD CLOSURES - BENGALLA MINE CONTINUED OPERATIONS PROJECT	139
11.3	CROWN ROADS PROPOSED TO BE CLOSED AT WYBONG	143
11.4	MT PLEASANT MODIFICATION 4 - RAIL LOOP S138 ROAD ACT 1993 PERMIT AMENDMENTS	149
11.5	CONCEPT DESIGN WILLIAMS BRIDGE MARTINDALE RD	156
11.6	2020-21 CAPITAL WORKS PROGRAM ROADS AND DRAINAGE	181
11.7	LIQUID TRADE WASTE REVIEW FOR 2019-2020	235
11.8	REGIONAL TENDER MATTRESS RECYCLING	248
12	CORPORATE AND COMMUNITY SERVICES	253
12.1	2019/20 ANNUAL FINANCIAL STATEMENTS	253
12.2	AUTHORISATION PURSUANT TO SECTION 226 OF THE LOCAL GOVERNMENT ACT 1993	255
12.3	POLICY REVIEW	256
12.4	AMENDMENT TO COUNCIL'S GOVERNANCE POLICY AND CODE OF CONDUCT	271

12.5	REPORT ON INVESTMENTS HELD AS AT 30 JUNE 2020	275
12.6	PLANNING AND ENVIRONMENT SERVICES	289
12.7	COMMUNITY SERVICES	298
<b>13</b>	<b>REPORTS FROM COMMITTEES</b>	<b>300</b>
	Nil	
<b>14</b>	<b>NOTICES OF MOTION / RESCISSION</b>	<b>300</b>
	Nil	
<b>15</b>	<b>QUESTIONS WITH NOTICE</b>	<b>300</b>
	Nil	
<b>16</b>	<b>COUNCILLORS REPORTS</b>	<b>300</b>
<b>17</b>	<b>QUESTIONS FOR NEXT MEETING</b>	<b>300</b>
<b>18</b>	<b>ADJOURNMENT INTO CLOSED COUNCIL</b>	<b>300</b>
<b>19</b>	<b>CLOSED COUNCIL</b>	<b>300</b>
19.1	TENDER NO 2019-2020-0426 CONCRETE AND MINOR ROAD WORKS <i>Item 19.1 is classified CONFIDENTIAL under the provisions of Section10A(2)(d)(I) of the local government act 1993, as it deals with commercial information of a confidential nature that would, if disclosed prejudice the commercial position of the person who supplied it, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.</i>	
19.2	MEMORANDUM OF UNDERSTANDING WITH UPPER HUNTER SHIRE COUNCIL FOR ACCEPTANCE AND DISPOSAL OF MIXED WASTE <i>Item 19.2 is classified CONFIDENTIAL under the provisions of Section10A(2)(c) of the local government act 1993, as it deals with information that would, if disclosed, confer a commercial advantage on a person with whom the council is conducting (or proposes to conduct) business, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.</i>	
19.3	REQUEST TO REMOVE WASTE MANAGEMENT CHARGE FROM ASSESSMENT <i>Item 19.3 is classified CONFIDENTIAL under the provisions of Section10A(2)(d)(I) of the local government act 1993, as it deals with commercial information of a confidential nature that would, if disclosed prejudice the commercial position of the person who supplied it, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.</i>	
19.4	MUSWELLBROOK INDOOR SPORTS CENTRE - MUSWELLBROOK YOUTH VENUE <i>Item 19.4 is classified CONFIDENTIAL under the provisions of Section10A(2)(d)(I) of the local government act 1993, as it deals with commercial information of a confidential nature that would, if disclosed prejudice the commercial position of the person who supplied it, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.</i>	
19.5	FUTURE FUND COMMITTEE MINUTES <i>Item 19.5 is classified CONFIDENTIAL under the provisions of Section10A(2)(c) of the local government act 1993, as it deals with information that would, if disclosed, confer a commercial advantage on a person with whom the council is conducting (or proposes to conduct) business, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.</i>	
19.6	EXECUTION OF REFINANCING OF \$13.3M FUTURE FUND LOAN	



*Item 19.6 is classified CONFIDENTIAL under the provisions of Section 10A(2)(c) of the local government act 1993, as it deals with information that would, if disclosed, confer a commercial advantage on a person with whom the council is conducting (or proposes to conduct) business, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.*

**19.7 OPTIMISING THE RETURN ON COUNCIL'S CASH INVESTMENTS**

*Item 19.7 is classified CONFIDENTIAL under the provisions of Section 10A(2)(c) of the local government act 1993, as it deals with information that would, if disclosed, confer a commercial advantage on a person with whom the council is conducting (or proposes to conduct) business, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.*

<b>20</b>	<b>RESUMPTION OF OPEN COUNCIL</b>	<b>302</b>
<b>21</b>	<b>CLOSURE</b>	<b>302</b>
	<b>DATE OF NEXT MEETING: TUESDAY 25 AUGUST 2020</b>	<b>302</b>



**MUSWELLBROOK SHIRE COUNCIL  
ORDINARY COUNCIL MEETING**

**AGENDA  
TUESDAY 28 JULY 2020**

- 1 ACKNOWLEDGEMENT OF COUNTRY**
- 2 CIVIC PRAYER**
- 3 APOLOGIES AND LEAVE OF ABSENCE**

**Moved:** \_\_\_\_\_ **Seconded:** \_\_\_\_\_

- 4 CONFIRMATION OF MINUTES OF PREVIOUS MEETING  
RECOMMENDATION**

That the Minutes of the Ordinary Meeting held on **30 June 2020**, a copy of which has been distributed to all members, be taken as read and confirmed as a true record.

**Moved:** \_\_\_\_\_ **Seconded:** \_\_\_\_\_

- 5 DISCLOSURE OF ANY PECUNIARY AND NON-PECUNIARY INTEREST**

Section 451 of the Local Government Act requires that if a councillor or member of a council or committee has a pecuniary interest in any matter before the council or committee, he/she must disclose the nature of the interest to the meeting as soon as practicable and must not be present at, or in sight of, the meeting, when the matter is being discussed, considered or voted on.

A pecuniary interest is an interest that a person has in a matter because of a reasonable likelihood or expectation of financial gain or loss (see sections 442 and 443 of the Local Government Act).

A non-pecuniary interest can arise as a result of a private or personal interest which does not involve a financial gain or loss to the councillor or staff member (eg friendship, membership of an association, or involvement or interest in an activity). A councillor must disclose the nature of the interest to the meeting as soon as practicable.

Council's Model Code of Conduct now recognises two forms of non-pecuniary conflict of interests:

- Significant
- Less than significant

A Councillor must make an assessment of the circumstances and determine if the conflict is significant.

If a Councillor determines that a non-pecuniary conflict of interests is less than significant and does not require further action, they must provide an explanation of why it is considered that the conflict does not require further action in the circumstances.

If the Councillor has disclosed the existence of a significant non-pecuniary conflict of interests at a meeting they must not be present at, or in sight of, the meeting, when the matter is being discussed, considered or voted on.

**6 PUBLIC PARTICIPATION****7 MAYORAL MINUTES****8 GENERAL BUSINESS****9 BUSINESS ARISING**

Nil

## 10 ENVIRONMENTAL SERVICES

### 10.1 PP016 - REZONING - IRONBARK RIDGE

<b>Attachments:</b>	<b>A. Planning Proposal - Ironbark Ridge Extension</b> <b>B. Summary of Submissions with Planning Comment</b> <b>C. Response to Submissions by Proponent</b> <b>D. Original Gateway Determination</b>
<b>Responsible Officer:</b>	<b>Derek Finnigan - Deputy General Manager</b>
<b>Author:</b>	<b>Sharon Pope - Executive Manager - Environment and Planning</b>
<b>Community Plan Issue:</b>	<b><i>Diversify the economy, facilitate the development of intensive agriculture and other growth industries, make the Shire a more attractive place to invest and do business</i></b>
<b>Community Plan Goal:</b>	<b><i>Review the Local Environmental Plan and Development Control Plan to improve investment certainty for industry.</i></b>
<b>Community Plan Strategy:</b>	<b><i>Progress completion of an LEP and DCP.</i></b>

#### PURPOSE

The purpose of the report is to advise Council of the outcomes of the public exhibition of the Planning Proposal to rezone an area of land east of the Ironbark Ridge Estate, being Lot 101 and 103 DP 1170190, from E3 Environmental Management to part R1 General Residential, part R5 Large Lot Residential and part E3 Environmental Management. A total of 34 submissions were received during the exhibition period.

As a result of the exhibition, several changes are proposed to the Planning Proposal. These include:

- Identifying additional land to be zoned R5;
- Identifying two different minimum lot sizes for areas zoned R5 to reflect the transition between existing large lots and proposed standard residential lots;
- Identifying areas to be retained as offset areas within zone E3 Environmental Management; and
- Nominating the site as being in an urban release area, to which Clause 6.3 of *Muswellbrook LEP 2009 (MLEP 2009)* would apply. This means a site specific DCP will need to be adopted by Council before any subdivision of the land may proceed.

The revised Planning Proposal is provided in Attachment A. It is recommended that the amendment to the Muswellbrook Local Environmental Plan (MLEP) 2009 proceed to finalisation.

#### OFFICER'S RECOMMENDATION

##### Council:

1. **Endorses the Planning Proposal contained in Attachment A (Planning Proposal – Rezoning – Ironbark Ridge Extension (PP\_2014\_MUSWE\_003\_00) and requests a PC Opinion to amend Muswellbrook Local Environmental Plan 2009;**
2. **Makes the amending Plan under delegation pursuant to section 3.36 of the Environmental Planning and Assessment Act 1979; and**
3. **Resolves to prepare site specific Development Control Plan provisions for the land, generally in accordance with the plan prepared by MM Hyndes Bailey dated 06.05.2020 and marked as File 20511 offset\_V2.**

Moved: \_\_\_\_\_ Seconded: \_\_\_\_\_

**BACKGROUND**

On 9 November 2012, Council received a request to rezone Lot 101 and 103 DP 1170190, located at Ironbark Road, Muswellbrook, with the intention of future subdivision of the land into residential and large lot residential lots.

At Council's 14 July 2014 Ordinary Council Meeting, Council resolved to accept the planning proposal and to forward it on to the Department of Planning and Environment (DPE) for a Gateway Determination. The Gateway Determination was issued on 21 October 2014 subject to conditions. This included requiring the proponent to provide additional supporting information/investigations, and for Council to consult with state agencies and modify the Planning Proposal, as necessary, prior to public exhibition. A copy of the Gateway Determination is included in Attachment D.

A brief timeline synopsis of the rezoning request consists of:

- 9 November 2012 - Submission of rezoning request to Council.
- 10 December 2012 – Ordinary Meeting of Council resolution to defer until the Muswellbrook Land Use Strategy investigation work had been completed.
- 10 March 2014 – Council adopts the Muswellbrook Residential and Rural Residential Strategy.
- 14 July 2014 - Ordinary Meeting of Council resolution to prepare a planning proposal and to forward on to the Department of Planning and Environment for a Gateway Determination
- 21 October 2014 - Gateway Determination was issued requiring supporting information/investigation and state agency consultation to be completed prior to public exhibition.
- 19 April 2018 – Revised rezoning request submitted to Council for review.
- 13 November 2018 – Council resolved to exhibit the Planning Proposal.
- 1 November 2019 to 16 December 2019 – Public exhibition of Planning Proposal
- 12 May 2020 - Revised concept plan for future subdivisions of the site provided by the Proponent.

**REPORT**

The subject site has an area of 81 hectares and is zoned E3 Environmental Management under the *Muswellbrook Shire Local Environment Plan (MLEP) 2009*. The Proponent seeks to change the current zoning of Lot 101 & 103 DP 1170190 to:

- Part R1 General Residential; and
- Part R5 Large Lot Residential.



Rezoning would:

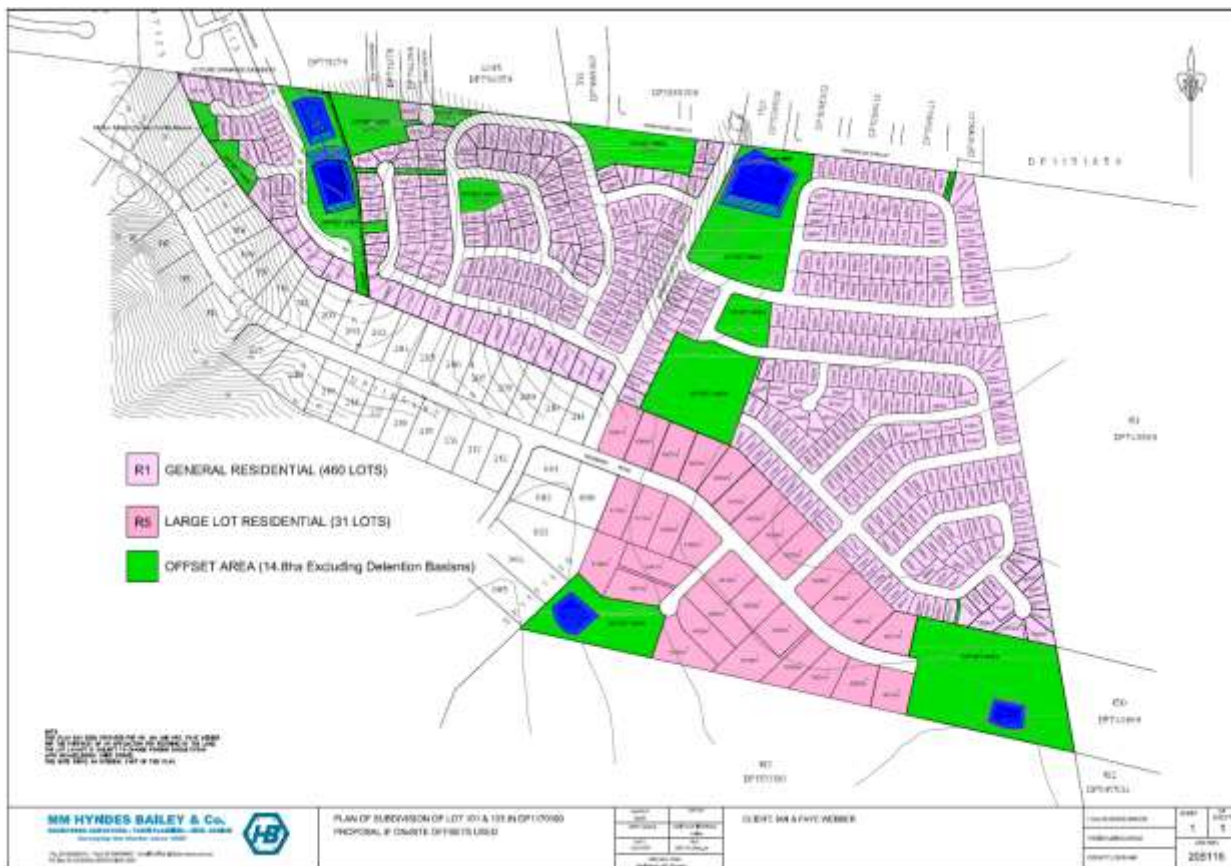
- Allow subdivision into a range of residential lot sizes, consistent with adjoining development.
- Increase potential housing supply in Muswellbrook.
- Be a logical extension of, and connection between, existing residential areas.
- Use existing and proposed servicing infrastructure.

The Planning Proposal that was exhibited was based on the following development concept:





Following the public exhibition period, and feedback from Councillors, the Proponent has proposed an amended development concept dated 06.05.2020 and provided below:



In response, Council staff have identified changes to the Planning proposal. These include:

- Identifying additional land to be zoned R5. The Proponent has reconfigured lots that adjoin existing properties in Ironbark Ridge to increase sizes to between 1500m<sup>2</sup> and 2000m<sup>2</sup>. Given the purpose of these lots is to provide a transition between the existing R5 zoned land and proposed R1 zoned land, the R5 zone objectives and permitted land uses are a better fit for this purpose. For example, while secondary dwellings may be permitted on R5 zoned lots, boarding houses, multi-dwelling housing and residential flat buildings would be prohibited.
- Identifying two different minimum lot sizes for areas zoned R5 to reflect the transition between existing large lots and proposed standard residential lots. It is proposed to apply a minimum lot size of 1500m<sup>2</sup> to the R5 zoned land that is intended to be a transition to the R1 zoned land. The combination of zone and minimum lot size would restrict any further subdivision of these lots.
- Identifying areas to be retained as offset areas as zone E3 Environmental Management. The concept plan indicates several locations intended to be retained for offsets and environmental purposes, such as stormwater quality improvement devices. The E3 zone objectives are a better fit for this purpose than the RE1 Public Recreation zone objectives. There is also an issue where land zoned RE1 must either be owned by Council or intended to be purchased by Council. It would be better to decide about acceptance of the dedication of this land to Council at the subdivision stage, when more detail on the maintenance regime is available.
- Nominating the site as being in an urban release area, to which Clause 6.3 of *Muswellbrook LEP 2009 (MLEP 2009)* would apply. This was also a requirement of the Gateway Determination. This means site specific Development Control Plan (DCP) provisions will need to be adopted by Council before any subdivision of the land may proceed.

Clause 6.3 is a standard clause from the Standard Local Environment Plan (LEP) template, that applies to any urban release area in NSW. It is intended to establish a framework for the subdivision of greenfield urban zoned land to establish a logical development pattern. In this case it would allow

Council to ensure future subdivision is generally in accordance with the plan prepared by MM Hyndes Bailey dated 06.05.2020 and marked as File 20511 offset\_V2.

The Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.

### **CONSULTATION**

Consultation occurred with the following agencies between October 2014 and April 2018:

- Department of Primary Industries – Agriculture & Water
- Mine Subsidence Board
- NSW Aboriginal Lands Council
- Wararuah LALC
- Environment & Heritage
- Resources & Energy
- Roads & Maritime Services
- NSW Rural Fire Service

The Planning Proposal was publicly exhibited from the period 01/11/2019 to 16/12/2019. Public notice was given in the Muswellbrook Chronicle, and written letters were sent to adjoining owners. Council received 34 submissions. A summary of the submissions with planning comments, is provided in Attachment B.

The issues raised relate to:

- Changed amenity
- Loss of property value
- Oversupply of housing
- Increased land rates
- Increased crime
- Increased traffic
- Inadequate Infrastructure
- Drainage problems
- Emergency evacuation issues
- Environmental impacts
- Proposal contrary to Council policies
- Muswellbrook Residential and Rural Residential Strategy is out of date

The Proponent's response to submissions is provided in Attachment C.

Several issues raised have been addressed by changes to the concept plan.

Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.



The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Road. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.

### **CONSULTATION WITH COUNCILLOR SPOKESPERSON**

The Planning Proposal was discussed at a briefing with Councillors on 26 November 2019. Councillors asked that the Proponent consider changes to the concept layout and proposed zones to:

- Create a transition between the existing large lots along Ironbark Rd and the proposed new residential lots; and
- Create a road interface to Acacia Drive Reserve, which contains the water tower.

### **OPTIONS**

The following are available:

1. Council resolves to support the Planning Proposal in Attachment A to amend *MLEP 2009* to rezone the land. This is the preferred option; or
2. Council resolves to make further changes to the Planning Proposal to amend *MLEP 2009* to rezone the land; or
3. Council resolves not to support the Planning Proposal to amend *MLEP 2009*. This is not recommended as the Muswellbrook Residential and Rural Residential Strategy, adopted by Council on 10 March 2014, identified this land as being suitable for residential and large lot residential development. Zoned land will contribute to the potential supply and diversity of housing types in the Shire, which satisfies requirements of the Hunter Regional Plan and Upper Hunter Strategic Regional Land Use Plan.

### **CONCLUSION**

The proposed amendment to *MLEP 2009* will provide zoned land suitable for a variety of residential development, thus providing continuing housing supply and choice in the local government area. Important flora and fauna habitat will be retained within areas zoned E3 Environmental Management.

### **SOCIAL IMPLICATIONS**

The proposed rezoning would allow subdivision of the land into approximately 500 lots. These are expected to be released over multiple stages according to demand (likely to be 20-30 lots per stage). The development of the land extends from existing land developed for residential and large lot residential purposes in the locality.

It is unlikely that there would be any adverse social impacts.

### **FINANCIAL IMPLICATIONS**

In terms of economic benefits, there will be employment opportunities during the construction of the various stages of the subdivision and houses, and increased choice for housing in the Shire.

There will be no financial implications for Council at this stage, apart from staff resources involved in the processing of the LEP amendment. As subdivision occurs Council will take on responsibility for new assets that will need to be maintained.

### **POLICY IMPLICATIONS**

The Planning Proposal will result in an amendment to Muswellbrook Local Environment Plan 2009.

### **STATUTORY IMPLICATIONS**

There are no statutory implications under the *Local Government Act 1993*.

---

**LEGAL IMPLICATIONS**

There are no legal implications for Council, provided the amendment process follows the process outlined in the *Environmental Planning & Assessment Act 1979*, and the *Environmental Planning & Assessment Regulation 2000*.

**OPERATIONAL PLAN IMPLICATIONS**

The proposed amendments are consistent with the objectives of *Council's Operational Plan 2020/21*.

**RISK MANAGEMENT IMPLICATIONS**

The risks associated with preparing a Planning Proposal are minimised by following the process outlined in the *Environmental Planning & Assessment Act 1979*, the *Environmental Planning & Assessment Regulation 2000*.

## Planning Proposal – Ironbark Ridge Extension Rezoning

### PP\_2014\_MUSWE\_003\_00

<b>Local Government Area:</b>			Muswellbrook Shire Council (MSC)
<b>Name of Draft LEP:</b>			Muswellbrook Local Environment Plan 2009 - PP_2014_MUSWE_003_00
<b>Subject Land:</b>			Lot 101 and 103 DP 1170190 Muswellbrook.
<b>Land Owner:</b>			Mr. I R and Mrs. F Webber
<b>Applicant:</b>			Casson Planning & Development Services
<b>Folder Number:</b>			PP 009 - Rezoning of Lots 101 and 103 DP 1170190 from E3 Environmental Management to part R1 General Residential, part R5 Large Lot Residential and part E3 Environmental Management
<b>Date:</b>			16 July 2020
<b>Author:</b>			David Casson & Libby Cumming & Sharon Pope
<b>Tables:</b>	<b>Table No.</b>	<b>Details</b>	
	1	Absolute Constraints	
	2	MRRRS Recommendations	
	3	Assessment of the Planning Proposal against relevant SEPPs	
	4	Consistency with applicable Ministerial Directions	
<b>Figures</b>	<b>No.</b>	<b>Details</b>	
	1	Extract from MSC Developer Servicing Plan – Sewer	
	2	Extract from MSC Developer Servicing Plan – Water	
	3	Extract from MRRRs	
	4	Extract from Terrestrial Biodiversity Map – Muswellbrook LEP 2009	
	5	Bushfire Prone Vegetation	
<b>Maps:</b>	<b>No.</b>	<b>Details</b>	
	1	Locality	
	2	Site Identification	
	3	Amended Conceptual Layout	
	4	Land Zoning Maps – Sheets LZN_008 and LZN_008A	
	5	Lot Size Maps - Sheets LSZ_008 and LSZ_008A	

---

**Ironbark Ridge Extension Planning Proposal**

---

6	Height of Buildings Maps – Sheets HOB_008 and HOB_008A
7	Floor Space Ratio Maps – Sheets FSR_008 and FSR_008A
8	Urban Release Area Map – Sheets URA_008 and FSR_008A

<b>Attachments:</b>	No.	Details
	1	Appendixes
	2	Evaluation criteria for the delegation of plan making functions

## Ironbark Ridge Extension Planning Proposal

### Part 1 – OBJECTIVES OR INTENDED OUTCOMES

The objective of the Planning Proposal is to amend Muswellbrook Local Environmental Plan 2009 (MLEP 2009) in order to rezone Lots 101 and 103 DP 1170190 from E3 Environmental Management to part R1 General Residential, part R5 Large Lot Residential and part E3 Environmental Management. It is intended to amend the Land Zoning map, Lot Size map, Floor Space Ratio map, the Height of Building map and Urban Release Area map.

The intended outcome is to provide land that will:

- Allow subdivision into a range of residential lot sizes, consistent with adjoining land use;
- Provide land for a variety of residential development to be undertaken and thereby contribute towards the supply of housing in Muswellbrook;
- Enable the logical extension of, and connection between existing residential areas; and
- Provide for the efficient use of existing and proposed servicing infrastructure.

### Part 2 – EXPLANATION OF PROVISIONS

The proposed objective will be achieved by amending the MLEP 2009 by: -

Amendment Applies to	Explanation of provision
Land Zoning maps – Sheets LZN_008 and LZN_008A	To rezone land from E3 Environmental Management to R1 General Residential, R5 Large Lot Residential and E3 Environmental Management (See Map 4)
Lot Size maps - Sheets LSZ_008 and LSZ_008A	To alter the lot sizing as per Map 5.
Height of Buildings maps – Sheets HOB_008 and HOB_008A	To alter the height of buildings as per Map 6.
Floor Space Ratio maps – Sheets FSR_008 and FSR_008A	To alter the floor space ratios as per Map 7.
Urban Release Area maps – Sheets URA_008 and URA_008A	To identify the land as an Urban release Area as per Map 8.

### Part 3 – JUSTIFICATION

#### Section A – Need for the Planning Proposal

##### 1. *Is the planning proposal a result of any strategic study or report?*

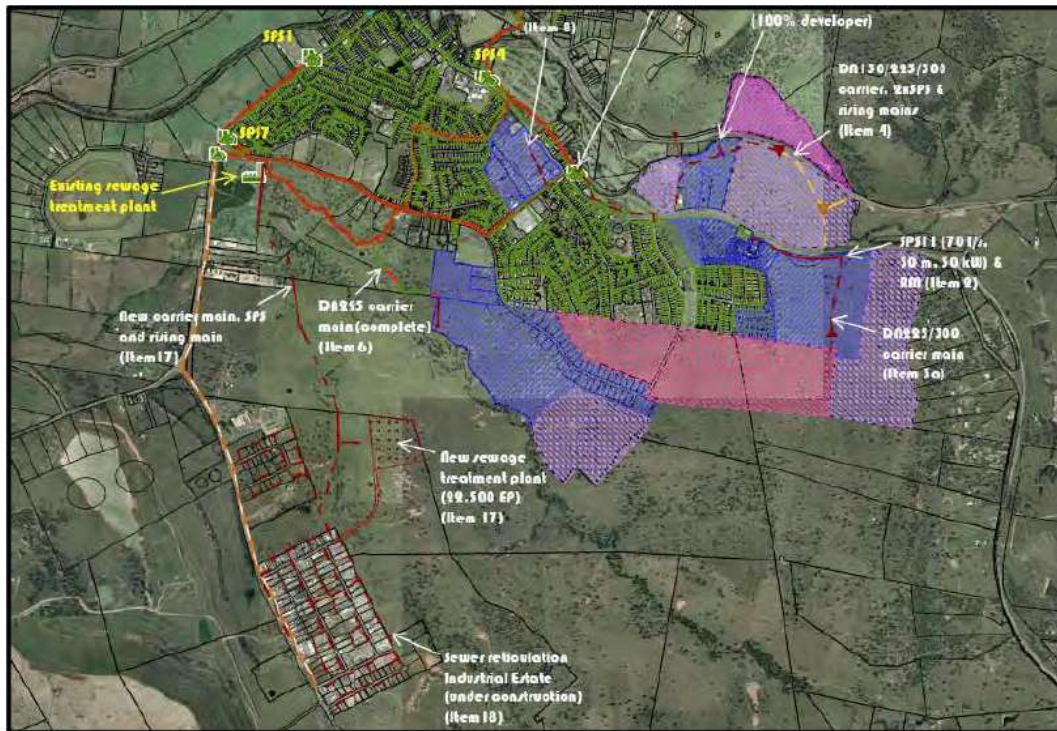
The planning proposal is supported by the Muswellbrook Residential and Rural Residential Strategy (MRRRS) adopted by Council at its ordinary meeting on 10 March 2014.

The MRRRS identifies land suitable for residential and large lot residential and prioritises rezoning of such land. The subject land is within an area identified as “Category 1” land in the above strategy (Candidate Area D - refer to below Figure 1).

### Ironbark Ridge Extension Planning Proposal

The Strategy states that 'Category 1' land as "land that is a logical extension of and located in close proximity to existing developed areas, building upon existing services and amenities and requires the augmentation of infrastructure".

The MRRRS considered three scenarios for population growth in the LGA. The medium growth scenario assumes an average annual growth rate of 1.2% which results in an additional 3828 residents by 2031. Rezoning of the subject land will ensure that appropriate land is available to meet demand for residential land which will be evident over the next 20 years.

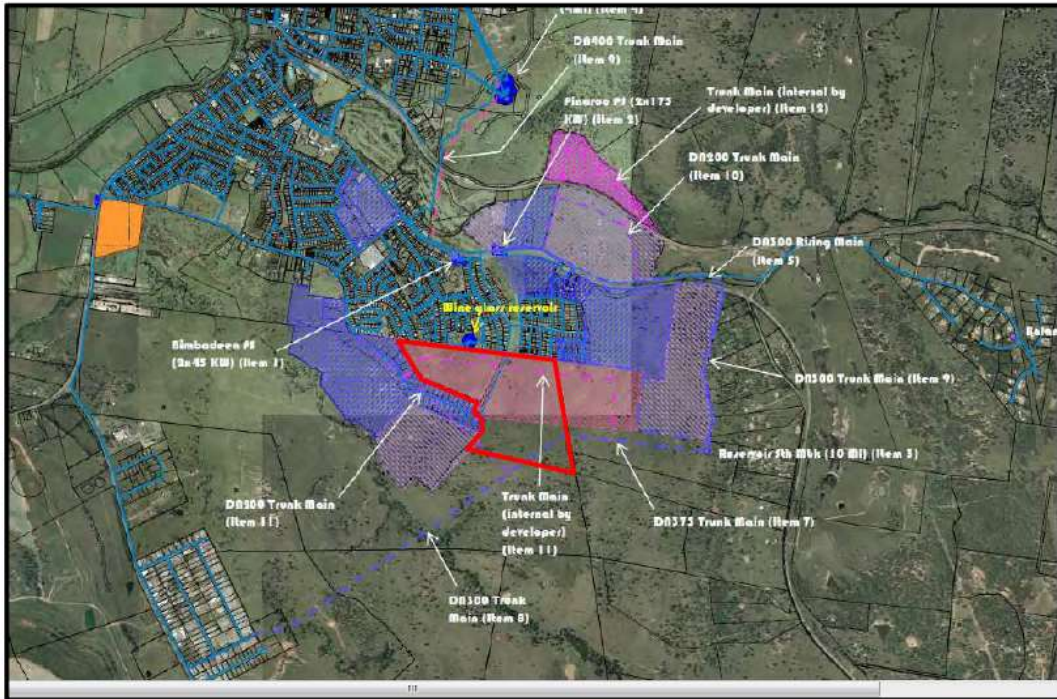


**Figure 1 - Extract from MSC Developer Servicing Plan – Sewer**

Council's Developer Servicing Plans also identifies the land as being suitable for the extension of new, and augmentation of existing services for water and sewer infrastructure to service the subject land and other adjoining land which is being developed. (Refer to Figure 1 and 2).



## Ironbark Ridge Extension Planning Proposal



**Figure 2 - Extract from MSC Developer Servicing Plan – Water**

Figure 1 and 2 above clearly shows the merit of rezoning the land, in terms of its location in relation to proposed extension of sewer and water services for nearby development. The Trunk Main South (sewer) shown above bisects the subject land and provides access to sewer for the future residential supply. The land is well located in respect of the proposed new sewer treatment plant.

2. *Is the planning proposal the best means of achieving the objectives or intended outcomes, or is there a better way?*

The rezoning would provide opportunities for residential development that will be contiguous with the adjoining existing residential zoned land and is the "logical link" between existing residential zones.

The following represents a summary of the findings of the absolute constraints for Candidate area D (the subject land), Urban Release Area North and Urban Release Area South:

### Ironbark Ridge Extension Planning Proposal

**Table 1**  
**Absolute Constraints**

Area	Total Constrained (ha)	Total Unconstrained (ha)	% Constrained
Muswellbrook D	5.0	235.6	2%
URA North	12.9	110.6	10%
URA South	25.0	26.7	48%

Candidate Area D ranked as more constrained for environmental constraints, due to it being within a Mines Subsidence Area. In terms of infrastructure constraints, Candidate Area D was the least constrained, and from a total constraint (environmental and infrastructure) ranking perspective, Candidate Area D was the least constrained followed by URA South then URA North.

Council adopted the MRRRS in March 2014 with its recommendations in respect of both General Residential and Rural Residential land for Muswellbrook:

**Table 2**  
**MRRRS Recommendations**

Type	Supply – demand gap snapshot (medium scenario)	Preferred sequencing	Timing for new release	Comment
General Residential	Forecast shortage between 2021 and 2026	1. Candidate Area D 2. URA South 3. URA North	New supply required after 2016 and before 2021	To ensure sufficient contestability and hence competition in the general residential market, at least one additional development front or new developer should be contemplated, particularly where it is sequential development and there is no net cost to Council or the State Government
Rural Residential	Forecast shortage between 2021 and 2026	1. Candidate Area B 2. Candidate Area A 3. Candidate Area C	New supply required after 2016 and before 2021	To ensure sufficient contestability and hence competition in the rural residential market, at least one additional development front or new developer should be contemplated, particularly where it is sequential development and there is no net cost to Council or the State Government

The subject land is included in Candidate Areas D and A referred to in the above recommendations (see Figure 3).



## Ironbark Ridge Extension Planning Proposal

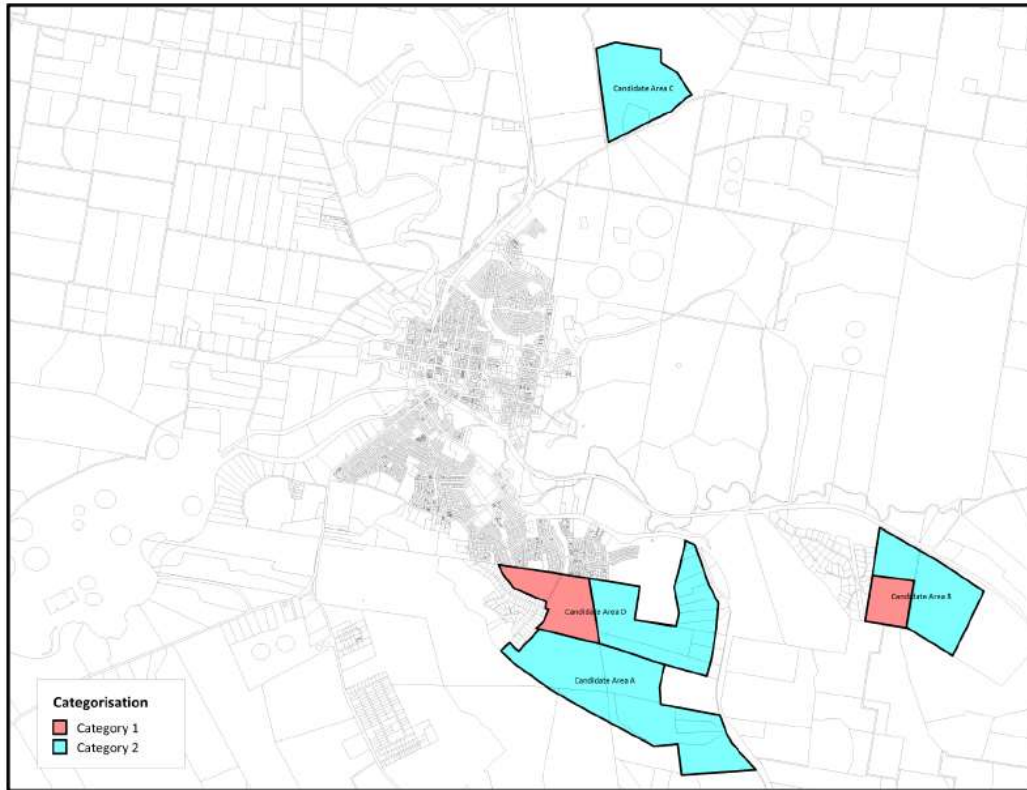


Figure 3 - Extract from MRRRS

The planning proposal is the best means of achieving the rezoning of subject land.

### Section B – Relationship to Strategic Planning Framework

#### 3. *Is the planning proposal consistent with the objectives and actions of the applicable regional or sub-regional strategy (including the Sydney Metropolitan Strategy and exhibited draft strategies)?*

##### Upper Hunter Strategic Regional Land Use Plan (UHSRLUP)

The UHSRLUP “*outlines a range of key challenges facing the Upper Hunter region and lists clear actions to address these challenges*” (p7). One of the key areas applies to Housing and Settlement which is particularly relevant to this Planning Proposal.

The plan indicates that “*in Muswellbrook there is potential for a further 1,300 new dwellings in the zoned residential land at South Muswellbrook. This is expected to be adequate for these areas for the short to medium term. However, with the additional demands coming from the growth of mining, councils will need to continue to assess the adequacy of supply of housing in the region*” (p51).

It also states that “*Housing affordability is highly varied across the region. However, one issue that is relevant across the entire region is the current lack of housing choice through the provision of smaller and more affordable housing options such as one and two bedroom*

---

**Ironbark Ridge Extension Planning Proposal**

---

houses and town houses. This current lack of housing choice is significantly contributing to housing stress" (p51), and that:

*"Rural residential and lifestyle housing growth should occur in close proximity to existing centres, towns and villages and is to be consistent with the settlement planning principles identified below with regard to residential lands" (p52).*

The Settlement Planning Principles set out for planning for housing growth in the UHSRLUP are:

- *Development will contribute to the diversity of housing types available. Any medium or higher density housing should be located in central and accessible locations to ensure access to a full range of services within a reasonable walking distance.*  
The rezoning would permit a range of housing types. It also forms a natural and logical extension from the existing Iron Bark Ridge Estate. It will provide for continuation of the roads, cycleway, and pedestrian path from the existing residential areas to proposed area.
- *Development will be located to maximise the efficiency of essential urban infrastructure, services and facilities, including transport, health and education.*  
The planning proposal seeks to rezone land to provide residential development. The site is considered to be suitably located for access to utilities and infrastructure.
- *Development will respect and respond to the character of the area and the identified settlement hierarchy of the region*  
The land represents an extension of existing large lot residential development and adjoins existing residential zoned land, repeating the settlement patterns in the immediate locality.
- *New residential areas will be planned with streets that make it easy for people to walk and cycle and with recreational and open space.*  
Consideration will be given to extension of footpath/cycleway within the subject site and link it to the existing/proposed open space and recreational areas consistent with Council requirements set out in Muswellbrook DCP 2009 and Auspec.
- *New residential and rural residential areas will respect environmental and cultural heritage and avoid areas most affected by natural hazards or having high cultural significance*  
The area does not raise any concerns in respect of environmental or cultural heritage issues, or natural hazards. Land with high environmental values are intended to remain zoned E3 Environmental Management.
- *New residential and rural residential areas should minimise the potential for land use conflict with land needed for valuable economic activities, such as valuable agricultural lands and natural resource lands. This includes avoiding locations where possible adverse impacts associated with industry (such as noise, dust, visual impacts or other amenity impacts) are likely to affect future residents*  
The land is being used for extensive agriculture with the grazing of cattle. The existing adjoining land uses of large lot residential and residential will effectively be 'mirrored' by this proposal. Rezoning of the subject land would not result in a loss of prime agricultural land or employment land.
- *New rural residential areas should be located adjacent to, or in close proximity to, existing urban centres and be within easy access of relevant infrastructure and services.*

---

### Ironbark Ridge Extension Planning Proposal

---

The land is adjacent to existing land zoned for such purposes and represents a logical extension of the urban area. Infrastructure and services are available and to be extended and/or augmented to develop the subject land.

The Settlement Planning Principles then lead into the following policy objectives:

- *Ensure an adequate supply of housing to meet community needs.*  
The rezoning of the land will ensure that there is not only adequate supply of vacant residential land, but choice of location and lot sizes for landowners.
- *Ensure a greater diversity of housing types, including smaller housing types, rental housing and temporary housing.*  
The subject land would have two zones allowing for a range of housing types, in accord with the zoning objectives and minimum lot size maps.
- *Improve the supply and range of affordable housing*  
Potential for a range of housing types provides potential for increased range of affordable housing.
- *Build cohesive and liveable communities by ensuring towns and villages are well designed, liveable and provide a range of housing types*  
New roads connections in the future will improve connection between residential areas. New open space and environmental areas will become available for public use.
- *Action 6.1 - Work with Singleton and Muswellbrook Councils to identify the nature of change in population growth and demand for housing and to identify sufficient land and other opportunities to facilitate an adequate supply of appropriately located housing to meet identified demand.* The identification of the subject land as being appropriate for residential and large lot residential development has been indicated in MRRRS. A review of existing residential zoned land within the general locality and of the land identified as urban release area, would suggest that it is timely to consider rezoning of additional land for such purposes.

Council's Developer Servicing Plan have identified the subject land for future development and infrastructure for both water and sewerage mains have been planned to have capacity to cater for its development as well as other land in the locality which is being developed.

#### Hunter Regional Plan 2036 (HRP)

The HRP sets out a vision for the Hunter Region. The proposed amendment to MLEP 2009 will address:

*Direction 17:- Create healthy built environments through good design*

*Action 17.1 - Develop best-practice guidelines for planning, designing and developing healthy built environments.*

*Direction 21:- Create a compact settlement*

*Direction 21.1 - Promote development that respects the landscape attributes and the character of the metropolitan areas, towns and villages.*

---

**Ironbark Ridge Extension Planning Proposal**

---

*Direction 21.4 - Create a well-planned, functional and compact settlement pattern that responds to settlement planning principles and does not encroach on sensitive land uses, including land subject to hazards, on drinking water catchments or on areas with high environmental values.*

*Direction 21.6 - Provide greater housing choice by delivering diverse housing, lot types and sizes, including small-lot housing in infill and greenfield locations.*

*Direction 21.7 - Promote new housing opportunities in urban areas to maximise the use of existing infrastructure.*

*Direction 22:- Promote housing diversity*

*Direction 22.2 - Encourage housing diversity, including studios and one and two-bedroom dwellings, to match forecast changes in household sizes.*

*Direction 22.3 - Develop local housing strategies to respond to housing needs, including social and affordable housing, and support initiatives to increase the supply of affordable housing.*

*Direction 25:- Monitor housing and employment supply and demand*

*Direction 25.3 - Sequence new greenfield urban development that makes efficient use of infrastructure networks and capacity.*

*Direction 26:- Deliver infrastructure to support growth and communities*

*Direction 26.1 - Align land use and infrastructure planning to maximise the use and capacity of existing infrastructure and the efficiency of new infrastructure.*

**4. *Is the planning proposal consistent with a council's local strategy or other local strategic plan?***

Muswellbrook Community Strategic Plan 2017 – 2027

The proposal is considered consistent with the following goals within the Muswellbrook Community Strategic Plan 2017 – 2027:

### Ironbark Ridge Extension Planning Proposal

**Issue:** *Community Infrastructure*

**Goal 19:** *Our community's infrastructure is planned well, is safe and reliable and provides required levels of service*

**Goal 20:** *A safe, secure and reliable water supply and sewerage services are provided to all residents that will ensure public health.*

**Goal 21:** *The road, footpath and cycleway networks are integrated and allow for the safe movement of residents around the Shire.*

#### 5. ***Is the planning proposal consistent with applicable State Environmental Planning Policies?***

The Planning Proposal is consistent with the following relevant State Environmental Planning Policies (SEPPs) outlined in Table 1 below.

**Table 3: Assessment of the Planning Proposal against relevant SEPPs**

SEPP	Relevance	Implications
<b>SEPP 6 — Number of Storeys in a Building</b>	The SEPP clarifies the reference to storey, floors and levels.	It is not proposed to include any provisions which would be inconsistent with the SEPP.
<b>SEPP 33 — Hazardous and Offensive Development</b>	The SEPP provides considerations for consent for hazardous & offensive development.	It is not proposed to include any provisions which would be inconsistent with the SEPP.
<b>SEPP (Koala Habitat Protection) 2019</b>	This Policy aims to encourage the conservation and management of areas of natural vegetation that provide habitat for koalas to support a permanent free-living population over their present range and reverse the current trend of koala population decline.	It is not proposed to include any provisions which would be inconsistent with the SEPP. Flora and Fauna investigations completed for the Planning Proposal have not identified the site as providing habitat for koalas. Significant tree stands will be zoned E3 Environmental Management.
<b>SEPP 55 — Remediation of Land</b>	This SEPP applies to land across NSW and states that land must not be developed if it is unsuitable for a proposed use because of contamination.	The previous use of the land has been limited to light agricultural practices only and it is unlikely to contain any contaminants. A preliminary site investigation has been carried out with findings that indicate contamination.
<b>SEPP 64 — Advertising and Signage</b>	The SEPP aims to ensure that outdoor advertising is compatible with the desired amenity and visual character of an area, provides effective communication in suitable locations and is of high-quality design and finish.	It is not proposed to include any provisions which would be inconsistent with the SEPP.

### Ironbark Ridge Extension Planning Proposal

<b>SEPP 65 — Design Quality of Residential Flat Development</b>	The SEPP relates to residential flat development across the state through the application of a series of design principles. Provides for the establishment of Design Review Panels to provide independent expert advice to councils on the merit of residential flat development.	It is not proposed to include any provisions which would be inconsistent with the SEPP.
<b>Housing for Seniors or People with a Disability 2004</b>	The SEPP aims to encourage provision of housing for seniors, including residential care facilities. The SEPP provides development standards.	It is not proposed to include any provisions which would be inconsistent with the SEPP.
<b>Building Sustainability Index: BASIX 2004</b>	The SEPP provides for the implementation of BASIX throughout the State.	It is not proposed to include any provisions which would be inconsistent with the SEPP.
<b>Mining, Petroleum Production &amp; Extractive Industries 2007</b>	The SEPP aims to provide proper management of mineral, petroleum and extractive material resources and ESD.	The subject land is not within the Mines Subsidence Area (refer Appendix A).  Consultation with NSW Resources & Energy has been undertaken and their response is included in Appendix B, confirming that the subject land is not covered by a current coal title. There is however a Petroleum Exploration Licence over a broad regional area which includes the subject site. The letter in Appendix B was dated 8/12/14. Appendix L includes a subsequent update from the NSW Department of Planning and Environment, Division of Resources & Geoscience, Geological Survey confirming that the previous advice was still valid.
<b>Major Development 2005</b>	The SEPP defines certain developments that are major projects to be assessed under Part 3A of the Environmental Planning and Assessment Act 1979 and determined by the Minister for Planning. It also provides planning provisions for State significant sites. In addition, the SEPP identifies the council consent authority functions that may be carried out by Joint Regional Planning Panels (JRPPs) and classes of regional development to be determined by JRPPs.	It is not proposed to include any provisions which would be inconsistent with the SEPP.
<b>Infrastructure 2007</b>	The SEPP provides a consistent approach for infrastructure and the provision of services across NSW, and to support greater efficiency in the location of infrastructure and service facilities.	It is not proposed to include any provisions which would be inconsistent with the SEPP.

### Ironbark Ridge Extension Planning Proposal

<b>SEPP (Primary Production and Rural Development) 2019</b>	The SEPP aims to facilitate economic use and development of rural lands, reduce land use conflicts and provides development principles.	<p>The subject land adjoins land currently zoned and developed as residential and large lot residential land. The land has been identified as appropriate for such use in Council's strategic planning documents. The land represents a relatively small reduction in available agricultural land and is well suited for residential and large lot residential development due to its location, availability of services and proximity of existing residential and large lot residential land uses.</p> <p>NSW Department of Primary Industries have confirmed that there is no objection to the planning proposal for the subject land – refer to Appendix I.</p> <p>The proposed changes under this planning proposal are considered of minor significance and are considered to be consistent with the intent of this SEPP.</p>
<b>Exempt and Complying Development Codes 2008</b>	The SEPP provides exempt and complying development codes that have State-wide application, identifying, in the General Exempt Development Code, types of development that are of minimal environmental impact that may be carried out without the need for development consent; and, in the General Housing Code, types of complying development that may be carried out in accordance with a complying development certificate.	It is not proposed to include any provisions which would be inconsistent with the SEPP.
<b>Affordable Rental Housing 2009</b>	The SEPP provides for an increase in the supply and diversity of affordable rental and social housing in NSW.	It is not proposed to include any provisions which would be inconsistent with the SEPP.

#### 6. *Is the planning proposal consistent with applicable Ministerial Directions?*

An assessment of the Planning Proposal and its consistency against the applicable Ministerial Directions is provided at Table 4 below.

### Ironbark Ridge Extension Planning Proposal

Table 4: Consistency with applicable Ministerial Directions

Ministerial Direction	Objective/s	Consistency / Comment
<b>1. Employment and Resources</b>		
<b>1.1 Business and Industrial Zones</b>	(a) Encourage employment growth in suitable locations, (b) Protect employment land in business and industrial zones, and (c) Support the viability of identified strategic centres.	N/A
<b>1.2 Rural Zones</b>	Protect the agricultural production value of rural land.	<p>This proposal seeks to rezone the subject land from E3 Environmental Management to part R1 General Residential and part R5 Large Lot Residential. The rezoning of the land is not consistent with the Direction; however it is consistent with the recommendations of the MRRRS adopted by Council and endorsed by the Department of Planning, Industry and Environment.</p> <p>The land is not prime agricultural land and it has been used for extensive agricultural activities in the past, predominantly grazing. The rezoning of the land will reduce the amount of available land for potential agricultural production, however the historic use of the land would suggest that such loss would be minimal. The land is ideally suited in terms of proximity to existing, similar residential and rural residential development and available / proposed infrastructure.</p> <p>NSW Department of Primary Industry have offered no objection to the Planning Proposal – refer Appendix I.</p> <p>The amendment is therefore considered of minor significance and its impacts to the rural zone also negligible.</p>



### Ironbark Ridge Extension Planning Proposal

<b>1.3 Mining, Petroleum Production and Extractive Industries</b>	Ensure that the future extraction of State or regionally significant reserves of coal, other minerals, petroleum and extractive materials are not compromised by inappropriate development.	The proposed R1 General Residential and R5 Large Lot Residential zones do not permit extractive industries. Given the proximity of the site to the existing residential areas, it is considered unlikely that the site would be deemed suitable for future mining or quarrying activities due to impacts. Advice from Resources & Energy dated 12/10/12 offered no objection to the proposed rezoning due to "faulted nature of the region and cindered coal seams" (refer Appendix B)  In this context, the planning proposal is consistent with the intent of this direction.
<b>1.4 Oyster Aquaculture</b>	Ensure Priority Oyster Aquaculture areas and oyster aquacultures generally are adequately considered when preparing a planning proposal.	N/A
<b>1.5 Rural Lands</b>	Ensure Planning Proposals that modify existing rural or environmental protection zones or minimum lot sizes to be consistent with SEPP Rural Lands 2008.	The rezoning of the land is not consistent with the Direction, however it is consistent with the recommendations of the MRRRS adopted by Council. The MRRRS has been endorsed by the Department of Planning, Industry and Environment.  The land the subject of this application is not prime agricultural land and it has been used for extensive agricultural activities in the past, predominantly grazing.  The rezoning will be of minor significance. NSW Department of Primary Industry have offered no objection to the Planning Proposal – refer Appendix L.
<b>2. Environment and Heritage</b>		
<b>2.1 Environment Protection Zones</b>	Protect and conserve environmentally sensitive areas.	The subject site is currently zoned as E3 Environmental partially identified as containing Terrestrial Biodiversity. It is proposed to rezone subject site to part R1 General Residential, part R5 Large Lot Residential and part E3 Environmental Management.  A Seven Part Test on Threatened Flora and Fauna by <i>Wildthing Environmental Consultants</i> has been commissioned for the subject land. The report

---

Ironbark Ridge Extension Planning Proposal

---

		<p>concluded that “with the application of the recommendations within this report it is unlikely that the proposed rezoning will be a significant adverse impact upon any viable local populations or individuals of the threatened species, populations or individuals of the threatened species, populations or communities considered in this report.”</p> <p>Office of Environment &amp; Heritage – refer to Appendix G1. The correspondence dated 18/11/14 indicated that additional survey was required during the flowering period of two nominated flora species, and also in respect of hollow bearing trees. This survey / assessment has been undertaken with the additional reports in Appendix D. Significant vegetation will be retained in the E3 zone.</p> <p>Therefore, the rezoning and subsequent residential development of the land in accordance with the recommendations will be managed to ensure it is of minor significance.</p>
--	--	--

### Ironbark Ridge Extension Planning Proposal

<b>2.2 Coastal Protection</b>	Implement the principles in the NSW Coastal Policy.	N/A
<b>2.3 Heritage Conservation</b>	Conserve items, areas, objects and places of environmental heritage significance and indigenous heritage significance.	Not within a heritage conservation area and there are no heritage listed items within the site. OEH is of the view that there are no associated constraints relating to Aboriginal cultural heritage.
<b>2.4 Recreation Vehicle Areas</b>	Protect sensitive land or land with significant conservation values from adverse impacts from recreation vehicles.	N/A
<b>3. Housing, Infrastructure &amp; Urban Development</b>		
<b>3.1 Residential Zones</b>	<p>(a) Encourage a variety and choice of housing types to provide for existing and future housing needs,</p> <p>(b) Make efficient use of existing infrastructure and services and ensure that new housing has appropriate access to infrastructure and services, and</p> <p>(c) Minimise the impact of residential development on the environment and resource lands.</p>	It is proposed to rezone subject site to two residential zones : R1 General Residential and R5 Large Lot Residential. The planning proposal is consistent with the recommendations of the MRRRS adopted by Council. The site is identified as the preferred candidate area of 'Category 1' in Muswellbrook. The rezoning will increase the diversity of housing options and provide contestability in the market. The planning proposal is consistent with the intent of this direction.
<b>3.2 Caravan Parks and Manufactured Home Estates</b>	<p>(a) Provide for a variety of housing types, and</p> <p>(b) Provide opportunities for caravan parks and manufactured home estates.</p>	N/A
<b>3.3 Home Occupations</b>	Encourage the carrying out of low-impact small businesses in dwelling houses.	Home occupations are permitted without consent in the proposed zones under Muswellbrook LEP 2009. The planning proposal is consistent with this direction.
<b>3.4 Integrating Land Use and Transport</b>	<p>Ensure that urban structures, building forms, land use locations, development designs, subdivision and street layouts achieve the following planning objectives:</p> <p>(a) improving access to housing, jobs and services by walking, cycling and public transport, and</p> <p>(b) increasing the choice of available transport and reducing dependence on cars, and</p> <p>(c) reducing travel demand including the number of trips generated by development and the distances travelled, especially by car, and</p>	<p>The rezoning is an extension of an established urban area and the existing street network/footpaths. The development will require extension to existing roads, footpath/cycleway with the locality, which will lead to improved connectivity outcomes for both existing and future residents in the area.</p> <p>See the report by Better Transport Future in Appendix H.</p>

### Ironbark Ridge Extension Planning Proposal

	(d) supporting the efficient and viable operation of public transport services, and (e) providing for the efficient movement of freight.	
<b>3.5 Development Near Licensed Aerodromes</b>	(a) Ensure the effective and safe operation of aerodromes, and (b) Ensure that their operation is not compromised by development that constitutes an obstruction, hazard or potential hazard to aircraft flying in the vicinity, and (c) Ensure development for residential purposes or human occupation, if situated on land within the Australian Noise Exposure Forecast (ANEF) contours of between 20 and 25, incorporates appropriate mitigation measures so that the development is not adversely affected by aircraft noise.	N/A
<b>3.6 Shooting Ranges</b>	The objective of this direction is to maintain appropriate levels of public safety and amenity, reduce land use conflict and identify issues that must be addressed when rezoning land adjacent to an existing shooting range.	N/A
<b>4. Hazard &amp; Risk</b>		
<b>4.1 Acid Sulfate Soils</b>	Avoid significant adverse environmental impacts from the use of land that has a probability of containing acid sulfate soils.	This direction does not apply as the site is noted as not having Acid Sulphate Soils.

### Ironbark Ridge Extension Planning Proposal

<b>4.2 Mine Subsidence and Unstable Land</b>	Prevent damage to life, property and the environment on land identified as unstable or potentially subject to mine subsidence.	Correspondence from the Mines Subsidence Board dated 18 July 2017 (Appendix A) confirms that the subject land has not being undermined. The planning proposal is considered to be consistent with the intent of this direction.
<b>4.3 Flood Prone Land</b>	<p>(a) Ensure that development of flood prone land is consistent with the NSW Government's Flood Prone Land Policy and the principles of the <i>Floodplain Development Manual 2005</i>, and</p> <p>(b) Ensure that the provisions of an LEP on flood prone land is commensurate with flood hazard and includes consideration of the potential flood impacts both on and off the subject land.</p>	<p>The site is not noted as being Flood Prone, but the land contains several watercourses, and small dams that are currently on the land. Therefore, there are components of this land that are floodprone. A drainage strategy addressing local stormwater flooding will be undertaken as part of any future development application for subdivision.</p> <p>The planning proposal is consistent with the intent of this direction.</p>
<b>4.4 Planning for Bushfire Protection</b>	<p>(a) Protect life, property and the environment from bush fire hazards, by discouraging the establishment of incompatible land uses in bush fire prone areas, and</p> <p>(b) Encourage sound management of bush fire prone areas.</p>	<p>A bushfire assessment report was completed following Gateway Determination. A response from the Rural Fire Service has been obtained and is included as Appendix F.</p> <p>The RFS had no objections subject to a future requirement that any subdivision complies with Planning for <i>Bush Fire Protection 2019</i> &amp; consideration given to potential threat from unmanaged grasslands (<i>AS3959-2009 Construction of buildings in bushfire-prone areas</i>)</p> <p>The planning proposal is consistent with the intent of this direction.</p>

### Ironbark Ridge Extension Planning Proposal

<b>5. Housing, Infrastructure &amp; Urban Development</b>		
<b>5.10 Implementation of Regional Plans</b>	Ensure Planning Proposals are consistent with a Regional Plan.	<p>The planning proposal is consistent with the objectives and actions contained in the UHSRLUP 2012 and its settlement planning principles as discussed previously.</p> <p>The planning proposal is consistent with the strategies of the Hunter Regional Plan.</p>
<b>6. Local Plan Making</b>		
<b>6.1 Approval and Referral Requirements</b>	Ensure that LEP provisions encourage the efficient and appropriate assessment of development.	The planning proposal is consistent with the intent of this direction.
<b>6.2 Reserving Land for Public Purposes</b>	<p>(a) Facilitate the provision of public services and facilities by reserving land for public purposes, and</p> <p>(b) Facilitate the removal of reservations of land for public purposes where the land is no longer required for acquisition.</p>	The planning proposal is consistent with the intent of this direction.
<b>6.3 Site Specific Provisions</b>	Discourage unnecessarily restrictive site-specific planning controls.	N/A
<b>7. Metropolitan Planning</b>		
Not Applicable		

## Ironbark Ridge Extension Planning Proposal

### Section C – Environmental, Social and Economic Impact

**7. Is there any likelihood that critical habitat or threatened species, populations or ecological communities, or their habitats, will be adversely affected as a result of the proposal?**

The land is currently being used for grazing purposes.

The subject land is identified on Council's Terrestrial Biodiversity Mapping as containing a small area of 'biodiversity' as indicated in Figure 4 below however the land is not biodiversity certified land (within the meaning of Part 7AA of the Threatened Species Conservation Act 1995).



**Figure 4 - Extract from Terrestrial Biodiversity Map – Muswellbrook LEP 2009**

The owners of the land commissioned a report from *Wildthing Environmental Consultants* which was entitled *"Seven Part Test on Threatened Flora and Fauna for a proposed rezoning at Lots 101 & 103 DP 1170190 Ironbark Road Muswellbrook NSW"*, dated June 2012. (Appendix D)

It provided the following recommendations and conclusion:

- *It is recommended that the removal of hollow-bearing trees be avoided where possible. If any habitat trees require removal it is recommended that a suitably qualified ecologist be on hand to supervise their removal to reduce the impact on any fauna species which may be present.*

---

**Ironbark Ridge Extension Planning Proposal**

---

- *To further mitigate the loss of hollows it is recommended that consideration be given to the installation of nest boxes of similar habitat value within any trees retained in reserve areas prior to any clearing taking place on site. Provided these recommendations are implemented and given the small amount of habitat to be impacted the proposal is unlikely to have an adverse effect on the life cycle of this vegetation community or the habitat it provides.*
- *It is recommended that where possible species consistent with Map Unit 11 – Upper Hunter White Box – Ironbark Grassy Woodland (Peake, 2006) be utilised within landscaping of reserve and open space areas and that existing trees be retained where possible within these areas and the larger lots in the south around Ironbark Road.*
- *It is recommended that ongoing weed control be conducted within any proposed reserve areas within the scope of the development. (p36)*
- *“In conclusion, with the application of the recommendations within this report it is unlikely that the proposed rezoning will bear a significant adverse impact upon any viable local populations or individuals of the threatened species, populations or communities considered in this report” (p39)*

The document was subsequently referred to the Office of Environment and Heritage (Appendix G1). The correspondence dated 18/11/14 indicated that additional survey was required during the flowering period of two nominated flora species, and also in respect of hollow bearing trees. Such survey / assessment has been undertaken with the additional reports in Appendix D and revised proposed subdivision layout in Map 3.

The addendum report refers to further updated plans of subdivision which indicate a total reserve area of some 13.35ha. Approximately 7.67ha of Narrow Leaved Ironbark – Grey Box Grassy Woodland (an EEC), out of a total of 18.26ha, will be retained within the E3 zoned areas on site. It is expected that additional areas of the EEC will need to be set aside on site OR offset outside the site. Final determination of the lot layout of the future subdivision will be required to take such factors into account and will be able to be assessed in final detail following submission of a development application for subdivision.

**8. *Are there any other likely environmental effects as a result of the planning proposal and how are they proposed to be managed***

The subject site is not shown as being flood affected land on Muswellbrook Councils Flood Maps. Local ephemeral drainage lines will need to be considered as part of the subdivision process. A preliminary site investigation report has been undertaken by Valley Civilab as a result of the Gateway Determination. A copy of the report is Appendix M.

Proper assessment of stormwater detention and treatment, to meet current standards, will be addressed at the subdivision application stage. Subject to appropriate engineering measures in the detailed design phase, it is considered that the site is suitable for development.

The site has a very small area shown as containing bushfire prone vegetation as shown in Figure 5. A response from the RFS to the planning proposed (refer Appendix F) advised no objection subject to compliance with the requirements of *Planning for Bush Fire Protection*.



---

Ironbark Ridge Extension Planning Proposal

---



**Figure 5: Bushfire Prone Vegetation**

**9. *How has the planning proposal adequately addressed any social and economic effects?***

The proposed rezoning would provide approximately 500 lots released over multiple stages according to demand (likely to be 20-30 lots per stage). The development of the land would provide a link between existing areas of land developed for residential and large lot residential purposes in the locality.

In terms of economic benefits, as well as ongoing employment opportunities during the construction of the various stages of the subdivision and subsequent dwellings, additional population living in Muswellbrook will support the viability of existing retail and support services.

It is unlikely that there would be any adverse impacts in terms of either social or economic associated with the planning proposal.

**Section D – State and Commonwealth Interests**

**10. *Is there adequate public infrastructure for the planning proposal***

Council's Technical Staff have advised that there is capacity within the existing water and sewer infrastructure to cater for the initial stages of development of the land. Augmentation will be required.

Provision of other services including Telecommunication and Electrical supply will be clarified with further consultation with the relevant service providers.

Investigations by Better Transport Futures, to assess potential impacts of the rezoning, identified that a report previously prepared for Council in 2010 by Parsons Brinkerhoff –

### Ironbark Ridge Extension Planning Proposal

Muswellbrook Traffic Study, had included the subject land in its assessment of expected residential and commercial growth in Muswellbrook up to 2037.

The report concluded that the additional lots attributable to future development within the subject land in the context of the overall growth expected in Muswellbrook would *"have a negligible impact upon the road network upgrades identified in the Muswellbrook Traffic Study. Further, it is considered that no additional road upgrades over and above those identified in the Muswellbrook Traffic Study will be required due to this proposed development"*.

A response from the Roads and Maritime Services to the Planning Proposal (refer to Appendix H) confirmed no objection to the proposed rezoning – all future subdivision proposals will require a Traffic Impact Study. As the site will connect to Bimbadeen Drive, there may be a requirement for a future contribution to the upgrade of the intersection of Bimbadeen Drive and Maitland Street.

#### **11. What are the views of State and Commonwealth public authorities consulted in accordance with the Gateway determination?**

Agencies consulted were:

- Subsidence Advisory NSW (previously Mines Subsidence Board) refer Appendix A. The correspondence dated 18/7/17 confirmed that the subject land has been reviewed and is no longer within a Mines Subsidence District.
- NSW Trade & Investment – Resources and Energy – Refer to Appendix B. Correspondence dated 8/12/14 confirmed that the subject area is not covered by a current coal title. It was noted a Petroleum Exploration License (PEL 4) covers a broad part of the region including the subject site.
- Office of Environment & Heritage – refer to Appendix G1 The correspondence dated 18/11/14 indicated that additional survey was required during the flowering period of two nominated flora species, and also in respect of hollow bearing trees. Such survey/assessment has been undertaken with the additional reports in Appendix D and revised subdivision layout in Appendix X. The response also recommended that an Aboriginal Cultural Heritage Assessment be undertaken. Such assessment is included as Appendix G1.
- Roads & Maritime Services – refer to Appendix H– The correspondence dated 8/12/14 advised that there was no objection to the rezoning, however a traffic impact study to identify likely traffic impacts and subsequent road upgrade requirements, would have to accompany any future subdivision proposals.
- NSW Department of Primary Industry – refer to Appendix I. Correspondence dated 4/12/14 advised that the subject land is not an ad-hoc dispersed area nor currently zoned for primary production. As a result, there is no objection to the planning proposal.
- NSW Department of Primary Industry – Office of Water – refer to Appendix J1– Correspondence dated 25/11/14 advised that a detailed stormwater management plan should be prepared to address stormwater quality and quantity. Note that such plan has been completed and is also included in Appendix J2.

---

**Ironbark Ridge Extension Planning Proposal**

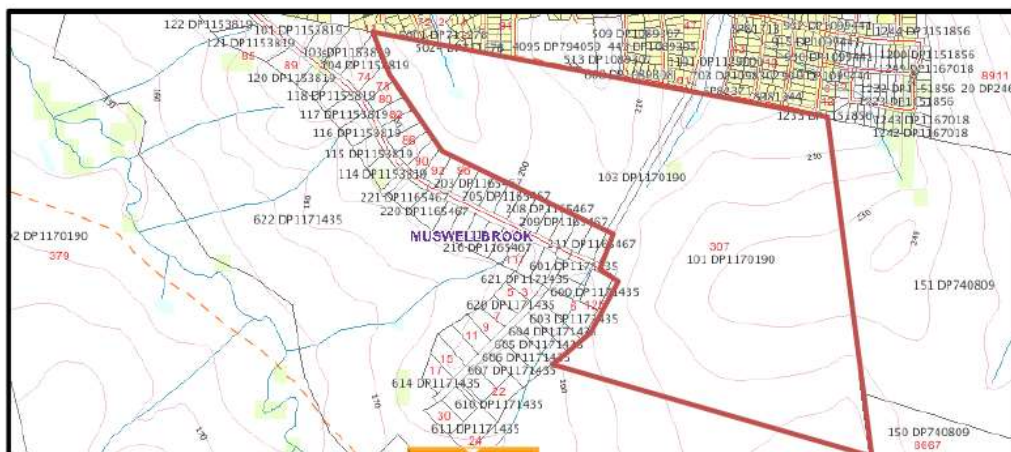
---

- NSW Aboriginal Land Council – refer to Appendix K1 – Correspondence dated 27/1/15 advising that the proposal had been referred to the Wanaruah Local Aboriginal Land Council (WLALC) for comment. Appendix K2 is the letter from the WLALC raising concerns about the content and process for the planning proposal and emphasizing that an Aboriginal Cultural Heritage Assessment will be required before any earthworks are undertaken. Note that such assessment has been completed and is Appendix X.

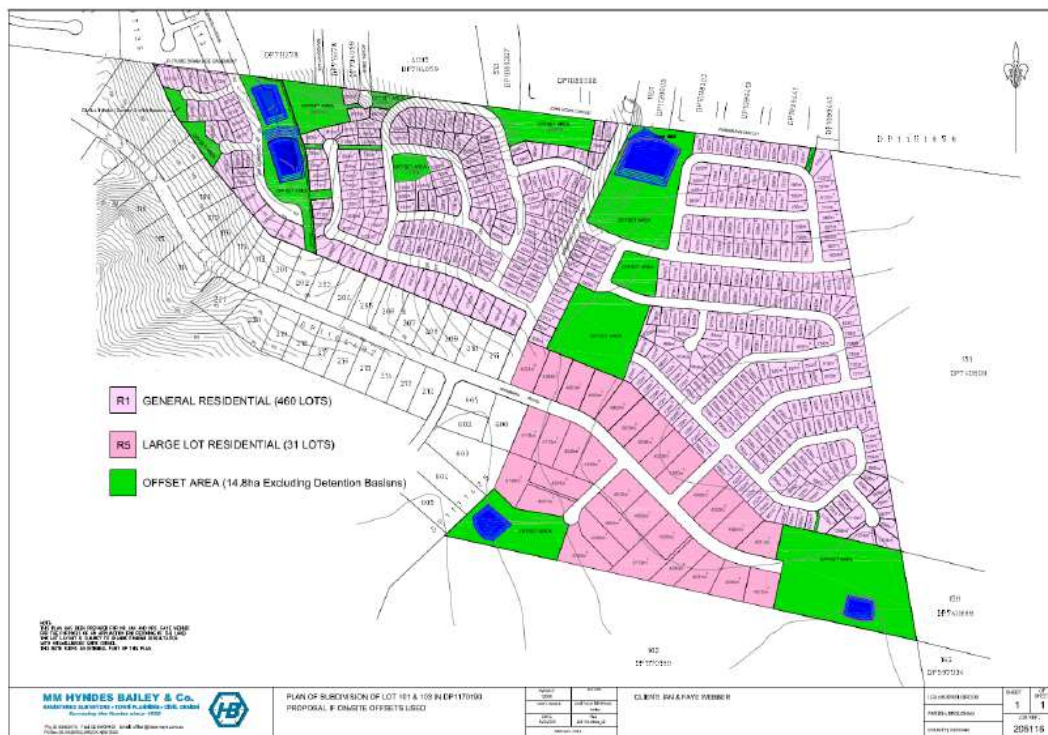
---

**Ironbark Ridge Extension Planning Proposal**

---

**Part 4 – MAPPING****Map 1 – Locality****Map 2 - Site Identification Map**

### Map 3 - Conceptual Lot Layout



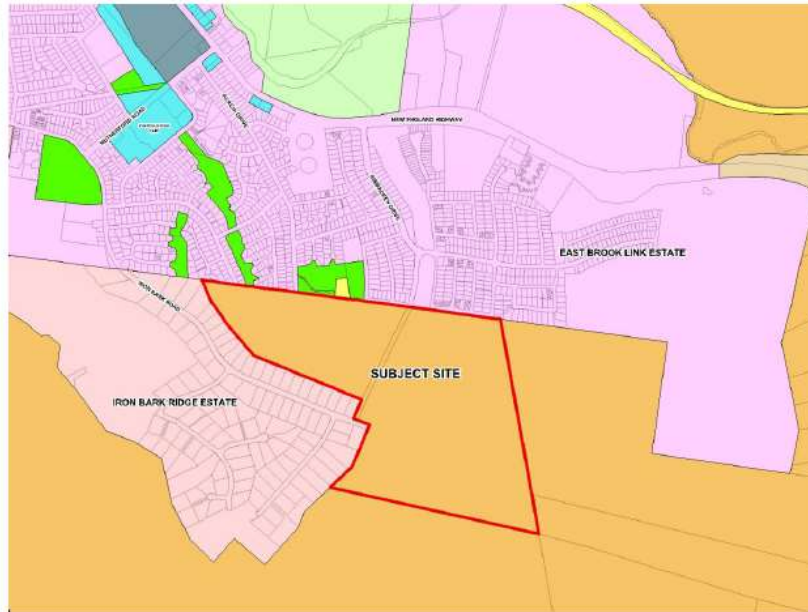
*Please note that this plan is conceptual only to provide an indicative lot layout and will be subject to changes as detailed studies occur for a future Subdivision Application.*



---

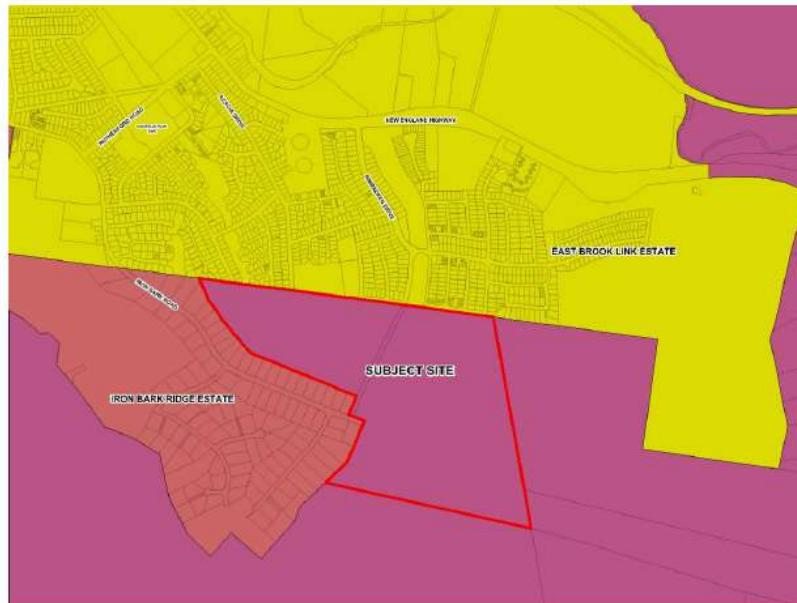
Ironbark Ridge Extension Planning Proposal

---

**Map 4 - Muswellbrook LEP 2009 - Land Zoning Map Sheet (LZN\_008 and 008A)***(a) Current Land Zoning – E3 Environmental Management**(b) Proposed Land Zoning – R1 General Residential, R5 Large Lot Residential and E3 Environmental Management*

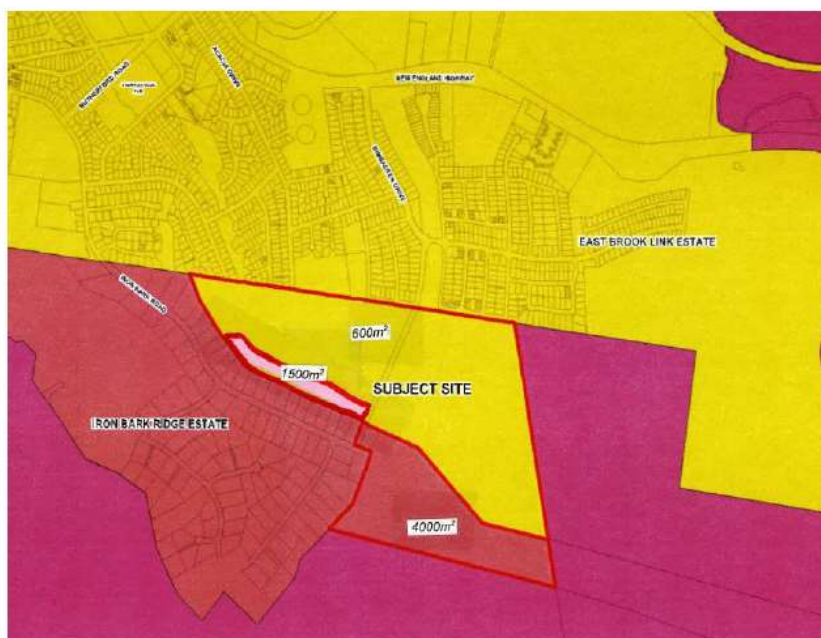
## Ironbark Ridge Extension Planning Proposal

Map 5 - MLEP 2009 - Lot Size Map Sheet (LSZ\_008 and 008A)



(a) Current Lot Size – minimum 80ha

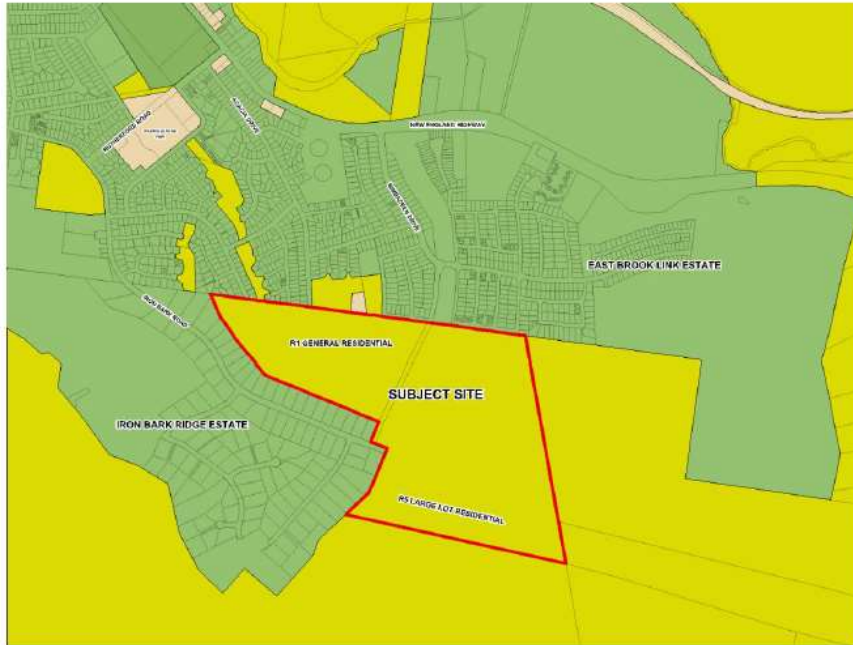
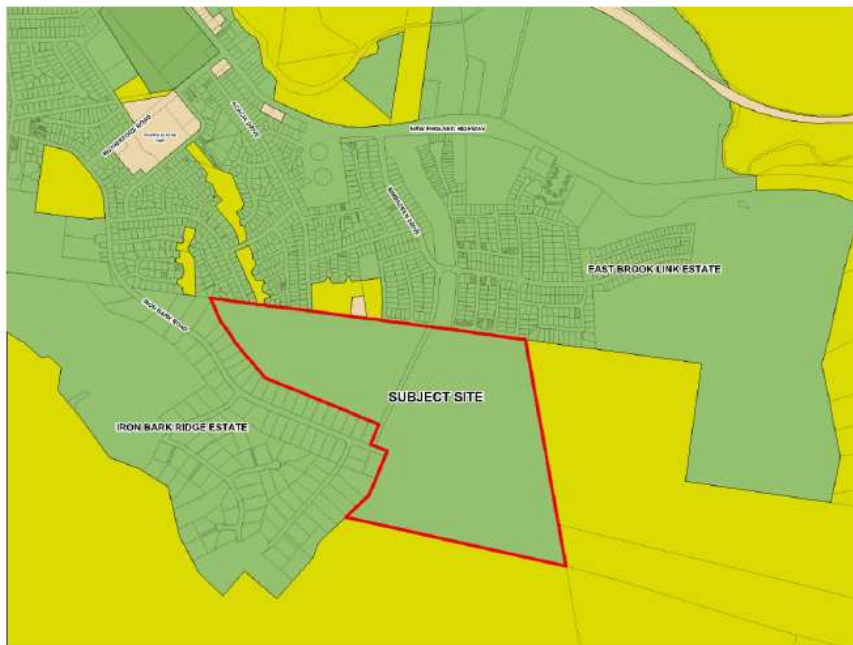
(b) Proposed Lot Size – R1 minimum 600m<sup>2</sup> and R5 minimum 4000m<sup>2</sup> & 1500m<sup>2</sup>



---

Ironbark Ridge Extension Planning Proposal

---

**Map 6 - MLEP 2009 Height of Building Map Sheet (HOB\_008 and 008A)****(a) Current Height of Building – 12m****(b) Proposed Height of Building – 8.5m**



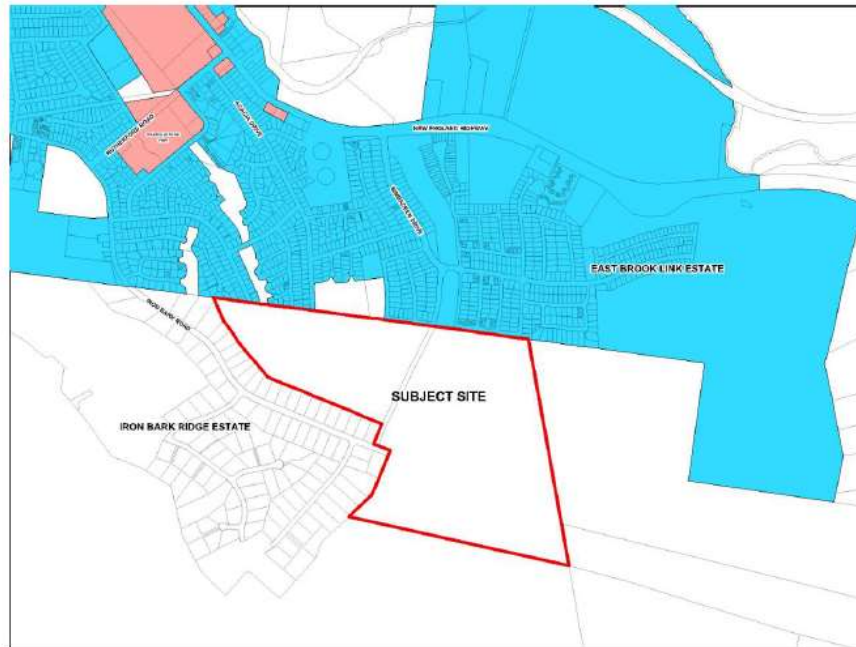
---

Ironbark Ridge Extension Planning Proposal

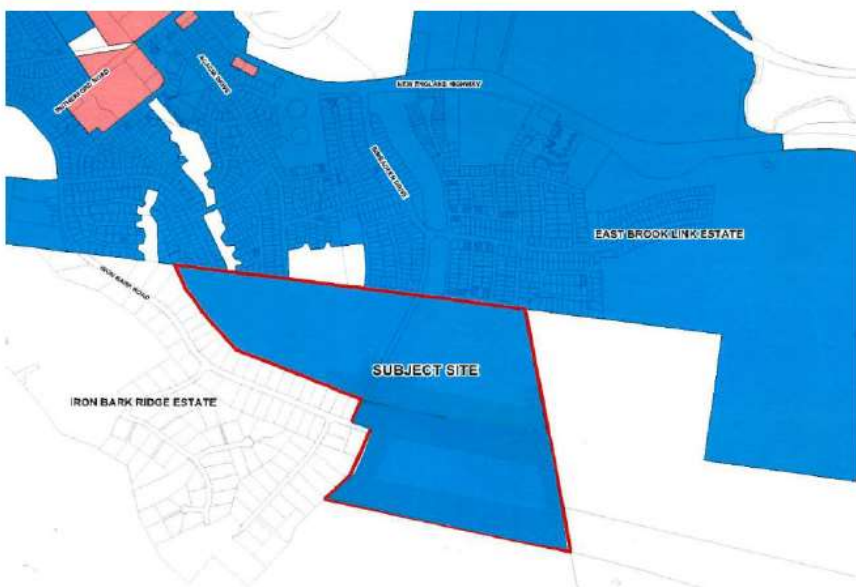
---

**Map 7 - MLEP 2009 Floor Space Ratio (FSR-008 and 008A)**

(a) *Current Floor Space Ratio – Nil*



(b) *Proposed Floor Space Ratio –0.5:1*



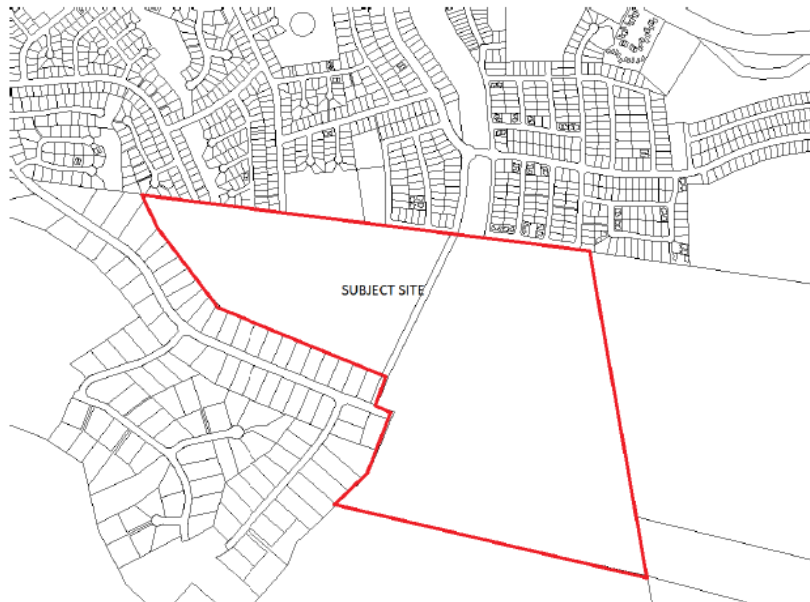
---

**Ironbark Ridge Extension Planning Proposal**

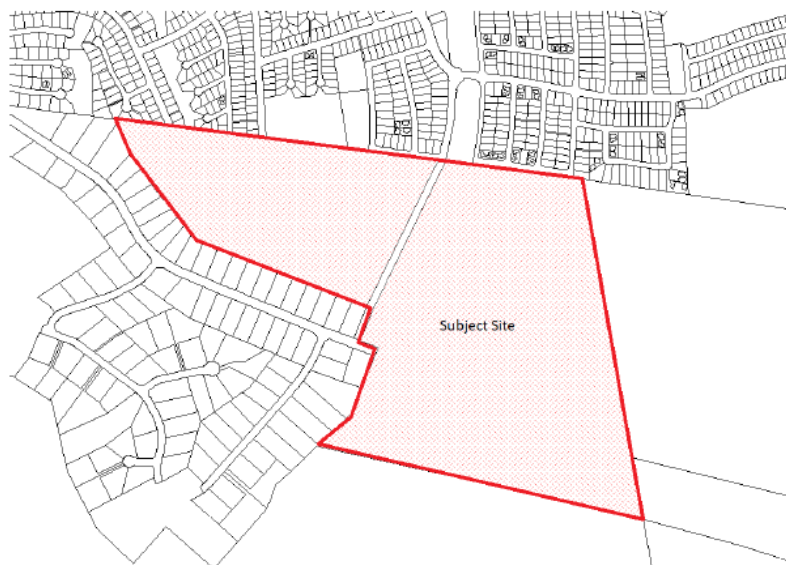
---

**Map 8 - MLEP 2009 Urban Release Area map (URA\_008 and 008A)**

*(a) Current Urban Release Area – Nil*



*(b) Proposed Urban Release Area*



### Ironbark Ridge Extension Planning Proposal

#### Part 5 – COMMUNITY CONSULTATION

The Gateway Determination dated 21/10/14 confirmed a 28-day exhibition period.

#### Part 6 – PROJECT TIMELINE

Action	Timeframe
Anticipated commencement date (date of Gateway determination)	21/10/14
Anticipated timeframe for completion of required technical information	24/04/2019
Timeframe for government agency consultation (pre exhibition)	complete
Public exhibition (commencement and completion dates)	02/11/2019 to 02/12/2019
Date of Public hearing (if required)	N/A
Consideration of submissions	30/06/2020
Timeframe for government agency consultation (post exhibition if required)	15/01/2020
Post exhibition planning proposal consideration / preparation	30/06/2020
Submission to Department to finalise LEP	30/07/2020
Date RPA will make Plan (if delegated)	
Date RPA will forward to the Department for notification (if not delegated)	

Council intends to utilise delegations under s3.36 of the EP & A Act 1979 to finalise the Planning Proposal.

## Attachment 1

---

### APPENDICIES

Appendix A:	Mines Subsidence Board - Advice
Appendix B:	NSW Resources & Energy – Coal Advice
Appendix C:	Traffic Report
Appendix D:	Seven Part Test on Threatened Flora and Fauna
Appendix E:	Section 149 Certificate
Appendix F:	Rural Fire Service – Advice
Appendix G1	Office of Environment and Heritage – Advice
Appendix G2	Aboriginal Cultural Assessment
Appendix H	Roads & Maritime Services – Advice
Appendix I	Department of Primary Industries – Advice
Appendix J1	Department of Primary Industries – Office of Water – Advice
Appendix J2	Stormwater Management Plan
Appendix K1	NSW Aboriginal Land Council - Advice
Appendix K2	Wannaruah Local Aboriginal Land Council – Advice
Appendix L	NSW Planning & Environment – Response to Muswellbrook Residential and Rural Residential Strategy 2014
Appendix M	Preliminary Site Investigation – Valley Civilab

**Ironbark Ridge Extension Planning Proposal**

---

**Attachment 2**

---

**Evaluation Criteria for the Issuing of an  
Authorisation**

## Attachment 2 – Ironbark Ridge

Evaluation criteria for the issuing of an Authorisation				
(Note – where the matter is identified as relevant and the requirement has not been met, council is to attach information to explain why the matter has not been addressed)	Council response		Department assessment	
	Y/N	Not Relevant	Agree	Disagree
Is the planning proposal consistent with the Standard Instrument Order, 2006?	Y			
Does the planning proposal contain an adequate explanation of the intent, objectives, and intended outcome of the proposed amendment?	Y			
Are appropriate maps included to identify the location of the site and the intent of the amendment?	Y			
Does the planning proposal contain detail related to proposed consultation?	Y			
Is the planning proposal compatible with an endorsed regional or sub-regional planning strategy or a local strategy endorsed by the Secretary?	Y			
Does the planning proposal adequately address any consistency with all relevant S117 Planning Directions?	Y			
Is the planning proposal consistent with all relevant State Environmental Planning Policies (SEPPs)?	Y			
Minor Mapping Error Amendments				
Does the planning proposal seek to address a minor mapping error and contain all appropriate maps that clearly identify the error and the manner in which the error will be addressed?	Y			
Heritage LEPs				
Does the planning proposal seek to add or remove a local heritage item and is it supported by a strategy/study endorsed by the Heritage Office?	N			
Does the planning proposal include another form of endorsement or support from the Heritage Office if there is no supporting strategy/study?	N			
Does the planning proposal potentially impact on an item of State Heritage Significance and if so, have the views of the Heritage Office been obtained?	N			
Reclassifications				
Is there an associated spot rezoning with the reclassification?		√		
If yes to the above, is the rezoning consistent with an endorsed Plan of management (POM) or strategy?		√		
Is the planning proposal proposed to rectify an anomaly in a classification?		√		
Will the planning proposal be consistent with an adopted POM or other strategy related to the site?		√		
Has Council confirmed whether there are any trusts, estates, interests, dedications, conditions, restrictions or covenants on the public land and included a copy of the title with the planning proposal?		√		

## Ironbark Ridge Extension Planning Proposal

Has council confirmed that there will be no change or extinguishment of interests and that the proposal does not require the Governor's approval?		√		
Has the council identified that it will exhibit the planning proposal in accordance with the Department's Practice Note regarding <i>classification and reclassification of public land through a local environmental plan and Best Practice Guideline for LEPs and Council Land</i> ?		√		
Has council acknowledged in its planning proposal that a Public Hearing will be required and agreed to hold one as part of its documentation?		√		
<b>Spot Rezoning</b>				
Will the planning proposal result in a loss of development potential for the site (ie reduced FSR or building height) that is not supported by an endorsed strategy?	N			
Is the rezoning intended to address an anomaly that has been identified following the conversion of a principal LEP into a Standard Instrument LEP format?	N			
Will the planning proposal deal with a previously deferred matter in an existing LEP and if so, does it provide enough information to explain how the issue that lead to the deferral has been addressed?	N			
If yes, does the planning proposal contain sufficient documented justification to enable the matter to proceed?		√		
Does the planning proposal create an exception to a mapped development standard?	N			
<b>Section 73A matters</b>				
Does the proposed instrument		√		
a) Correct an obvious error in the principal instrument consisting of a misdescription, the inconsistent numbering of provision, a wrong cross-reference, a spelling error, a grammatical mistake, the insertion of obviously missing words, the removal of obviously unnecessary words or a formatting error?; b) Address matter in the principal instrument that are of a consequential, transitional, machinery or other minor nature?; or c) Deal with matters that do not warrant compliance with the conditions precedent for the making of the instrument because they will not have any significant adverse impact on the environment or adjoining land? (Note – the Minister/GSC (or Delegate) will need to form an Opinion under section 73(A)(1)(c) of the Act in order for a matter in this category to proceed).				

## Notes

- Where a council responds 'yes' or can demonstrate that the matter is 'not relevant', in most cases, the planning proposal will routinely be delegated to council to finalise as a matter of local planning significance.
- Endorsed strategy means a regional strategy, or any other local strategic planning document that is endorsed by the Secretary of the Department.
- Matters that will routinely be delegated to a Council under administration are confirmed on the Department's website [www.planning.nsw.gov.au/Plans-for-Your-Area/Local-Planning-and-Zoning](http://www.planning.nsw.gov.au/Plans-for-Your-Area/Local-Planning-and-Zoning)

### Ironbark Ridge Planning Proposal Submissions Summary

Submissions	Issue	Planning Response
1	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties on Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p>
2	<b>Increase in Traffic Congestion</b> - The roads and infrastructure in the existing estate are very narrow. This causes traffic issues especially with on-street parking and multiple blind spots.	<p>The revised concept plan creates three (3) new lots off an extension of Acacia Drive, and twenty-one (21) new lots off Calgaroo Avenue. The width of these two roads are standard for residential areas and the new lots will not exceed capacity of these roads.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as local collector roads and bus routes, so are wider than standard, providing parking and travelling lanes and have full kerb and gutter.</p>
	<b>Emergency Evacuation Issues</b> - The increase in population and narrow streets with the increase traffic volumes will be a problem, for emergencies.	<p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as local collector roads. This connection will provide alternative access options in case of a disruption to the local road network improving emergency evacuation potential.</p>



Submissions	Issue	Planning Response
	<b>Crime</b> - The increase in housing will lead to an increase in the crime rate. Will these houses be housing commission rentals or private buyers?	<p>If Crime Rate is measured as a ratio to population, then it is not expected that the crime rate would increase. For example, if currently 1 person in every 1000 commits crime, this is not expected to change as a result of new housing.</p> <p>If Crime Rate is measured in absolute numbers, as population increases so would the total number of people who might be inclined to commit. There is no correlation between land zone and crime rates.</p> <p>Council has no role in determining who will occupy constructed dwellings.</p>
	<b>Noise During Building</b>	Construction activities are going to generate noise. There are time limits set for construction activity to ensure it is limited to daytime hours and restricted on weekends.
	<b>Changed amenity</b> – My house currently backs on to the open farmland	Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.
	<b>Oversupply</b> – Will result in a drop-in property prices and rental prices, and increase time taken to sell or rent properties.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83</p>

Submissions	Issue	Planning Response
		<p>existing vacant lots.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>
	<b>Rates Increase</b> - My rates will also increase due to the development.	Variations to rates occur due to increases in the costs of materials and labour (CPI) and proposals to improve services and amenities (rate variations above the Rate Peg amount). Slight changes occur if land values in one location change at a significantly different rate to the majority of lots in the Shire. This is not expected to happen as a result of the rezoning of this site or its subsequent development for housing.
3	<b>Inadequate Infrastructure</b> – we are on level 1 water restrictions; infrastructure is not available for another 500 homes.	Council's water and sewer servicing plan (Developer Servicing Plan) indicates that the land can be provided with services by augmenting existing infrastructure and constructing new infrastructure. The Servicing Plan provides a basis for the Proponent to contribute to water service upgrading.
	<b>Emergency Evacuation Issues</b> – there are not enough roads into and out of this area during an emergency.	<p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads. This connection will provide alternative access options in case of a disruption to the local road network improving emergency evacuation potential.</p>
	<b>Changed amenity</b> - This is an area with rural outlook. Keep the rural aspect of the estate with a buffer zone or make the block sizes larger.	Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.

Submissions	Issue	Planning Response
		The Proponent has revised their concept plan to provide lots sized between 1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook that haven't sold; and not many DAs for new homes approved in past year.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
4	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. If a nature strip can be left to protect the flora and fauna living in the area, as well as maintaining the separation from town, we would feel less inclined to oppose the rezoning based on environmental impacts.	The Proponent has revised their concept plan to provide lots sized between 1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Drop in property values</b> – Too many new lots combined with changed amenity will result in a drop-in property prices.	Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.

Submissions	Issue	Planning Response
	<b>Crime</b> - The increase in standard housing will lead to an increase in the crime rate.	<p>R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designated standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. There is no correlation between land zone and crime rates.</p> <p>R5 Large Lot Residential zoned land provides an opportunity for a different style of housing but is not the normal lot type as it does not meet the needs of many households due to ongoing up-keep requirements and costs and would be an inefficient and expensive development pattern regarding provision of infrastructure.</p>
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Proposal is contrary to Council documents</b> - All previous documents from the Council dictated that any extensions upon the area that is Ironbark Ridge would be of the same R5 large block sizes, if at all. Our Planning Certificate said the adjoining land was zoned E3 Environmental Management which restricted housing.	<p>The site was most likely zoned E3 Environmental Management as it contains remnant native vegetation, amongst pasture, and the site is not identified as prime agricultural land.</p> <p>On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p>
5	<b>Changed amenity</b> – Our lifestyle block will be compromised by the proposed close proximity to high density “affordable” housing on smaller blocks; R1 residential blocks will attract a lower socio-economic demographic to the local area;	R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designate standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1.

Submissions	Issue	Planning Response
		R5 Large Lot Residential zoned land provides an opportunity for a different style of housing but is not the normal lot type as it does not meet the needs of many households due to ongoing up-keep requirements and costs and would be an inefficient and expensive development pattern regarding provision of infrastructure.
	<b>Inadequate Infrastructure</b> - Where are the new people going to work, (local mines and power stations are set to close in the next 2-5 years); Where are they going to go to school / child care, health care, doctors etc. are already at max capacity;	<p>Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.</p>
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners, connecting the road to Bimbadeen will change this.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Oversupply</b> – Given the current number of blocks currently available to buy in the Muswellbrook area, do we require another 553 small town blocks.	The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted

Submissions	Issue	Planning Response
		<p>the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
6	<p><b>Oversupply</b> - There is no demand for that many new blocks in Muswellbrook. Workers do not want to live in the "DUST ZONE", they build in Scone or Singleton or further South and travel; All the undeveloped /failed estates make Muswellbrook look like a slum! Prices are decreasing, not an attractive option to live here.</p>	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<p><b>Inadequate Infrastructure</b> – struggling to supply water to existing properties; Local Schools at capacity now.</p>	<p>Council's water and sewer servicing plan (Developer Servicing Plan) indicates that the land can be provided with services by augmenting existing infrastructure and constructing new infrastructure. The Servicing Plan provides a basis for the Proponent to contribute to water service upgrading.</p>

Submissions	Issue	Planning Response
	<b>Increase in Traffic</b> - Bimbadeen Drive is a speedway now without this extra development.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
7	<b>Proposal is contrary to Council documents</b> – The Objective of E3 is to limit development	The Planning Proposal involves changing the zoning of most of the site from E3 to R1 and R5. These are both residential zones and the objective of these zones is to provide land for a variety of housing types. Subdivision of the land would be consistent with the new zone objectives.
	<b>Oversupply</b> – There are over 180 lots listed on Domain and no demand for more lots.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>



Submissions	Issue	Planning Response
	<b>Changed amenity</b> – There will be a loss of open space and trees	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>Approx 15ha of natural area will be retained in E3 Environmental Management zones providing protection for many existing trees and orchids on the site.</p>
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, open space, paths, and shops for current population.	<p>Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.</p>
8	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p>



Submissions	Issue	Planning Response
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Loss in Property Values</b> - It will no longer be semi-rural; it will devalue our home dramatically.	Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.
9	<b>Changed amenity</b> – There will be a loss of open space and trees. Keep the rural aspect of the estate with a buffer zone or make the block sizes larger; There will be increased noise during construction and from new residents.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p> <p>Construction activities are going to generate noise. There are time limits set for construction activity to ensure it is limited to daytime hours and restricted on weekends.</p>

Submissions	Issue	Planning Response
	<b>Crime</b> - The increase in R1 zoned land will lead to an increase in the crime.	R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designated standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. There is no correlation between land zone and crime rates.
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Emergency Evacuation Issues</b> – there are not enough roads into and out of this area during an emergency and inadequate parking will congest streets.	<p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads. This connection will provide alternative access options in case of a disruption to the local road network improving emergency evacuation potential.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.

Submissions	Issue	Planning Response
		<p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Drainage</b> – The site slopes and drainage impacts on neighbouring properties are a concern.	<p>The controls in Muswellbrook DCP 2009 and AUSPEC require that:</p> <ul style="list-style-type: none"> <li>• Peak runoff from the developed catchment is not to exceed the peak runoff from the existing catchments for all return periods. This includes sub-catchments within the development; and</li> <li>• Concentrated flows do not cause an increased number of erosion events in natural waterways nor any additional sedimentation.</li> </ul> <p>To achieve these requirements each new dwelling will be required to have 10,000ltr water tanks, the subdivision design will include a series of stormwater quality improvement devices that also slow the velocity of stormwater, and interallotment drainage to intercept overland stormwater flows.</p> <p>Detailed stormwater management plans are required to be prepared for submission with future subdivision applications.</p>
	<b>Proposal is contrary to Council documents</b> - All previous documents from the Council showed the E3 zoning would stay and new subdivisions would be R5 large block sizes.	On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.
10	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home dramatically.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and</p>

Submissions	Issue	Planning Response
		amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, open space, paths, and shops for current population; Where will people work.	<p>Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other

Submissions	Issue	Planning Response
		<p>land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
11	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. There will be a loss of open space and trees.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p>
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>

Submissions	Issue	Planning Response
	<b>Emergency Evacuation Issues</b> – there are not enough roads into and out of this area during an emergency and inadequate parking will congest streets.	<p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads. This connection will provide alternative access options in case of a disruption to the local road network improving emergency evacuation potential.</p>
	<b>Crime</b> - The increase in R1 zoned land will lead to an increase in the crime.	R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designated standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. There is no correlation between land zone and crime rates.
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home dramatically.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>
12	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to</p>

Submissions	Issue	Planning Response
		R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook and no demand for extra lots.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home dramatically.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, water already; Where will people work.	Council's water and sewer servicing plan (Developer Servicing Plan) indicates that the land can be provided with services by augmenting existing infrastructure and constructing new infrastructure. The Servicing Plan provides a basis for the Proponent to contribute to water service upgrading.

Submissions	Issue	Planning Response
		<p>Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.</p>
	<b>Proposal is contrary to Council documents</b> – The land is currently zoned E3, why was it zoned this way and what has changed that it should now be zoned R1.	<p>The site was most likely zoned E3 Environmental Management as it contains remnant native vegetation, amongst pasture, and the site is not identified as prime agricultural land.</p> <p>On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p>
13	<b>Proposal is contrary to Council documents</b> – The land is currently zoned E3 and is an environmental buffer for Mt Arthur.	The land is not required as a buffer for the Mt Arthur coal mine, although BHP did offer to buy the land in the past for a combination of buffer and biodiversity offset purposes.
	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable, it will result in loss of privacy and increased noise.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to</p>



Submissions	Issue	Planning Response
		R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook and no demand for extra lots.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
14	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook and no demand for extra lots.	The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply

Submissions	Issue	Planning Response
		<p>of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. This is against the Human Rights Act as I will lose privacy and be exposed to more noise. There will also be a loss of open space, trees and wildlife.	The Proponent has revised their concept plan to provide lots sized between 1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Crime</b> - The increase in R1 zoned land will lead to an increase in the crime.	R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designated standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. There is no correlation between land zone and crime rates.
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>

Submissions	Issue	Planning Response
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home and others in the area.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>
	<b>Emergency Evacuation Issues</b> – there are not enough roads into and out of this area during an emergency and inadequate parking will congest streets.	<p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads. This connection will provide alternative access options in case of a disruption to the local road network improving emergency evacuation potential.</p>
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, health care and water already; Where will people work.	<p>Council's water and sewer servicing plan (Developer Servicing Plan) indicates that the land can be provided with services by augmenting existing infrastructure and constructing new infrastructure. The Servicing Plan provides a basis for the Proponent to contribute to water service upgrading.</p> <p>Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an</p>

Submissions	Issue	Planning Response
		opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.
	<b>Drainage</b> – The site slopes and drainage impacts on neighbouring properties are a concern.	<p>The controls in Muswellbrook DCP 2009 and AUSPEC require that:</p> <ul style="list-style-type: none"> <li>• Peak runoff from the developed catchment is not to exceed the peak runoff from the existing catchments for all return periods. This includes sub-catchments within the development; and</li> <li>• Concentrated flows do not cause an increased number of erosion events in natural waterways nor any additional sedimentation.</li> </ul> <p>To achieve these requirements each new dwelling will be required to have 10,000ltr water tanks, the subdivision design will include a series of stormwater quality improvement devices that also slow the velocity of stormwater, and interallotment drainage to intercept overland stormwater flows.</p> <p>Detailed stormwater management plans are required to be prepared for submission with future subdivision applications.</p>
15	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	The Proponent has revised their concept plan to provide lots sized between 1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home and others in the area.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>

Submissions	Issue	Planning Response
	<b>Why wasn't information about rezoning available since 2014?</b>	On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe. This document has been available to the public since 2014 but is unlikely to be in common use except if a specific query was submitted to Council for information on future residential growth plans.
	<b>Crime</b> - The increase in R1 zoned land will lead to an increase in the crime.	R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designated standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. There is no correlation between land zone and crime rates.
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook and no demand for extra lots. Too many new lots combined with changed amenity will result in a drop in property prices.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>

Submissions	Issue	Planning Response
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners, connecting the road to Bimbadeen Dr will change this.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
16	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space. We will have a new road built on our side boundary.	The Proponent has revised their concept plan to provide lots sized between 1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>

Submissions	Issue	Planning Response
17	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners. If it goes ahead connections should be made to Acacia Dr, Mahogany Ave and Calgaroo Ave to allow traffic to disperse and not impact on Ironbark Road.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook and no demand from workers.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home and others in the area.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the</p>



Submissions	Issue	Planning Response
		employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.
	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space. Keep the rural aspect of the estate with a buffer zone or make the block sizes larger.	The Proponent has revised their concept plan to provide lots sized between 1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, health care and water already; Where will people work.	<p>Council's water and sewer servicing plan (Developer Servicing Plan) indicates that the land can be provided with services by augmenting existing infrastructure and constructing new infrastructure. The Servicing Plan provides a basis for the Proponent to contribute to water service upgrading.</p> <p>Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.</p>
18	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.

Submissions	Issue	Planning Response
		The Proponent has revised their concept plan to provide lots sized between 1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, health care and water already; Where will people work.	<p>Council's water and sewer servicing plan (Developer Servicing Plan) indicates that the land can be provided with services by augmenting existing infrastructure and constructing new infrastructure. The Servicing Plan provides a basis for the Proponent to contribute to water service upgrading.</p> <p>Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies</p>

Submissions	Issue	Planning Response
		the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners. If it goes ahead connections should be made to Acacia Dr, Mahogany Ave and Calgaroo Ave to allow traffic to disperse and not impact on Ironbark Road.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>The Muswellbrook Traffic Study was completed for Council in 2010 to identify strategic road network upgrades required to accommodate predicted residential and commercial growth in Muswellbrook to the year 2037. The Traffic Study identified that Bimbadeen Road be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
19	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	The Proponent has revised their concept plan to provide lots sized between 1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>The Muswellbrook Traffic Study was completed for Council in 2010 to identify strategic road network upgrades required to accommodate predicted residential and commercial growth in Muswellbrook to the year 2037. The Traffic Study identified that Bimbadeen Road be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full</p>

Submissions	Issue	Planning Response
		<p>kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, health care and water already; Where will people work.	<p>Council's water and sewer servicing plan (Developer Servicing Plan) indicates that the land can be provided with services by augmenting existing infrastructure and constructing new infrastructure. The Servicing Plan provides a basis for the Proponent to contribute to water service upgrading.</p> <p>Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and</p>

Submissions	Issue	Planning Response
		community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.
20	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	The Proponent has revised their concept plan to provide lots sized between 1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Proposal is contrary to Council documents</b> - The estate is surrounded by E3 Environmental zoned land. We were informed by both the real estate and council this was to provide a buffer zone to distinguish R5 and existing R1 residential that is on the other side of the ridge line.	<p>The site was most likely zoned E3 Environmental Management as it contains remnant native vegetation, amongst pasture, and the site is not identified as prime agricultural land.</p> <p>On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p>
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home and others in the area.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>
	<b>The Environment investigation is not recent</b> - We believe the documents and studies carried out being out of date. We are disappointed that the discovery of the rare orchid was not properly investigated and the land left to be grazed on.	The site has a high degree of modification to native vegetation due to clearing, grazing and weed incursion. A Seven Part Test on Threatened Flora and Fauna by Wildthing Environmental Consultants was commissioned for the subject land. The report concluded that "with the application of the

Submissions	Issue	Planning Response
		<p>recommendations within this report it is unlikely that the proposed rezoning will be a significant adverse impact upon any viable local populations or individuals of the threatened species, populations or individuals of the threatened species, populations or communities considered in this report.”</p> <p>The updated concept plan of subdivision indicates a total offset area of 14.8ha on site. Approximately 7.67ha of Narrow Leaved Ironbark – Grey Box Grassy Woodland (an EEC), out of a total of 18.26ha, will be retained within the E3 zoned areas on site.</p> <p>An area around the single specimen of the Diuris tricolor (Pine Donkey Orchid) found on site will also be retained in an area zoned E3.</p> <p>It is expected that additional areas of the EEC may need to be set aside on site OR offset outside the site. Final determination of the lot layout of the future subdivision will be required to take such factors into account.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Lack of Notification</b> - Not all residents received letters. It took a campaign of community members to get council to send letters to landowners directly adjacent to the	<p>There were some issues with the notification process and availability of the documents on Council's website at times. For this reason, the exhibition was extended. The Planning Proposal was publicly exhibited from the</p>



Submissions	Issue	Planning Response
	development. Once all letters were received council issued a new closing date for submissions <a href="#">from December 2nd to December 16th 2019</a> . The planning proposal was removed from the council website on the 2nd December.	period 01/11/2019 to 16/12/2019. Public notice was given in the Muswellbrook Chronicle, and written letters were sent to 67 adjoining and nearby property owners. The exhibition material was available on the website and at Council's Administration building. The notification letter provided contact details for staff and a number of residents asked for either a printed copy of the Planning Proposal or an electronic copy via email.
	<b>Different conceptual plans.</b> Which one is relevant?	The most recent concept plan was prepared by MM Hyndes Bailey, is dated 06.05.2020 and marked as File 20511 offset_V2. The subdivision design process will likely lead to further variations to the layout.
	<b>Council is using outdated documents</b> - The LEP is to be updated every 5 years which has not happened. The Settlement Strategy is also outdated. We understand council has budget issues, but this is unacceptable to use these documents as the change in Muswellbrook prospects and employment opportunities have changed. The MRRS is outdated as well.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision.</p> <p>The fact that growth rates slowed between 2016 and 2018 is not critical as this site was identified as the most suitable of a number of urban fringe locations for urban growth. Slower growth rates may affect the Proponent's timing for bring land to the market. Housing approvals for the past 6 years have been highly variable, but approvals in 2019 and 2020 are much closer to the number of approvals issued in 2014.</p>
	<b>Proposal is contrary to Council documents</b> – The land is currently zoned E3, why was it zoned this way and what has changed that it should now be zoned R1.	<p>The site was most likely zoned E3 Environmental Management as it contains remnant native vegetation, amongst pasture, and the site is not identified as prime agricultural land.</p> <p>On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p>
21	<b>Proposal is contrary to Council documents</b> – The land is currently zoned E3, why was it zoned this way and what has changed that it should now be zoned R1.	<p>The site was most likely zoned E3 Environmental Management as it contains remnant native vegetation, amongst pasture, and the site is not identified as prime agricultural land.</p> <p>On 10 March 2014 Council adopted the Muswellbrook Residential and Rural</p>



Submissions	Issue	Planning Response
		Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home and others in the area.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Affordable housing not good for Town</b> - Affordable housing goes against the town's plan to grow and bring young families to this area. As small R1 blocks will typically secure investors,	R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designate standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned

Submissions	Issue	Planning Response
	and these being predictably occupied by out of town workers who don't put their money back into our local economy.	R1. More than 50% of households own or are buying their home in the Shire.
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, health care, water, power, sewerage and internet already; Where will people work. Additional recreational facilities including parks and pathways will need to be included.	<p>Council's water and sewer servicing plan (Developer Servicing Plan) indicates that the land can be provided with services by augmenting existing infrastructure and constructing new infrastructure. The Servicing Plan provides a basis for the Proponent to contribute to water service upgrading.</p> <p>Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.</p>
	<b>How will Council ensure we don't get another Eastbrook Links</b> - a half-finished eye saw as people enter our town, unfinished blocks, dams with fences falling down, no up keep and maintenance, concrete and rubble left over years after the developers have left, few paths, no nature strips and only one very small children's play area.	<p>Council's requirements for new subdivisions are set-out in Muswellbrook DCP 2009 and AUSPEC. These will be applied at the subdivision stage by the Proponent when preparing the subdivision design, and as conditions approval.</p> <p>Council has no control over the financial health of developers but is able to require developers to maintain grass adjoining existing residences to reduce fire and vermin risks.</p>
	<b>Lack of engagement</b> - This application has been in since 2012 and the residents have been given less than a month to respond, most of who have never received the letter and have only found out by word of mouth.	<p>The Planning Proposal wasn't in a finalised state to enable exhibition until November 2019.</p> <p>There were some issues with the notification process and availability of the documents on Council's website at times. For this reason, the exhibition was extended. The Planning Proposal was publicly exhibited from the period 01/11/2019 to 16/12/2019. Public notice was given in the Muswellbrook Chronicle, and written letters were sent to 67 adjoining and</p>

Submissions	Issue	Planning Response
		nearby property owners. The exhibition material was available on the website and at Council's Administration building. The notification letter provided contact details for staff and a number of residents asked for either a printed copy of the Planning Proposal or an electronic copy via email.
	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space; The blocks that directly back onto existing Ironbark Road properties should be zoned as R5 with gradual downsizing of blocks towards R1 as the land is further away from the existing estate; Nature strips should be included to separate the current estate from the new estate; There is a need to leave the Ironbark trees standing.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p>
	<b>Increase in Traffic</b> - they should not use Ironbark Road as the access to these new subdivisions, as the road is already a very busy street from the intersection with Rutherford road and Calgaroo Avenue. The increased in traffic could put children at risk of being hit trying to access Highbrook Park.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>A meeting required</b> - between the owners of the land and the affected parties to voice concerns, as this may allow both parties to find some common ground on this issue.	The purpose of the public notification period is to provide people an opportunity to comment on the proposal. Due to the Privacy and Personal Information Protection Act, Council redacts submissions, so the Proponent would be unable to identify who made a submission. The identity of the Proponent is available if someone did wish to make contact.

Submissions	Issue	Planning Response
22	<b>Council is using outdated documents</b> - The population growth of Muswellbrook has been approximately 1.9% over the past 7 years. This growth figure is much lower than that forecast in the strategic plan conducted in 2014 that has been used as supporting information to the rezoning application.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision.</p> <p>The fact that growth rates slowed between 2016 and 2018 is not critical as this site was identified as the most suitable of a number of urban fringe locations for urban growth. Slower growth rates may affect the Proponent's timing for bring land to the market. Housing approvals for the past 6 years have been highly variable, but approvals in 2019 and 2020 are much closer to the number of approvals issued in 2014.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space; The blocks that directly back onto existing Ironbark Road properties should be zoned as R5 with gradual downsizing of blocks towards R1 and buffers between the R1 and R5.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between</p>

Submissions	Issue	Planning Response
		1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Increase in Traffic</b> - they should not use Ironbark Road as the access to these new subdivisions.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>The Muswellbrook Traffic Study was completed for Council in 2010 to identify strategic road network upgrades required to accommodate predicted residential and commercial growth in Muswellbrook to the year 2037. The Traffic Study identified that Bimbadeen Road be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Affordable housing not good for Town</b> - Small R1 blocks will typically secure investors, and these being predictably occupied by out of town workers who don't put their money back into our local economy. The focus of council should be to encourage permanent residents of the town not people who drive in and drive out.	R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designated standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. More than 50% of households in the Shire own or are purchasing their home.
23	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The</p>

Submissions	Issue	Planning Response
		Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting home owners; Traffic lights would need to be installed on the New England Highway and Bimbadeen Drive intersection.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>The Muswellbrook Traffic Study was completed for Council in 2010 to identify strategic road network upgrades required to accommodate predicted residential and commercial growth in Muswellbrook to the year 2037. The Traffic Study identified that Bimbadeen Road be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter. New signals are currently proposed for New England Highway and Bimbadeen Drive intersection.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home and others in the area.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, open space, paths, and shops for current population; Poor road and intersection infrastructure in general area; Where will people work.	<p>Council's water and sewer servicing plan (Developer Servicing Plan) indicates that the land can be provided with services by augmenting existing infrastructure and constructing new infrastructure. The Servicing Plan provides a basis for the Proponent to contribute to water service upgrading.</p> <p>Council has a goal of diversifying the employment available in the Shire and</p>

Submissions	Issue	Planning Response
		<p>improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
24	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space; Extra dwellings and traffic will create noise impacts.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p>



Submissions	Issue	Planning Response
		The Proponent has revised their concept plan to provide lots sized between 1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Compliance with Council Construction Standards</b> - Unlike Eastbrook links developers what commitment does council have that the proposed developer will actual comply with these requirements; Traffic Study claims different lot numbers to other docs – what is corrected number?	<p>The Muswellbrook Traffic Study was completed for Council in 2010 to identify strategic road network upgrades required to accommodate predicted residential and commercial growth in Muswellbrook to the year 2037. The Traffic assessment submitted with the rezoning request assessed the traffic movements associated with the potential new lots and found the movements could be accommodated in the road network upgrades identified in the Muswellbrook Traffic Study.</p> <p>The proposal has been to the RMS on a number of occasions and they have not objected to the proposal.</p> <p>A detailed traffic study will be required as part of future subdivision applications. Street widths, geometry and intersections will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Lack of engagement</b> - This application has been in since 2012 and the residents have only found out now; we were informed that the land was zoned E3 when we purchased adjoining land. How and why is council still not in a position to finalise the plan. Why weren't local residents made aware of the proposed plan some 4.5 years since the initial Gateway determination.	<p>On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe. This document has outlined Council's Policy since that date.</p> <p>The Planning Proposal wasn't in a finalised state to enable exhibition until November 2019.</p>
	<b>Crime</b> - The increase in R1 zoned land will lead to an increase in the crime; Covenants restrict the style of fencing so we cannot stop criminals.	R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designated standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. There is no correlation between land zone and crime rates.
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting	The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.

Submissions	Issue	Planning Response
	homeowners; RMS requires Traffic lights to be installed on the New England Highway and Bimbadeen Drive intersection. How can Council consider proposal while these lights are still not constructed?	<p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter. New traffic signals are proposed for the New England Highway and Bimbadeen Drive intersection.</p> <p>The Muswellbrook Traffic Study was completed for Council in 2010 to identify strategic road network upgrades required to accommodate predicted residential and commercial growth in Muswellbrook to the year 2037. The Traffic assessment submitted with the rezoning request assessed the traffic movements associated with the potential new lots and found the movements could be accommodated in the road network upgrades identified in the Muswellbrook Traffic Study.</p> <p>The proposal has been to the RMS on a number of occasions and they have not objected to the proposal.</p> <p>A detailed traffic study will be required as part of future subdivision applications. Street widths, geometry and intersections will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Proposal is contrary to Council documents</b> – The Objective of E3 is to limit development.	<p>The site was most likely zoned E3 Environmental Management as it contains remnant native vegetation, amongst pasture, and the site is not identified as prime agricultural land.</p> <p>On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook. How about council sells the existing available land identified in multiple current estates that have been stalled with no action.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing</p>

Submissions	Issue	Planning Response
		<p>urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, open space, paths, and shops for current population; Poor road and intersection infrastructure in general area; Where will people work.	<p>Council's water and sewer servicing plan (Developer Servicing Plan) indicates that the land can be provided with services by augmenting existing infrastructure and constructing new infrastructure. The Servicing Plan provides a basis for the Proponent to contribute to water service upgrading. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.</p>
	<b>Contamination</b> – Planning Proposal say the previous use of land has been limited to light agricultural practices only and it is unlikely to contain contaminants, but findings indicate contamination. If contamination has been identified, please identify to the existing residence what this contamination is.	The preliminary site contamination investigation concluded that herbicide and pesticide use associated with farming activities represent the only likely source of contamination on the site. The risk of contamination is very low as infrastructure, such as yards and dips, have not been present on the site since at least 1958 (first available aerial photo).
	<b>Habitat loss</b> – The development will impact on grey box grassy woodland (an EEC) and native orchids. No	The site has a high degree of modification to native vegetation due to clearing, grazing and weed incursion. A Seven Part Test on Threatened

Submissions	Issue	Planning Response
	development should be approved.	<p>Flora and Fauna by Wildthing Environmental Consultants was commissioned for the subject land. The report concluded that “with the application of the recommendations within this report it is unlikely that the proposed rezoning will be a significant adverse impact upon any viable local populations or individuals of the threatened species, populations or individuals of the threatened species, populations or communities considered in this report.”</p> <p>The updated concept plan of subdivision indicates a total offset area of 14.8ha on site. Approximately 7.67ha of Narrow Leaved Ironbark – Grey Box Grassy Woodland (an EEC), out of a total of 18.26ha, will be retained within the E3 zoned areas on site.</p> <p>An area around the single specimen of the Diuris tricolor (Pine Donkey Orchid) found on site will also be retained in an area zoned E3.</p> <p>It is expected that additional areas of the EEC may need to be set aside on site OR offset outside the site. Final determination of the lot layout of the future subdivision will be required to take such factors into account.</p>
25	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this ‘transition’ row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p>
	<b>Crime</b> - The increase in R1 zoned land will lead to an increase in the crime.	<p>R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designate standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. There is no correlation between land zone and crime rates.</p>

Submissions	Issue	Planning Response
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, and health services for current population; Our Main Street is empty and existing businesses are struggling, many closing their doors after only a few months of opening.	<p>Council's water and sewer servicing plan (Developer Servicing Plan) indicates that the land can be provided with services by augmenting existing infrastructure and constructing new infrastructure. The Servicing Plan provides a basis for the Proponent to contribute to water service upgrading.</p> <p>Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.</p>

Submissions	Issue	Planning Response
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home and others in the area.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>
26	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p>

Submissions	Issue	Planning Response
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home and others in the area.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>
27	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p>



Submissions	Issue	Planning Response
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Crime</b> - The increase in R1 zoned land will lead to an increase in the crime.	<p>R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designate standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. There is no correlation between land zone and crime rates.</p> <p>R5 Large Lot Residential zoned land provides an opportunity for a different style of housing but is not the normal lot type as it does not meet the needs</p>

Submissions	Issue	Planning Response
		of many households due to ongoing up-keep requirements and costs, and would be an inefficient and expensive development pattern regarding provision of infrastructure.
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home and others in the area.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>
28	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting home owners. My biggest fear is that we will end up with speed humps outside our property (as we are on the approach to the first hill in the estate) if the amount of traffic increases. If the rezoning goes ahead and there is over 500 houses constructed, add 2 cars per household to the already ~100 households in Ironbark Ridge and you end up with 1,200 plus cars which will use Ironbark Road daily.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Inadequate Infrastructure</b> – inadequate schools, child care, open space, paths, and shops for current population; Poor road and intersection infrastructure in general area; it's not uncommon to get caught in a line of traffic at the roundabout, going into Muswellbrook Fair, this will only get worse if the rezoning is allowed to go ahead. It can be a struggle at times to get a car park, add another 1,000 cars into the mix and it will be impossible.	<p>Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p> <p>Muswellbrook Contributions Plan 2001 applies to the site. The Plan requires a per lot contribution toward urban roads, drainage, open space and community facilities. The draft Local Strategic Planning Statement identifies</p>

Submissions	Issue	Planning Response
		<p>the need to update this Plan in the short-term. This would provide an opportunity to apportion the cost of upgrading/signalising the Bimbadeen Road/Maitland Street intersection over several landowners who propose development in the area and would benefit from the upgrade.</p> <p>The Muswellbrook Traffic Study was completed for Council in 2010 to identify strategic road network upgrades required to accommodate predicted residential and commercial growth in Muswellbrook to the year 2037. The Traffic assessment submitted with the rezoning request assessed the traffic movements associated with the potential new lots and found the movements could be accommodated in the road network upgrades identified in the Muswellbrook Traffic Study.</p> <p>The proposal has been to the RMS on a number of occasions and they have not objected to the proposal.</p> <p>A detailed traffic study will be required as part of future subdivision applications. Street widths, geometry and intersections will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Loss in Property Values</b> - It will no longer be semi-rural, it will devalue our home and others in the area.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth.</p> <p>Many factors go into the value of properties, the most substantial is the availability of employment. Council has a goal of diversifying the employment available in the Shire and improving the amenity and liveability of Muswellbrook and Denman through new walking trails, new services and amenities and supporting retail development. An increase in households will make current and new make the retail offering more viable.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing</p>

Submissions	Issue	Planning Response
		<p>urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
29	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between</p>

Submissions	Issue	Planning Response
		1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners; Traffic lights would need to be installed on the New England Highway and Bimbadeen Drive intersection.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
30	<b>Council is using outdated documents</b> - The population growth of Muswellbrook has been approximately 1.9% over the past 7 years. This growth figure is much lower than that forecast in the strategic plan conducted in 2014 that has been used as supporting information to the rezoning application; The Proposal fails to provide justification or show a genuine need for this development. It fails to consider the major changes and challenges facing the town	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a <u>minimum</u> of ten years supply of land zoned and ready for subdivision.</p> <p>The fact that growth rates slowed between 2016 and 2018 is not critical as this site was identified as the most suitable of a number of urban fringe locations for urban growth. Slower growth rates may affect the Proponent's timing for bring land to the market. Housing approvals for the past 6 years have been highly variable, but approvals in 2019 and 2020 are much closer to the number of approvals issued in 2014.</p>
	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between</p>

Submissions	Issue	Planning Response
		1500m <sup>2</sup> and 2000m <sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m <sup>2</sup> .
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook; This proposal will greatly impact the existing Ironbark Ridge Estate house values, to the point where they become unattractive to future residents, and to the detriment of existing homeowners; R1 Small Blocks are usually unattractive to buyers looking to owner-occupy. They are the preferred block for investors and renters.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p> <p>R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designated standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. More than 50% of households own or are buying their home in the Shire.</p>
	<b>Environmental Impact</b> - R1 zoning will result in significant habitat destruction and a resultant decrease in flora and fauna to everyone's loss.	The site has a high degree of modification to native vegetation due to clearing, grazing and weed incursion. A Seven Part Test on Threatened Flora and Fauna by Wildthing Environmental Consultants was commissioned for the subject land. The report concluded that "with the application of the recommendations within this report it is unlikely that the proposed rezoning will be a significant adverse impact upon any viable local populations or individuals of the threatened species, populations or individuals of the threatened species, populations or communities considered in this report."

Submissions	Issue	Planning Response
		<p>The updated concept plan of subdivision indicates a total offset area of 14.8ha on site. Approximately 7.67ha of Narrow Leaved Ironbark – Grey Box Grassy Woodland (an EEC), out of a total of 18.26ha, will be retained within the E3 zoned areas on site.</p> <p>An area around the single specimen of the Diuris tricolor (Pine Donkey Orchid) found on site will also be retained in an area zoned E3.</p> <p>It is expected that additional areas of the EEC may need to be set aside on site OR offset outside the site. Final determination of the lot layout of the future subdivision will be required to take such factors into account.</p>
31	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p>
	<b>Proposal is contrary to Council documents</b> - The estate is surrounded by E3 Environmental zoned land. We weren't informed that R1 residential might be possible on the E3 land.	On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook; Where will demand for more housing come from with power stations and mines closing.	The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing



Submissions	Issue	Planning Response
		<p>urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>Inadequate Infrastructure</b> – There are poor road and intersection infrastructure in general area; Traffic Study doesn't address this adequately.	<p>The Muswellbrook Traffic Study was completed for Council in 2010 to identify strategic road network upgrades required to accommodate predicted residential and commercial growth in Muswellbrook to the year 2037. The Traffic assessment submitted with the rezoning request assessed the traffic movements associated with the potential new lots and found the movements could be accommodated in the road network upgrades identified in the Muswellbrook Traffic Study.</p> <p>The proposal has been to the RMS on a number of occasions and they have not objected to the proposal.</p> <p>A detailed traffic study will be required as part of future subdivision applications. Street widths, geometry and intersections will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
32	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p>

Submissions	Issue	Planning Response
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
33	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose rural views, trees, wildlife and open space.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p>
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>

Submissions	Issue	Planning Response
	<b>Oversupply</b> – There are already many blocks for sale in the Muswellbrook; Where will demand for more housing come from.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>More R1 zoned land not good for Town</b> - The R1 lots proposed will add very little and take so much from the community. These blocks are aimed at investors and not family living, ultimately it will lead to a fly in fly out rental market where families do not stay in town and spend their money.	<p>R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designated standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. More than 50% of households own or are buying their home in the Shire.</p>
34	<b>Changed amenity</b> - This is a large lot residential area with rural outlook. Having small lots adjacent to large lots is unacceptable. We will lose privacy.	<p>Change to the visual setting, and consequently the amenity of existing properties, will occur on a gradual basis, most likely over a period of 20 years. Change on the urban fringe is inevitable in any location experiencing population growth. But there are also benefits for the wider community in having enough housing to meet demand, and for Muswellbrook to have an increase in households to make the retail centres more viable.</p> <p>The Proponent has revised their concept plan to provide lots sized between 1500m<sup>2</sup> and 2000m<sup>2</sup> adjacent to existing properties in Ironbark Rd. The Planning Proposal has been amended to zone this 'transition' row of lots to R5 Large Lot Residential, and the minimum lot size map requires lots to have a minimum area of 1500m<sup>2</sup>.</p>

Submissions	Issue	Planning Response
	<b>Increase in Traffic</b> - Currently the roads are very family friendly with the only vehicles coming into the area being people who live in Ironbark Ridge Estate or are visiting homeowners.	<p>The rezoning and subsequent development of the land for housing will increase traffic volumes, however streets will be posted with 50km/hr speed limits typical of urban areas.</p> <p>Bimbadeen Road will be extended to meet Ironbark Road. Both Ironbark and Bimbadeen Roads have been designed and constructed as a collector roads and bus routes, so are wider and have full kerb and gutter.</p> <p>The final street network will be subject to detailed design work as part of future subdivision applications. Street widths and geometry will need to comply with engineering standards in Muswellbrook DCP 2009 and AUSPEC.</p>
	<b>Oversupply</b> – There are already a large number of blocks for sale in the Muswellbrook; Where will demand for more housing come from.	<p>The conversion of rural land to urban land takes a considerable time, given the studies and approvals required. The target set by the State Government is that each Council area should aim to have a minimum of ten years supply of land zoned and ready for subdivision. On 10 March 2014 Council adopted the Muswellbrook Residential and Rural Residential Strategy. This strategy identifies that the land is suitable for residential and large lot residential development, that development would be a logical extension of existing urban areas and that rezoning the land was a high priority compared to other land on the urban fringe.</p> <p>Zoning the land does not mean that 500 new lots will be available for purchase immediately. Developments are usually staged by Proponents, so they have a cashflow that covers costs and still enables a rate of return on their investment.</p> <p>There have been no new subdivisions for residential purposes in the Shire in the past 3 years, however approvals have been granted for dwellings on 83 existing vacant lots in that time.</p>
	<b>More R1 zoned land not good for Town</b> - The R1 lots proposed will add very little and take so much from the community. These blocks are aimed at investors and not family living.	<p>R1 General Residential is the only residential zone currently utilised in Muswellbrook LEP 2009 to designated standard residential areas. Approximately 90% of the population of the Shire lives on a lot that is zoned R1. More than 50% of households own or are buying their home in the Shire.</p>

**MM HYNDES BAILEY & Co.**  
**REGISTERED SURVEYORS - TOWN PLANNING - CIVIL DESIGN**  
*Surveying the Hunter since 1920*



*Incorporating KF Murphy & Assoc.*

Upper Hunter

P.O. Box 26

Muswellbrook NSW 2333

Lower Hunter

9/12 Alma Road

New Lambton NSW 2305

ph: (02) 6543 2475

[office@hbsurveys.com.au](mailto:office@hbsurveys.com.au)

Reference: 205116

2 March 2020

The General Manager  
Muswellbrook Shire Council  
Attention: Sharon Pope  
PO Box 122  
MUSWELLBROOK NSW 2333

Dear Madam,

**RE: Proposed Subdivision – Lot 101 and 103 DP 1170190– Ironbark Road, Muswellbrook**

In response to the public submission and in accordance with recent discussions, please find below our response in regard to these matters.

Please find enclosed the revised plans for proposed subdivision of Lot 101 and 103 in DP1170190.

As a general note, we would like to make the comment that this is a rezoning application and not a development application, many of the submissions are requesting detail that would be part of future development applications. Any layout proposal submitted is for information purposes only and will be again subject to full development application approval and public exhibition. This is a rezoning application not a development application.

We note the demographic of the all the submissions is from the immediate vicinity of the development, and there are no submissions from other residents/ land owners in the wider community. While we accept that change may not be desirable to all, it is inevitable result of a town as it grows and thrives. The lack of submissions from parties not directly adjacent to the development would suggest that the broader community are not opposed to future growth of the Muswellbrook town.

**1. Flora and Fauna**

Additional and significant additional work was undertaken with our Flora and Fauna consultant to provide a development that is in accordance with the current legislation. It appears however that this final plan, that included significant increases to green space within the subdivision was not updated in the council documentation that was placed on exhibition. This plan is now enclosed and includes additional amendments discussed below. The concept subdivision plan now includes 14.8 of open space areas.

We further note it was a directive in regard to layout, of the previous Director of Engineering, Mr. Neil Pope, that open space be kept to an absolute minimum to reduce cost of future ongoing maintenance of open space areas.

We note that all the land immediately surrounding this proposal is owned by BHP and is under environmental protection thus all residents can be confident of this land being open space for the foreseeable future. The subject site being now the only available land for expansion south for the existing development.

## **2. Connectivity to Ironbark Ridge Rural Residential.**

There have been lengthy and significant discussion with council staff over the years with regard as to how best join the residential and rural subdivision. The submitted proposal was in accordance with previous council staff preferred option. However, we acknowledge this was always going to be one of the more difficult areas to address. We have reviewed the layout to provide a transition lots size between the existing rural residential and future residential lots with a lot size of 1600m<sup>2</sup> to 2000m<sup>2</sup>.

## **3. Traffic**

The proposal has gone to RMS a number of occasions throughout the rezoning process. We acknowledge that additional detailed traffic study will be required with DA, however the RMS is satisfied that the traffic information provided is suitable for rezoning purposes. This is not a development application and the timing of future development applications will impact the traffic upgrades that may be required and any detailed traffic management studies undertaken now, will be out of date at the time of any future development application. In particular it should be noted that the Muswellbrook by-pass commencement would significantly alter the intersection requirements at the Highway.

The development does have two access points

- (a) Via Bimbadeen Drive
- (b) Via Rutherford Road and Ironbark Ridge Road.

## **4. Public not being aware of this proposal.**

The applicant does not accept that this application is a "surprise" to public, the extension of the existing residential areas in South Muswellbrook has been on council plans strategic plans for decades, and this proposal has been moving forward slowly for many years.

Council specifically requested the owners not sell this parcel of land to BHP as it would restrict future expansion of the South Muswellbrook Area which was council preferred area for future residential growth of the town.

We highlight the following items;

- (a) The land is noted in Muswellbrook Councils Strategic documentation as a High Significance future release area.
- (b) This proposal has already gone through the gateway process which determined that the land was suitable for rezoning which required council support in a public meeting.
- (c) The land is located centrally between two existing development areas "Ironbark Ridge" and "Eastbrook Links"
- (d) The development of "Ironbark Ridge" required specific increased infrastructure and planning to allow future expansion into this area
  - (i) The Ironbark Road – Additional road width and Full Kerb and Gutter to provide Bus Route Loop to Bimbadeen Drive
  - (ii) Watermain loop between "Ironbark Ridge" and "Eastbrook Links"
  - (iii) Creation of Lot 103 in DP1170190 to ensure security of future link road between the 2 existing release areas.



#### **5. Release and Demand for land.**

We note a perception in the submissions that there will be a hundreds of fully completed development lots spring up overnight. This will not be the case, land development takes significant time, the area will be released and developed over a significant period of time and it will be many years before it will be fully completed.

The application has been through the gateway process which deemed the proposal warranted in regard to future demand for land.

The rezoning of land is long a difficult process and needs to be undertaken in advance of high demand periods. Starting the process in high demand period will not achieve delivery of land in a timely manner to the market place and is likely to cause market bubbles that Muswellbrook has experienced in the past.

Muswellbrook currently has only one significant release area available for residential lots being Eastbrook Links. This is very unsuitable situation for any town to be in, as it puts future residential development in the hands of only one developer with an effective Monopoly on land release for the town. This proposal provides future competition and choice for the market place.

This development is proposed with larger lots than the adjoining "Eastbrook Links" and the applicant rejects outright that the development will be in any way a reflection of that development. The current owners are locals and it is not in their interest to provide a low quality subdivision next to their "Ironbark Ridge" rural residential subdivision that might reduce it's land prices in the future.

#### **6. Safety**

Council's request that the proposed lot layout be modified to include a road frontage to Lot 4095 in DP794059 being the reserve for the water reservoir. We accepted as a good outcome for the development, and have accordingly, modified the layout to reflect this change.

#### **Conclusion**

This rezoning application has been many years in the planning with many council staff and consultants involved in its development. The proposal has gone through Gateway, has gone to all external authorities a number of times and is noted as a high priority future release area for Muswellbrook. The owners have always worked cooperatively with council to provide best outcomes for this proposal and the Ironbark Ridge development.

Based on the information above and the further refinement of the future subdivision plan we seek councils support in finalizing this application.

Yours faithfully,



Michael Cole  
*Registered Surveyor*



**Planning &  
Environment**

Mr Steve McDonald  
General Manager  
Muswellbrook Shire Council  
PO Box 122  
MUSWELLBROOK NSW 2333

Our ref: PP\_2014\_MUSWE\_003\_00

Att: Pathum Gunasekara

Dear Mr McDonald

**Planning proposal to amend Muswellbrook Local Environmental Plan 2009**

I am writing in response to your Council's letter dated 18 July 2014 and updated planning proposal dated 1 October 2014 requesting a Gateway determination under section 56 of the Environmental Planning and Assessment Act 1979 ("EP&A Act") in respect of the planning proposal to rezone land from E3 Environmental Management to part R1 General Residential and part R5 Large Lot Residential under the Muswellbrook Local Environmental Plan 2009.

As delegate of the Minister for Planning, I have now determined the planning proposal should proceed subject to the conditions in the attached Gateway determination.

Council may still need to obtain the agreement of the Secretary to comply with the requirements of relevant S117 Directions, such as Direction 1.2 Rural Zones, Direction 1.3 Mining, Petroleum Production and Extractive Industries, Direction 1.5 Rural Lands, Direction 2.1 Environment Protection Zones and Direction 2.3 Heritage Conservation. Council should ensure this occurs prior to the plan being made.

The amending Local Environmental Plan (LEP) is to be finalised within 18 months of the week following the date of the Gateway determination. Council should aim to commence the exhibition of the planning proposal as soon as possible. Council's request for the Department of Planning and Environment to draft and finalise the LEP should be made 6 weeks prior to the projected publication date.

The State Government is committed to reducing the time taken to complete LEPs by tailoring the steps in the process to the complexity of the proposal, and by providing clear and publicly available justification for each plan at an early stage. In order to meet these commitments, the Minister may take action under section 54(2)(d) of the EP&A Act if the time frames outlined in this determination are not met.

Should you have any queries in regard to this matter, I have arranged for Mr Trent Wink from the Hunter office to assist you. Mr Wink can be contacted on (02) 49042716.

Yours sincerely,

A handwritten signature in black ink, appearing to read "David Rowland".

21 October 2014  
**David Rowland**  
General Manager  
Hunter and Central Coast Region  
Planning Services

Hunter and Central Coast Region - Hunter Office - Level 2 26 Honeysuckle Drive (PO Box 1226) Newcastle NSW 2300  
Phone 02 4904 2700 Fax 02 4904 2701 Website [planning.nsw.gov.au](http://planning.nsw.gov.au)



## Planning & Environment

### Gateway Determination

**Planning proposal (Department Ref: PP\_2014\_MUSWE\_003\_00):** to rezone land at Ironbark Road, Muswellbrook.

I, the General Manager, Hunter and Central Coast Region at Department of Planning and Environment as delegate of the Minister for Planning, have determined under section 56(2) of the EP&A Act that an amendment to the Muswellbrook Local Environmental Plan (LEP) 2009 to rezone land from E3 Environmental Management to part R1 General Residential and part R5 Large Lot Residential should proceed subject to the following conditions:

1. Prior to commencing public exhibition, Council is to update its planning proposal and provide the following supporting information:
  - Amend the Urban Release Area Map to identify the subject lands as an urban release area;
  - Contamination and Geotechnical Assessment;
  - Bushfire Assessment; and
  - Consult and take into consideration any comments made by the Office of Environment and Heritage, the NSW Rural Fire Service, Mine Subsidence Board or any other public authority consulted in accordance with the Minister's S117 Directions.
2. Community consultation is required under sections 56(2)(c) and 57 of the Environmental Planning and Assessment Act 1979 ("EP&A Act") as follows:
  - (a) the planning proposal must be made publicly available for a minimum of **28 days**; and
  - (b) the relevant planning authority must comply with the notice requirements for public exhibition of planning proposals and the specifications for material that must be made publicly available along with planning proposals as identified in section 5.5.2 of *A Guide to Preparing LEPs (Department of Planning & Environment 2013)*.
3. Consultation is required with the following public authorities under section 56(2)(d) of the EP&A Act and/or to comply with the requirements of relevant S117 Directions:
  - NSW Rural Fire Service
  - Office of Environment and Heritage
  - NSW Aboriginal Land Council
  - NSW Department of Primary Industries – Agriculture
  - NSW Department of Primary Industries - Minerals and Petroleum
  - Mine Subsidence Board
  - Transport for NSW - Roads and Maritime Services

Each public authority is to be provided with a copy of the planning proposal and any relevant supporting material, and given at least 21 days to comment on the proposal.

4. A public hearing is not required to be held into the matter by any person or body under section 56(2)(e) of the EP&A Act. This does not discharge Council from any obligation it may otherwise have to conduct a public hearing (for example, in response to a submission or if reclassifying land).

5. The timeframe for completing the LEP is to be **18 months** from the week following the date of the Gateway determination.

Dated 21<sup>st</sup> day of October 2014.



David Rowland  
General Manager  
Hunter and Central Coast Region  
Planning Services  
Department of Planning and Environment  
  
Delegate of the Minister for Planning

## 10.2 UPDATE ON NEW SHIRE WIDE LEP AND DCP

<b>Attachments:</b>	<b>A. Advice from DPIE on draft Planning Proposal</b>
<b>Responsible Officer:</b>	<b>Derek Finnigan - Deputy General Manager</b>
<b>Author:</b>	<b>Sharon Pope - Executive Manager - Environment and Planning</b>
<b>Community Plan Issue:</b>	<b><i>Diversify the economy, facilitate the development of intensive agricultur and other growth industries, make the Shire a more attractive place to invest and do business</i></b>
<b>Community Plan Goal:</b>	<b><i>Review the Local Environmental Plan and Development Control Plan to improve investment certainty for industry.</i></b>
<b>Community Plan Strategy:</b>	<b><i>Progress completion of an LEP and DCP.</i></b>

### PURPOSE

The purpose of the report is to provide information on the progress of developing a new Local Environmental Plan (LEP) and Development Control Plan (DCP) for Muswellbrook Shire.

### OFFICER'S RECOMMENDATION

The information contained in the report be noted.

Moved: \_\_\_\_\_ Seconded: \_\_\_\_\_

### REPORT

The preparation of a new LEP and DCP for the Shire has been underway since 2017. Unfortunately, changes to the *Environmental Planning and Assessment Act* in late 2017 introduced new requirements that have been the focus for staff until recently. A schedule of different milestones in the process is provided below, with an indication of what has been completed and what remains to be completed.

Action	Comment
Background discussion Paper for Review of LEP and preliminary community consultation.	<b>Complete.</b> Background Paper completed February 2018.
Complete Local Strategic Planning Statement	Currently on public exhibition.
Complete Planning Proposal	Drafting of Planning Proposal occurring in collaboration with DPIE staff. Latest advice received 13 July 2020 indicates general agreement (see attachment A).
Obtain Gateway Determination	The Planning Proposal will need to be reported to Council for a resolution to seek a Gateway Determination.
LEP zone tables drafted	New zones inserted. Development permitted with and without consent identified for all zones based on changes to the suite of zones being used. Zone objectives re-



Action	Comment
	drafted. Changes explained in previous Councillor workshop.
LEP clauses drafted	New clauses inserted. Redundant clauses deleted. Changes explained in previous Councillor workshop.
LEP maps prepared	
Specific technical studies completed (if required by Gateway)	
Exhibition (in accordance with Gateway Determination), with public agencies and the community	
Report to Council on results of public exhibition of the draft LEP.	
Referral to Parliamentary Counsel and the Minister for Planning and Public Spaces to make the Plan	
Publication of the Plan.	
Review of DCP Provisions and Structure	Work completed by Planning consultant in April 2018
Prepare Community Participation Plan	<b>Complete.</b> Plan became operational at the end of November 2019.
Review DCP provisions to reflect new LEP zones and clauses.	
Prepare graphics for Denman and Muswellbrook Town Centre Master Plans	
Amend standard provisions that currently occur in the DCP to be locality specific.	
Prepare a vegetation management section for the DCP.	
Exhibit the draft DCP.	
Report results of public exhibition of draft	

Action	Comment
DCP to Council.	
Steps taken to enable DCP to become effective	

With regard to the new LEP, by the end of June 2021 it is proposed to:

- Obtain Gateway Determination;
- Complete exhibition (in accordance with Gateway Determination); and
- Report to Council on results of public exhibition of draft LEP.

With regard to the new DCP, by the end of June 2021 it is proposed to:

- Complete a revision of the parking requirements, to simplify parking standards and reflect centralised parking approaches proposed in Denman and Muswellbrook;
- Rewrite heritage provisions to provide more guidance for alterations and additions to heritage buildings and maintenance of curtilages around heritage items;
- Have prepared vegetation management provisions to manage issues currently being experienced with the *SEPP (Koala Habitat Protection) 2019*, *SEPP (Vegetation in Non-Rural Areas) 2017* and the *Biodiversity Conservation Act*; and
- Update flood provisions to reflect recently completed flood studies and to provide more guidance for new buildings, and alterations and additions to buildings, in flood prone locations.

## Review of Muswellbrook Shire Council draft Planning Proposal for a new comprehensive LEP

PP Item No.	Proposed change	Comment	Further work
<b>Part 1 of PP</b>			
1.	Clause 1.2 new aims and objectives	No issues. These should be informed by how they have been used in practice for DAs. Could consider including matters relevant to the LSPS vision.	<b>Low</b>
2, 3, 4	New frameworks for agribusiness, mining and CICs	These three PP components are inter-related to an extent. More detailed commentary is provided below, however an overall comment is that there needs to be a body of work which pulls the justification for the planning controls together. The rural strategy work could do this i.e. identify the Ag land/use issues and the proposed planning control responses. The 2018 discussion paper does not adequately do this. Eg it proposes a new RU zoning framework but doesn't explain why the new RU1 mapping varies to the SEPP BSAL mapping, CIC protection is proposed but there is no reference to the need for a local clause.	
2.1, 4.1	RU1 to protect primary industry land including equine, viticulture, hay and dairying	<p>A new RU1 zone could be supported. We would need to understand where it is to apply and why, particularly where it varies from the SEPP mapping (the map on p143 of the 2018 discussion paper does not align with the BSAL and CIC SEPP mapping). DPI Ag's view would be useful.</p> <p>DPI Ag are also preparing agricultural land mapping to inform councils' land use planning and would be a useful input. This, coupled with the rural land strategy proposed in the LSPS, seem logical bodies of work to ensure a co-ordinated approach is taken to addressing Ag issues. This could then inform where a RU1 zone would best apply and justify inconsistencies with SEPP mapping.</p> <p>We would also want to understand changes in permitted uses. We've updated the land use matrix so it would be worth considering. The uses in the December narrative document generally look ok. The matrix can be accessed here:  <a href="https://www.planning.nsw.gov.au/Plans-for-your-area/Local-Planning-and-Zoning/Resources">https://www.planning.nsw.gov.au/Plans-for-your-area/Local-Planning-and-Zoning/Resources</a></p> <p>We would not support zone objectives which seek to discourage mining as currently</p>	<b>Medium (further evidence needed)</b>



		included in the Dec narrative.	
2.2, 4.2	RU2 for less productive land, encourage and protect primary industry and to maintain scenic character	<p>A new RU2 zone could be supported. We would need to understand where it is to apply and why. Would minimum lot size change also? Understanding the purpose of the RU2 is important i.e. the Dec narrative suggests it is the balance of the rural lands, the map in the discussion paper (p143) includes some SEPP CIC and BSAL (also proposed RU1), the zone objective suggests it is also about rural land with high scenic value (could proposed RU1 land also have high scenic value?). The views of DPI Ag would be worthwhile.</p> <p>The rural land strategy work would be useful in identifying the purpose of the RU2 and how it would work alongside the RU1 and proposed local provisions (cl 7.14, 7.16).</p>	<b>Medium (further evidence needed)</b>
2.3, 3.1	SP1 Zone for Mining and Agribusiness	<p>Appears to seek to replace the SEPP led framework with a LEP led framework to govern mining permissibility. Challenges with SP1 zone and a RU1 or RU2 zone should apply to mine sites. The Dec narrative doc suggests both would permit agribusiness (intensive livestock/ intensive plant). Applying a SP1 zone to the sites would also preclude future alternative employment generating uses should they be suitable for the site but not fit within the definition of agribusiness uses.</p> <p>If the desire is to apply a discrete zone to former mine lands targeted for agribusiness rather than rural activities more broadly, then perhaps spot rezoning sites to a new, tailored Rural zone could be considered. There'd need to further analysis though to identify why this approach would be preferable to retaining a RU1 or RU2 zone. The rural strategy could examine the strategic merit of this type of approach.</p> <p>Prohibiting mining in the RU1 and RU2 zones as proposed in PP Attachment 1 Item 6 would not be supported. This approach is contrary to the SEPP and SI framework.</p>	<b>High (inconsistent with State mining policy)</b>
2.4, 3.2	SP2 Infrastructure to allow transition of coal fired power sites to agribusiness	Unlikely to be supported. Rezoning the site to a suitable zone as the existing use ends would appear to achieve the same outcome. SP2 only applies to certain infrastructure sites and agribusiness would not be considered incidental or ancillary to electricity generation.	<b>High (inconsistent with the SI)</b>
2.5, 4.3	New clause 7.14 – planning controls for Critical Industry Cluster	Introducing a clause to protect CICs could be supported, noting that it would need to be consistent with the SEPP e.g. provisions that apply to local development. However, it is unclear what the issues are that the clause needs to address and whether those issues are common between equine and viticulture. Presumably its character/ amenity but are	<b>Medium (further evidence needed)</b>

		these the only issues? And would the clause be supported by DCP provisions? Analysis detailing the CIC issues is needed and this could be the rural land strategy. Cessnock is progressing work relating to its vineyards which may be worth considering.	
2.6, 3.3	New clause 7.16 – development on land associated with coal mines to assist transition to agribusiness	<p>Further information is required to determine whether this clause could be supported. Mine sites are currently zoned RU1 and therefore agribusiness (among other uses) is already permitted on the sites so the purpose of the clause needs to be understood. If the clause is to overcome SSD consent conditions or licence requirements, then this may not be possible. Legal advice may be required if this is its intent.</p> <p>A local clause with associated mapping could be supported to incentivise take-up of the lands for agribusiness. Presumably understanding incentives would come from the work identified in the LSPS actions under priorities 1 and 6. A local clause to manage issues relevant to agribusiness eg heads of consideration clause to manage land use conflict, biosecurity etc could be supported also. Cessnock LEP cl 7.11 is another approach but it would need strong justification for why limiting uses on the site would not potentially sterilise sites or result in missed opportunities. It treads a fine line with being considered a subzone so it may also depend on PCO's view.</p>	<b>Medium (further evidence needed)</b>
5.1	An existing E3 zone	Unlikely to be an issue if it is just carrying over existing E3 zoning to the same lands under the new LEP. This should be stated in the PP or a supporting report. The LSPS flags more work here (planning priority 14, short term action). If this work is to inform the PP then we would need to understand how the issues inform new zones and a sensitive lands map.	<b>Low</b>
5.2, 7	An existing W1 zone	Unlikely to be an issue.	<b>Low</b>
5.3	Clause 7.10 – Special character area – urban riparian corridors	Further information is required to determine whether this clause could be supported. The LSPS flags more work here (planning priority 17, short term action). Otherwise the 2018 discussion paper and Dec narrative provide limited information. While it is probably fine, we would need to understand what is proposed, where it applies and why. Presumably the areas would be mapped. This could be included in the PP or in a supporting document.	<b>Medium (further evidence needed)</b>
5.4	Establishing within the DCP a policy on native veg where development does not trigger offsets threshold	Noted.	<b>N/A (DCP)</b>
6	Framework to manage rural	This section overlaps with the earlier sections of the PP. Its not clear what is intended	<b>Medium</b>

	landscapes	here. Are scenic character controls proposed to be embedded in the LEP components listed in this section or are these just the proposed controls that relate to non-urban land? If the former then we need to know how the components are to address scenic amenity, where it would apply and why its merited. Depending on what is proposed, further justification may be required e.g. if a scenic amenity local clause is proposed, what work has been undertaken to evaluate the amenity of the various landscapes that the clause would apply to?	<b>(further evidence needed)</b>
6.4	Clause 5.3 development near zone boundaries	This is an optional standard clause so local content can be added. Local content is not able to conflict with the standard components of the clause.	<b>Low</b>
8.1	Creating a zone B2 for Denman commercial district	No issues. Justification for this should be included in the PP or a supporting document. We note this is currently provided in the 2018 discussion paper.	<b>Low</b>
8.2	Rezone balance of Denman from RU5 to R1	No issues. Justification for this should be included in the PP or a supporting document. We note this is currently provided in the 2018 discussion paper and is dependent on the town centre being rezoned to B2.	<b>Low</b>
8.3	Clause 5.10 Heritage Conservation	It is unclear what is intended here. This is a compulsory standard clause. Local content can be added to cl 5.10 but it is not able to conflict with the standard components.	<b>Low</b>
8.4	Clause 7.12 Design consideration in heritage conservation areas	Further information is required to determine whether this clause could be supported. Design considerations generally relate more to DCP controls than LEP controls. It is unclear what is proposed and why it is required. Is the clause based on a clause in another LEP?	<b>Low (further explanation needed)</b>
9.1	New B3 zone for Muswellbrook commercial centre	No issues. Presumably the zone boundary would align with that in the Muswellbrook Town Centre Strategy. If not, further justification would be needed. Permitted uses should be specified in the PP or a supporting document. Uses needs some analysis i.e. those shown in the Dec narrative reflect the mandated uses only, no further uses have been added. Completing the land use matrix would allow you to analyse what uses are being permitted across your B zones.	<b>Low</b>
9.2	New B4 zone for near the Muswellbrook railway station	Further information is needed to understand what is proposed, particularly in the context of the Muswellbrook Town Centre strategy. The PP suggests B4 would only apply to near the railway station yet the strategy indicates that it would also apply to most of the existing B2 (excluding those areas proposed for B3) in the town centre. The strategy identifies lands to be rezoned from B2 to R3 but no R3 is proposed in the PP.  Related to these matters is the extent of the B zoned land in the town centre. The LSPS	<b>Low (further explanation needed)</b>

		includes a short term action to review the extent of the B zones to reflect demand and it is unclear whether this has occurred. The 2018 discussion paper flags this as an important task for the Muswellbrook town centre.	
9.3, 10G	Apply the B5 zone to the former Muswellbrook Showground site	The site is already zoned B5. The new zone objectives and permitted uses appears to be the change. These look ok, limited potential for land use conflict with the other uses permitted in the zone.	Low
9.4	Amendments to floor space ratio and heights in the Muswellbrook town centre	This is probably ok assuming they align with the town centre strategy. Details should be included in the PP or in a supporting attachment.	Low
10A	Shop top housing in B4	No issues	Low
10A	Attached dwellings and multi-dwelling housing in R1	These are not new provisions. Both are already permitted with development consent in the R1 zone.	
10A	Attached dwellings in R2	R2 is a new zone for the LEP and its introduction should be noted in the PP. Where it applies is unclear. 2018 discussion paper suggests R2 should apply to all R1 outside the Muswellbrook town centre strategy study area. Town centre strategy recommends R2 for some land, R3 for others, but no R1. Dec narrative suggests R2 would apply to Denman and Sandy Hollow, contrary to PP and 2018 discussion paper. Proposed permitted uses look ok but should be included in PP or supporting attachment.	Low (further explanation needed)
10A, 10F	Permitting "moveable dwellings" in all zones where	This will need a local clause to enable and the PP or supporting attachment should note this. See Lake Mac LEP 2014 for example.	Low
10A	Local clause for loss of housing seeking contributions for housing under s7.32 of the Act	Further information is required to understand what is proposed and how it would work. S7.32 relates to SEPP 70 and loss of affordable housing is one of the circumstances whereby a council may levy for affordable housing. For a clause to be included in the LEP under s7.32, Council first needs to have an affordable housing contribution scheme. Information regarding this can be found here ( <a href="https://www.planning.nsw.gov.au/Policy-and-Legislation/Housing/Diverse-and-affordable-housing/SEPP-70-Affordable-Housing-Revised-Schemes">https://www.planning.nsw.gov.au/Policy-and-Legislation/Housing/Diverse-and-affordable-housing/SEPP-70-Affordable-Housing-Revised-Schemes</a> ).	Medium (further evidence required)
10B	New clause 4.1C Exceptions to minimum subdivision lot size for boundary adjustments	No issues.	Low
10C, 10D	New clause 7.15 Development on land associated with the Liddell and Bayswater Power Station	Further information is required to understand what is proposed and how it would be achieved. It is unclear how the clause would facilitate transition and new renewable energy generation. Existing zoning allows renewable electricity generation. Alternative	Medium (further explanation)

		uses could be facilitated through a spot rezoning. See earlier comments re SP2.	<b>required)</b>
10E	New clause 7.20 Sewerage treatment plant buffer (Muswellbrook and Denman)	Probably ok. Singleton LEP has example clause. PP or supporting document should detail what is proposed. Presumably it would be mapped as well.	<b>Low</b>
10H	Clause 7.8 Flood planning controls for development below the FPL	Flood planning in the SI is managed through the flood planning model clause. Variations from the model clause need to be strongly justified. Further information is required to understand what is proposed, how it would be achieved, and why the flood planning model clause in combination with DCP provisions is not suitable.	<b>Medium (further explanation required)</b>
10H	Clause 7.9 Floodplain risk management controls for developments like child care centres and residential care facilities including occupation and evacuation from sites		
10I	Permit additional types of minor development as exempt development	Probably ok. PP or supporting document should outline what is intended.	<b>Low</b>
11	Correct anomalies and errors identified through use of MLEP2009	No issues.	<b>Low</b>
<b>Part 2 of PP</b>			
	LZN Map	Generally, no issues with the list of zones, noting the comments above. RU5 Village is not listed so its unclear what zone is proposed for Sandy Hollow.  Part 1 of the PP suggests that only minor changes are proposed to the land use table for some of these zones. Given the age of the LEP 2009 and changes to the SI since, it would be worth Council considering completing the land use matrix to evaluate whether any other permissibility changes need to be made.	<b>Low</b>
	Lot Size Map – near Sandy Hollow for rural residential lots to replace those lost to mining	Further information is required to understand what is proposed and the need for rural residential supply to be provided at Sandy Hollow. The 2018 discussion paper states that further residential supply at Sandy Hollow should not occur noting there's high supply but low demand, and limited services. Further justification is therefore required. We note that the discussion paper does not appear to consider the potential for rural residential at Sandy Hollow which would be a different offering to the existing RU5 and would increase housing diversity. Whether the loss of rural residential housing due to	<b>Medium (further evidence required)</b>

		<p>mining activity translates to demand for rural residential at Sandy Hollow is also unclear (eg if housing is lost near Muswellbrook does this mean more demand at Sandy Hollow).</p> <p>Justification for a proposed site would need to have considered the suitability of alternative sites, site constraints, adjoining uses and the relevant Ministerial directions. The minimum lot size would also need to best align with potential demand. A comparative analysis of rural residential take-up in the Shire or adjoining LGAs (eg Upper Hunter) may assist in determining a suitable minimum lot size.</p>	
	Lot Size Map – rural residential development in lot 1 DP 323945, Yarrowa Road, Denman	Further information is required to understand what new minimum lot size is proposed and why. This should be detailed in the PP or a supporting document.	<b>Low (further explanation required)</b>
	Environmentally sensitive land map	Further information is required to understand what this map (and associated clause) is to apply to and why. This should be detailed in the PP or a supporting document.	<b>Low (further explanation required)</b>
	Floor space ratio map changes to implement Muswellbrook Town Centre Master Plan	Probably ok. The extent of the changes should be detailed in the PP or a supporting document.	<b>Low</b>
	Heritage Map adjustments to correct map errors for Denman Hall and Pickering	Probably ok. The change should be detailed in the PP or supporting document.	<b>Low</b>
	Height of buildings map changes to implement the Muswellbrook Town Centre Master Plan	Probably ok. The extent of the changes should be detailed in the PP or a supporting document.	<b>Low</b>
	Land Reservation Acquisition Map	No changes are identified in the PP.	
	Terrestrial Biodiversity Map changes to implement current OEH mapping	Further information is required. We need to understand the basis/ accuracy of the mapping, land affected, and the extent of change proposed.	<b>Medium (further explanation required)</b>
	Urban Release Area Map	No changes are identified in the PP.	
	Key sites map identification of Liddell and Bayswater Power Station sites and mine sites	Mapping sites on the key sites map is probably ok. We need to understand the clauses further first, per the comments above.	<b>Low</b>
	Land Classification (Part Lots) map	No changes are identified in the PP.	

Other comments			
	Consistency with LSPS	If Council's LSPS is finalised by the time the PP is ready to progress, then the PP will need to demonstrate how it gives effect to the LSPS. Relevant matters should be identified and the relationship of the planning proposal to those matters discussed. In addition to the vision/ planning priorities, it could talk to the PP delivering LSPS actions. There's several short term 'Council will review its planning controls to...' actions and this section could clarify which of these actions the PP addresses.	
	Overall planning proposal	<p>As the planning proposal is intended to inform a new comprehensive LEP, Council will need to detail all the provisions that it intends to include in the LEP. Any variations need to be detailed so it is clear what specific change is proposed and why its justified (see comments above). The clause numbering in the PP (cl. 7.20 waste buffers) suggests that there may be up to 20 local clauses proposed yet substantially less are detailed in this draft PP. Not sure whether this is an oversight.</p> <p>Port Stephens Council was one of the few Hunter councils to progress their comprehensive LEP as a planning proposal last time. It would be worth checking it out as an example of how you could approach it. Refer to PP_2012_PORTS_003 on our LEP tracker.</p>	
	Appendices – Attachment 1	The documents listed in this attachment appear relevant. As mentioned above, there's some conflict between some documents and Council's position on some matters may have changed since those documents were been prepared eg the 2018 discussion paper proposes a new RU7 zone. A document which clarifies the role of these existing documents would be beneficial. Similarly, a document which consolidates the changes proposed (this could be the Council report or an expanded version of the PP).	



### 10.3 NATIVE BIRD PUBLIC ART DENMAN

<b>Attachments:</b>	Nil
<b>Responsible Officer:</b>	Derek Finnigan - Deputy General Manager
<b>Author:</b>	Ziggy Andersons - Ecologist and Sustainability Team Leader
<b>Community Plan Issue:</b>	<i>Our local rivers and creeks are enhanced, utilised and valued</i>
<b>Community Plan Goal:</b>	<i>Adopt an Urban Riparian Masterplan and implement actions.</i>
<b>Community Plan Strategy:</b>	<i>Muswellbrook Urban Riparian Master Plan (MURMP) is adopted and implemented.</i>

#### PURPOSE

A piece of public art, in the form of a sculpture titled "The Kookaburra" was commissioned as a component of the 2019-20 Major Landcare Budget and was intended as an interactive piece for Hyde Park, Denman. Concerns have been raised by staff and members of the community about possible vandalism at this location. The purpose of this report is to obtain Council's input in selecting the location for installation of "The Kookaburra".

Based on the positive community feedback to the concept of having public art in Denman, the purpose of this report is also to seek approval from Council to commission two more large native bird statues and install them in locations where they can get the most exposure for the purpose of engaging the community in the natural environment of Muswellbrook Shire.

#### OFFICER'S RECOMMENDATION

**Council approves:**

1. **The Installation of the Kookaburra sculpture in the Denman Memorial and Remembrance Park; and**
2. **The commissioning of two further sculptures (Magpie and Frog-mouth Owl) and:**
  - a. **Installation of the Magpie at Simpson Park to identify the starting point of the Muscle Creek Nature Trail; and**
  - b. **Installation of the Tawny Frog-mouth Owl in the park on the corner of the Golden Hwy and Goulburn Drive, Sandy Hollow.**

**Moved:** \_\_\_\_\_ **Seconded:** \_\_\_\_\_

#### BACKGROUND

A piece of public art, in the form of a sculpture titled "The Kookaburra" was commissioned from the 2019-20 Major Landcare Budget. It was initially intended as an interactive piece for children to be located at Hyde Park, Denman. It is to engage with younger members of the community by giving them a visual and tactile representation of some of the native animals that occur within the area, fostering an interest in the natural environment of Muswellbrook Shire.

The Kookaburra was created by Mark Ray (son of Stan) and is carved from a single piece of Diodora Cedar sourced from a property at Martindale.

Consultation with the community via Facebook was undertaken with many positive comments being received.

**CONSULTATION**

When the Kookaburra sculpture was proposed, a post was created on the Muswellbrook Shire Council Facebook page asking for a preference from three possible design options and locations in Denman. The three designs nominated were a Kookaburra, a Magpie, and a Blue-tongued Lizard. This post was also shared to the Denman Community Facebook page.

These posts generated a lot of positive feedback, with over 60 comments on the Muswellbrook Shire Council Facebook page, and 80 on the Denman Community Facebook page. The consensus was that the Kookaburra was the preferred design, and that it should be installed along the new Denman Nature Trail. People also asked for more public art to be commissioned and displayed.

Some comments from the Facebook posts are provided below:

*‘why not all three spaced along the track, they are all natives to our beautiful part of the world and are seen throughout this area’*

*‘Why only one??? Couldn’t this be an ongoing project?’*

*‘would be great to see council support the work of this great local chainsaw artist!!’*

*‘great idea that could be added to each year’*

Consultation was also undertaken with Community Infrastructure to discuss suitable locations for sculptures, including consideration of the logistics of installing and maintaining the sculptures. It was suggested that Simpson Park Muswellbrook, the park on the corner of the Golden Hwy and Goulburn Drive Sandy Hollow, and Denman Memorial and Remembrance Park Denman would be the best locations for the sculptures due to their exposure, ease of installation and maintenance, and reduced risk of vandalism.

The Denman Nature Trail wasn’t considered suitable due to difficulty with installation and maintenance anywhere other than near the carpark or the lookout. However, both these sites were considered to have a high likelihood of vandalism. A series of smaller strategically placed pieces of public art would be more appropriate along the Denman Nature Trail.

Other sites that could be considered for the Kookaburra are:

- Hyde park along the Denman Name Trail, Denman;
- Arbor Park, Denman;
- Highbrook Park playground, Muswellbrook;
- The Muscle Creek Nature Trail; and
- The TEC forecourt.

Given the consultation that has already occurred with the Community, a location in Denman is considered most appropriate for the Kookaburra.

**CONSULTATION WITH COUNCILLOR SPOKESPERSON**

A copy of the report has been forwarded to the Mayor, Councillor Rush, the Deputy Mayor, Councillor Rush, the Councillor Spokesperson for the Arts, Councillor Foy, and the Councillor Spokesperson for Infrastructure, Councillor Woodruff, for review.

**REPORT**

The Sustainability Unit has commissioned a large Kookaburra sculpture from local artist Mark Ray (see below image) and was intended to be an interactive installation for children. At this stage only one sculpture has been commissioned. However, due to positive community feedback and the quality of the Kookaburra statue, the Sustainability Unit would like to expand the project and commission a series of at

least three sculptures of native birds, including a Magpie and Tawny Frog-mouth Owl, in addition to the Kookaburra. The commissioning of smaller native bird statues has also been considered for placement along the Denman Nature Trail.



Kookaburra statue (approx. 2m tall)



Example of a Magpie also by Mark Ray

## OPTIONS

### Option 1

Install the Kookaburra in the Denman Memorial and Remembrance Park. This is the preferred location for this sculpture given the expected high level of public use of this Park.

### Option 2

Install the Kookaburra in Hyde park along the Denman Name Trail.

### Option 3

Commission two further sculptures (Magpie and Frog-mouth Owl) and:

- a) Install the Magpie at Simpson Park to identify the starting point of the Muscle Creek Nature Trail; and
- b) Install the Tawny Frog-mouth Owl in the park on the corner of the Golden Hwy and Goulburn Drive Sandy Hollow.

### Option 4

Commission two further sculptures (Magpie and Frog-mouth Owl or other native bird) and:

- a) Install the Magpie at a location nominated by Council; and
- b) Install the Tawny Frog-mouth Owl at a location nominated by Council.

## CONCLUSION

Commissioning sculptures by local artists supports local business and provides a way to engage with younger members of the community by giving them a visual and tactile representation of some of the native animals that occur within the area fostering an interest in the natural environment of Muswellbrook Shire.

It is recommended that the Kookaburra sculpture be installed at the Denman Memorial and Remembrance Park due to expected high levels of exposure to children, the main target group of the sculpture, and lower levels of vandalism. It is also recommended that Council commission two further sculptures (Magpie and Frog-mouth Owl) for installation at other locations in the Shire.

**SOCIAL IMPLICATIONS**

There are no foreseen negative social implications.

The potential positive social outcomes include:

- improved visual amenity of the selected location/s and therefore increased use and appreciation of the location;
- the engagement of the community in the local natural environment and therefore their increased involvement in nature-based recreation and awareness and appreciation for that environment; and
- increasing the community's sense of place and identity due to the installation of a piece of public art that the community had a say in the design and location of, crafted by a well-respected local artist.

By commissioning these pieces from a local artist, Council is supporting local artists and industry.

The sculptures are a way of engaging the community with Muswellbrook Shire's natural environment and therefore encouraging the conservation of this unique environment.

**FINANCIAL IMPLICATIONS**

The funding for this project will be sourced from the Major Landcare Projects capital budget. The Kookaburra was funded from the 2019-20 Major Landcare Budget Nature Trail project. If approved, the remainder of the project will be funded from the 2020-2021 Major Landcare project budget subject to Council endorsement. The approved 2020-21 capital budget for Major Landcare Projects (3920.5617.504) is \$150,000.

A separate report is being prepared for submission to Council to detail the projects intended to be funded from the Major Landcare Projects budget for the 2020-21 financial year.

**POLICY IMPLICATIONS**

Nil known.

**STATUTORY IMPLICATIONS**

Nil known.

**LEGAL IMPLICATIONS**

Nil known.

**OPERATIONAL PLAN IMPLICATIONS**

The submission of this report complies with item 05.02.01 of the Operational Plan: 'Deliver at least one piece of permanent public art as a part of urban revitalisation'.

**RISK MANAGEMENT IMPLICATIONS**

The statues have been designed and built to make them as robust and stable as possible. The intention is to firmly attach them to a concrete pad so that they can't be knocked over and be a safety risk to the community.

## 11 COMMUNITY INFRASTRUCTURE

### 11.1 EDDERTON ROAD STAGE 1 PRACTICAL COMPLETION

<b>Attachments:</b>	A. Edderton Road Stage 1 Completion Works.pdf B. Edderton Road Stage 1 Special Conditions and Schedule
<b>Responsible Officer:</b>	Derek Finnigan - Deputy General Manager
<b>Author:</b>	Imelda Williams - Technical Officer - Traffic & Roads
<b>Community Plan Issue:</b>	<i>Our community's infrastructure is planned well, is safe and reliable and provides required levels of service</i>
<b>Community Plan Goal:</b>	<i>Facilitate investment in high quality community infrastructure necessary to a regional centre.</i>
<b>Community Plan Strategy:</b>	<i>Investigate and recommend appropriate management treatments for road safety and traffic management.</i>

#### PURPOSE

To advise Council that Hunter Valley Energy Coal has provided notification that they are of the opinion that all relevant "Works" associated with Stage 1 Edderton Road have met Practical Completion. The report requests Council to consider the recommendation that Council is now satisfied that the Edderton Road Stage 1 Works have reached Practical Completion and, further, gives approval for the new realigned portion of Edderton Road to be opened for use by the public. The report also recommends that Council resolves to progress the statutory road closure process, and transfer the old portion of Edderton Road to Hunter Valley Energy Coal (HVEC) in accordance with the Edderton Road Realignment Deed and Division 3 Part 4 of the *Roads Act 1993*.

#### OFFICER'S RECOMMENDATION

Council resolves:

1. That it is satisfied that Practical Completion of the Edderton Road Stage 1 Works has been achieved;
2. In consultation with Transport for NSW, to grant approval for the newly constructed portion (temporary road portion) of Edderton Road to be opened for use by the general public;
3. To delegate to the General Manager to grant consent to the s.138 *Roads Act 1993* application, generally in accordance with the Special Conditions attached to this report, to deny public access to part of the existing alignment of Edderton Road (proposed closed road portion) to allow for the transition of traffic to the realigned Edderton Road; and
4. To propose the closure of the proposed closed road portion in accordance with Division 3 Part 4 of the *Roads Act 1993* and to approve staff to proceed with the statutory road closure process, including advertising the proposed closure.

Moved: \_\_\_\_\_ Seconded: \_\_\_\_\_

#### BACKGROUND

On 24 September 2010 the NSW Minister for Planning granted Major Project Approval no. 09\_0062 to Hunter Valley Energy Coal (HVEC) for the Mt Arthur Mine- Open Cut Consolidation Project. This approval was modified on 26 September 2014 (Project Approval).

As set out in condition 47(d) of the Project Approval, HVEC is required to realign Edderton Road and its intersection with Denman Road, prior to mining within 200 metres of Edderton Road, to the satisfaction of Council and Roads & Maritime Services (RMS), now Transport for NSW (TfNSW). Council entered into the Edderton Road Realignment Deed (Deed) with HVEC on 3 July 2018 outlining the conditions and requirements surrounding the proposal.

## CONSULTATION

Aspects of this matter have been referred to Council through various reports on several previous occasions.

## CONSULTATION WITH COUNCILLOR SPOKESPERSON

Councillor Scholes & Councillor Woodruff.

## REPORT

### Practical Completion

Council has received notification from HVEC advising that in accordance with Clause 16.2 of the Deed, HVEC is of the opinion that all relevant "Works" associated with Stage 1, have met Practical Completion.

Under the terms of the Deed, Council's acceptance that "Practical Completion" has been achieved has the following ramifications:

- Subject to HVEC complying with its obligations under the Deed to construct the Stage 1 Works to Practical Completion and to dedicate the realigned portion of Edderton Road, Muswellbrook Shire Council (MSC) acknowledges and agrees that HVEC has the right to excavate the Proposed Closed Road Portion on road closure in accordance with the existing approvals, including the Project Approval.
- MSC must progress the legal closure of the proposed road closed portion. Upon closure of same, MSC is required to transfer the closed road portion to HVEC.
- If the proposed closed road portion is not "legally" closed under the *Roads Act 1993* by the time HVEC needs to mine the proposed closed road portion in accordance with the Project Approval, then MSC agrees that:
  - Provided the Stage 1 Works are then completed to Practical Completion and the Stage 1 Road Dedication Plan has been registered, HVEC can excavate the surface and sub-stratum of the proposed closed road portion; and
  - Section 146 (1)(c) of the *Roads Act 1993* has effect and MSC has no entitlement to any minerals that are mined by HVEC from or below the surface of the Proposed Closed Road Portion.

If HVEC exercises these rights:

- HVEC will carry out any excavation of the proposed closed road portion in accordance with all the relevant Approvals; and
- HVEC will indemnify and release MSC and keep MSC indemnified against and from all claims arising from any action or omission by HVEC or its employees, agents, invitees or contractors on or in respect of the Proposed Closed Road Portion prior to registration and transfer of the Proposed Closed Road Portion to HVEC.

Council staff have reviewed the submitted As Built, quality and other necessary documentation which will form the basis for determining Practical Completion. These documents include:

Documents	Status as at 15 July 2020
Edderton Road Maintenance Management Plan	Received.
Edderton Road Asset Management Plan	Received.
Guarantee Amount paid in the form of a \$45 Million	Council has received the Bank Guarantee

Documents	Status as at 15 July 2020
Bank Guarantee	
Work as Executed Plans for the Stage 1 Construction Works	Received
Supporting Quality Documents including Hold Point Documentation	Received.
Works Verifier Final Verification Certificate and certifying documents	Received.
Asset Data Sheet	Received
Road Dedication Plan for the realigned portion of Edderton Road	Road dedication plan has been registered DP1259663 for the realigned portion of Edderton Road.
Road Dedication Plan for Edderton/Denman Road intersection showing road widening	Currently with Council to issue a subdivision certificate prior to registration
Road Safety Audit and Closeouts	Received
Written documentation from Transport for NSW as evidence of the satisfaction of the Denman/Edderton Road intersection	Received
Ausgrid Certification of satisfaction of the street lighting	Received
Nominated TGS/TCP and ROL evidence for s.138 application to transition traffic to new realigned road	Received, working in consultation with TfNSW

#### s.138 Road Act 1993 Approval

To facilitate the transition of traffic onto the new alignment (temporary road portion), Council has received a s.138 *Roads Act 1993* application from HVEC which proposes works to enable this transition to occur. Once traffic has been diverted onto the new alignment, access to the existing alignment (proposed closed road portion) by the public will no longer be possible due to the construction limitations, including the required removal of the side-track used during the construction of the Stage 1 “tie in” works. Also, the use of the existing portion of Edderton Road by the public will no longer be necessary or desirable.

The existing intersection of Edderton and Denman Road is proposed to be decommissioned as part of this approval in order to physically deny access to the public. Plans for this work are attached to the report and include the following provisions:

- public notification of proposed changed traffic conditions, including variable message boards and advertising;
- initial soft closure under traffic control to enable the transition of traffic to the new aligned temporary road section; and
- permanent decommissioning works at the existing intersection, which include removal of existing line marking, guideposts, road pavement, and signage, installation of new line marking, and fencing of the road boundary.

Council is in consultation with Transport for NSW (formerly RMS) to seek concurrence and Road Occupancy Licence approval prior to works being undertaken on Denman Road.

#### Legal Closure of Redundant Portion of Edderton Road

Following determination that Practical Completion of Stage 1 Works has been met, Council is to further consider the approval to progress the legal closure of the redundant portion of Edderton Road. A resolution of Council is required to permanently close the redundant portion of Edderton Road in



accordance with Division 3 Part 4 of the Roads Act 1993. Once closed, the land comprising the former closed road can be transferred to HVEC in accordance with the provisions of the Act and the Edderton Road Realignment Deed, and subject to a Council resolution to dispose of the closed road portion. Staff are seeking approval to advertise the permanent closure of Edderton Road and progress the closure process to finality.

### OPTIONS

- Option 1:
- (a) Council deems that Practical Completion for Edderton Road Stage 1 Works has been achieved.
  - (b) Approve the s.138 *Roads Act 1993* application to prevent the public from using the existing portion of Edderton Road once the realigned portion has been completed to the satisfaction of Council as outlined in the report. Delegate to the General Manager authority to sign the Section 138 consent for the works generally in accordance with the Special Conditions outlined in the attachment.
  - (c) Resolve to progress the road closure process with a view to closing the now redundant portion of Edderton Road and approve staff to progress same as outlined in the report.
- Option 2:
- (a) Council does not deem that Practical Completion for Edderton Road Stage 1 Works has been achieved and provide justification to this effect.
  - (b) Council does not approve the s.138 *Roads Act 1993* application to prevent the public from using the existing portion of Edderton Road.
  - (c) Council does not resolve to permanently close the now redundant portion of Edderton Road and approve the road closure to progress as outlined in the report.

### CONCLUSION

It is recommended that Option 1 is the preferred course of action in this instance.

### SOCIAL IMPLICATIONS

Opening the new, realigned section of Edderton Road to the public will provide an improved road for road users.

### FINANCIAL IMPLICATIONS

Council will recoup any costs through the s.138 *Roads Act 1993* fees outlined in the Special Conditions. All costs associated with the construction are the responsibility of HVEC.

### POLICY IMPLICATIONS

The road closure is in accordance with Council's Road Closure Policy.

### STATUTORY IMPLICATIONS

Any works undertaken within a road reserve require the roads authority approval through the s.138 *Roads Act 1993* permit approval. Council is the roads authority for Edderton Road.

### LEGAL IMPLICATIONS

Council is the Roads Authority under the Roads Act 1993 and is acting within its powers.

### OPERATIONAL PLAN IMPLICATIONS

The submission of the report complies in principle with item 19.3.4 of the Operational Plan: 'Investigate and recommend appropriate management treatments for road safety and traffic management'.

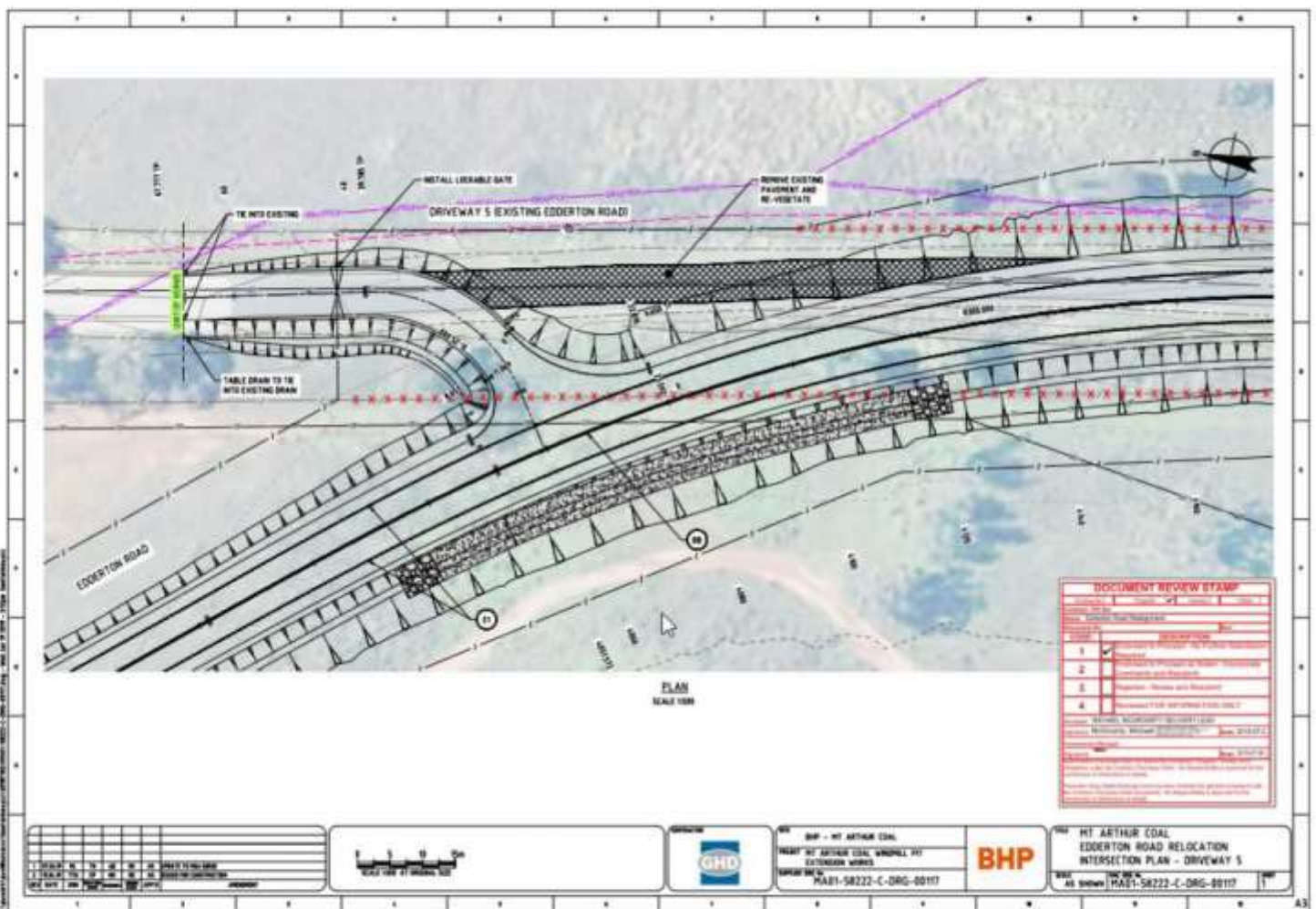
### RISK MANAGEMENT IMPLICATIONS

The applicant under the General Conditions is to carry Insurance to indemnify Council should any incident be identified related to the approval.

### Proposed works







## Schedule of Details

1. The Works	The works are the works within the Road Reserve of Edderton Road as detailed in the letter and application from Hunter Valley Energy Coal dated 7 November 2019. Council Doc ID 1125328	
2. The Site	Edderton Road Stage 1 and associated areas as shown in the above documents.	
3. Estimated Cost of the Works	\$ Bond not required (GST inclusive)	[Clause 6.1(e)]
4. Estimated Council Costs	Plan Approval \$ 3,200 (GST inclusive) Construction Observation Traffic Control Plans \$ 570 (GST inclusive) Final Acceptance (WAE review etc) \$ 500 (GST inclusive)  Total \$4,270 (GST inclusive)	[Clause 6.1(f)]
5. Defects Liability Period	12 Months	[Clause 8.4]
6. Consent Holder's Representative	Name.....Sarah Bailey..... Position..... NSW Approvals Principal HVEC ..... Mobile Phone.....0467 743 639..... Office Phone..... Email sarah.k.bailey@bhpbilliton.com. Facsimile.....	[Clause 14.3]
7. Council's Representative	Name.....Kellie Scholes..... Position.....Manager Roads Drainage & Technical Services..... Mobile Phone.....0407 252 142..... Office Phone.....02 6549 3756..... Email .....Kellie.scholes@muswellbrook.nsw.gov.au..... Facsimile.....02 93005774.....	[Clause 14.3]



**Special Conditions**

1. The works must not commence until MSC has advised the Consent Holder in writing that Practical Completion of Stage 1 works – in accordance with the Edderton Road Realignment Deed have been determined.
2. The Consent Holder is to advise Council's Engineering Officer Works Quality on 0418 110 010 five (5) days prior to commencing work to confirm inspections prior to commencement.
3. The Consent Holder is to advise directly affected residents and property owners of the program of Works that may affect access etc. at least a week in advance, and incorporate reasonable access needs for those properties. The Consent Holder is to advise the general public of the Works by variable message board in the local area of the Works and advertisement in the local newspaper at least a week prior to site works.
4. The Variable Message Sign (VMS) should have the wording "Changed Traffic Conditions" "Side Road Closed" "Edderton Road xxkm ahead" or similar depending on the direction of travel. The VMS should remain in place for a period of 6 weeks.
5. The Consent Holder must apply to Transport for NSW (formerly RMS) for a Right to Occupy Licence prior to any work being undertaken on Denman Road.
6. The occupation of the Road should be undertaken in a safe manner, with adequate dust control.
7. The Consent Holder shall check for other services and is responsible for any damage to services and Council Infrastructure. Dial before you dig 1100.
8. This Consent does not constitute approval of any environmental impacts and the Consent Holder is responsible for any such impacts and any approvals that may be required in relation to the Works.
9. The Consent Holder must ensure that mud and dust from the Works is not carried on to the Road. (It is suggested that a device (from the Soil management book) may be required to remove such materials prior to entering the roads. Note: If required a shake down device should be placed off the existing road so that the public did not need to travel on it.)

## 11.2 PROPOSED COUNCIL PUBLIC ROAD CLOSURES - BENGALLA MINE CONTINUED OPERATIONS PROJECT

<b>Attachments:</b>	<b>A. Roads proposed to be closed within Bengalla</b>
<b>Responsible Officer:</b>	<b>Derek Finnigan - Director - Community Infrastructure</b>
<b>Author:</b>	<b>Imelda Williams - Technical Officer - Traffic &amp; Roads</b>
<b>Community Plan Issue:</b>	<b><i>Our community's infrastructure is planned well, is safe and reliable and provides required levels of service</i></b>
<b>Community Plan Goal:</b>	<b><i>Facilitate investment in high quality community infrastructure necessary to a regional centre.</i></b>
<b>Community Plan Strategy:</b>	<b><i>Investigate and recommend appropriate management treatments for road safety and traffic management.</i></b>

### PURPOSE

The report recommends that Council consider the closure and sale of unnamed Council public roads within the Bengalla Continuation of Mining Project.

### OFFICER'S RECOMMENDATION

#### Council:

1. **Endorses, subject to the relevant statutory processes and negotiation of the relevant agreement(s), the closure and sale of the unnamed Council public roads as outlined in the report;**
2. **Approves staff to proceed with the road closure process in accordance with Division 3 Part 4 of the *Roads Act 1993*;**
3. **Deems that construction has taken place on the unnamed Council public roads under consideration and as such remain vested in Council upon closure, in accordance with s. 38E (2) (a) of the *Roads Act 1993*;**
4. **Subject to resolution 1 above, delegates to the General Manager:**
  - (a) the negotiation of the commercial terms of the contemplated sale of the roads; and**
  - (b) the execution of all relevant documents required to carry out the road closure and sale process.**

**Moved:** \_\_\_\_\_ **Seconded:** \_\_\_\_\_

### BACKGROUND

On 3 March 2015 the Secretary of the Department of Planning and Environment granted State significant development consent for SSD-5170 for the Bengalla Continuation of Mining Project (BCMP) under Division 4.1 of Part 4 of the *Environment Planning Assessment Act 1979 NSW*.

SSD-5170 authorises the continuation of mining west of existing mining operations, utilising the existing coal washing, handling and loading facilities. All other key approvals for the BCMP have been granted including mining leases and an environment protection licence.

Sparke Helmore on behalf of Bengalla Mining Company Pty Ltd (BMC) has written to Council requesting:

- Closure of the Council public roads as indicated on the attached plan; and

- Upon closure of the roads, purchase of the land within by the Bengalla Joint Venture in their respective shares.

## CONSULTATION

MACH Energy Australia Pty Ltd as adjoining landowner

General Public - the proposed road closures will be advertised calling for Public submissions as part of the road closure process

Department of Planning, Industry and Environment.

## CONSULTATION WITH COUNCILLOR SPOKESPERSON

A copy of the report has been forwarded to the Mayor, Councillor Rush, the Deputy Mayor, Councillor Scholes, and the Councillor Spokesperson for Infrastructure, Councillor Woodruff, for review.

## REPORT

Council has received a written request from Sparke Helmore on behalf of the Bengalla Mining Company Pty Ltd (BMC) for the closure and sale of the roads outlined in this report. The roads under consideration for closure have been identified as unnamed, presently unformed Council public roads and lie within the mining footprint of Bengalla Continuation of Mining Project and shown on the plan attached to this report. The BMC owns all of the freehold land adjoining the roads proposed to be closed. Concurrence has been provided by MACH Energy Australia Pty Ltd, owners of the Mount Pleasant Coal Project which adjoins the Bengalla Mine to the north, in the vicinity of the Council Roads.

Investigations into the matter have revealed that sections of the roads previously formed part of the "Musclebrook to Denman and Merriwa Roads at the Woburn Crossing" which was deviated and shown in various road plans dating back to the 1870's. The roads lie within the old Overton and "Bengala" Estates as shown on the Clanricard Roads Branch Parish Map. There is currently little to no evidence of any previous road alignment on the ground today and Council is relying on historic Parish Maps and searches undertaken. Section 38E(2) of the Roads Act 1993 legislates which Council roads can remain vested in Council upon closure and provides that:

*"The land comprising a former road*

- In the case of a public road that was previously vested in council (other than a public road in respect of which no construction has ever taken place) – remains vested in the council and*
- in any other case- becomes (or if previously vested in the Crown, remains) vested in the Crown as Crown land".*

As advised by the NSW Department of Planning, Industry and Environment it is up to Councils to interpret "construction". Councils may consider construction to take any form and have occurred at any time. The legislation does not specify that the construction must currently be visible. In this instance the investigation undertaken does indicate that the roads were previously in use by the public and that some form of construction may have taken place. To this effect it would be prudent for Council to make the decision that the roads proposed to be closed remain vested in Council upon closure in accordance with the Act to allow for the sale to the BMC. Without this proclamation by Council the roads will likely vest in the Crown as Crown Land upon closure in accordance with the Act.

The roads proposed to be closed are not required to provide continuity for an existing road network and are not reasonably required as a road for public use (whether for present or future needs) and therefore meet the objectives of Council's Road Closure Policy. The roads proposed to be closed do not form part of the "recommended Road Network Plan" in the "Muswellbrook Mine Affected Roads Network Plan Review" adopted by Council at the 26 May 2020 Ordinary meeting of Council.

The road closures also do not appear to fall under the Notice of Motion previously adopted by Council being.

1. *Without predetermining any particular application under Division 3 of Part 4 of the Roads Act 1993, Council adopts a general policy that significant alterations to the rural road network should not be*



*resolved until after the adoption of a new Local Environment Plan – presently anticipated in May 2019.*

2. *Review and finalisation of the Contributions Plan accompanying the Mine Affected Roads Strategy.'*

### **OPTIONS**

Option 1: Council considers that the proposed closure of the roads would not significantly alter the rural road network, are deemed suitable for closure at this time and to proceed with the closure and sale of the roads in accordance with Division 3 Part 4 of the *Roads Act 1993* and as outlined in the recommendations to Council of this report.

Option 2: Council consider that the Notice of Motion as outlined above is relevant in this matter and the roads proposed to be closed will not be considered by Council until the review and finalisation of the Contributions Plan accompanying the Mine Affected Roads Strategy.

### **CONCLUSION**

It is recommended that Council consider Option 1 as the most appropriate course of action at this time.

### **SOCIAL IMPLICATIONS**

Nil known.

### **FINANCIAL IMPLICATIONS**

All fees and charges will be met by the proponent. In accordance with s. 43 (4) of the *Roads Act 1993* the money received by a council from the proceeds of sale of the land is not to be used by the council except for acquiring land for public roads or for carrying out road work on public roads.

### **POLICY IMPLICATIONS**

The proposed road closures meet the objectives of Council's Road Closure Policy.

### **STATUTORY IMPLICATIONS**

Nil known.

### **LEGAL IMPLICATIONS**

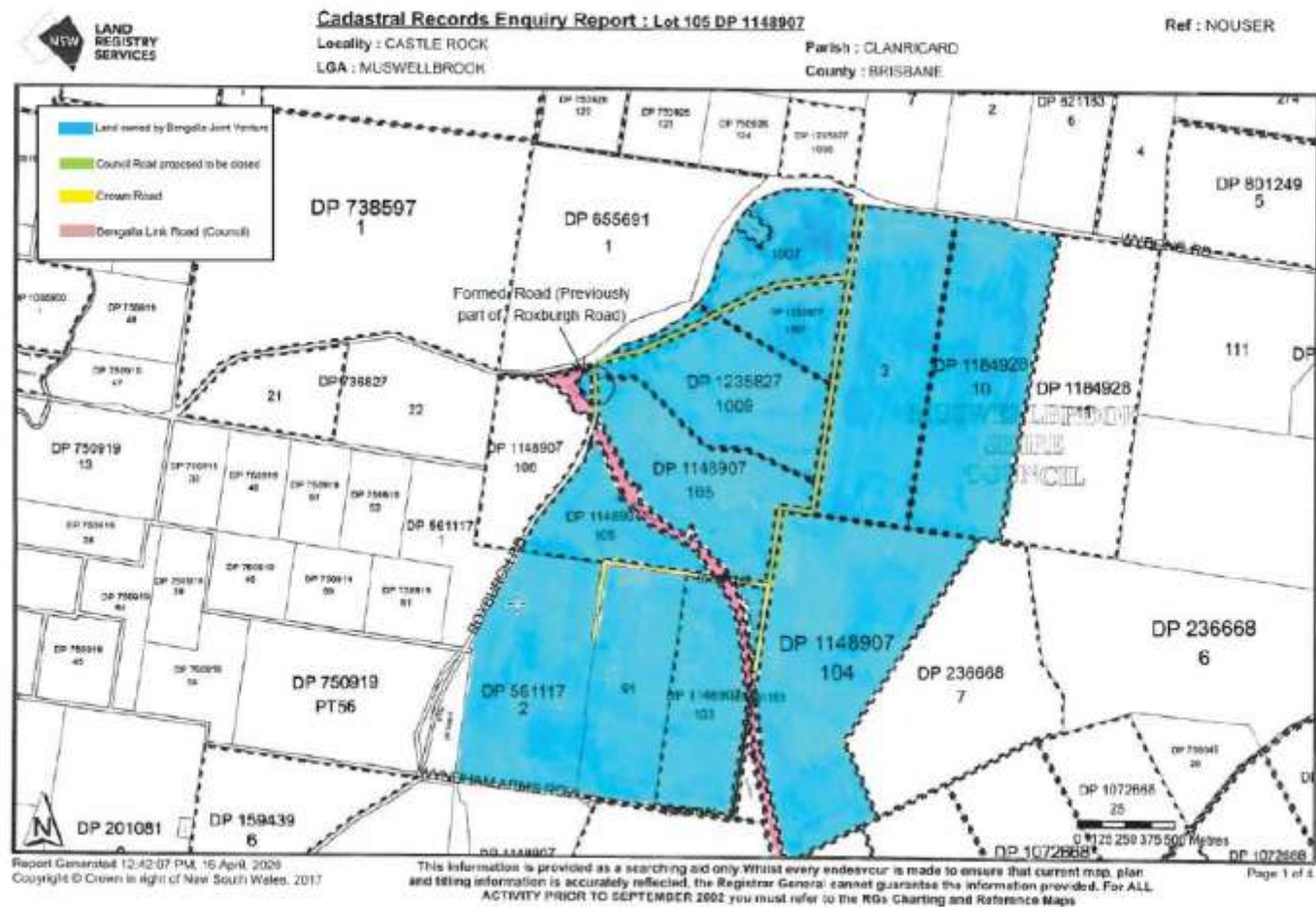
Council is the Roads authority and is acting within its powers. The road closure will be undertaken in accordance with Division 3 Part 4 of the *Roads Act 1993*.

### **OPERATIONAL PLAN IMPLICATIONS**

The submission of this report complies in principle with item 19.3.4 of the Operational Plan: Investigate and recommend appropriate management treatments for road safety and traffic management.

### **RISK MANAGEMENT IMPLICATIONS**

The Council roads identified for closure are located entirely within the project boundary of the BCMP and largely within Mining Lease No. 1728 and Mining Lease No. 1729 held by Bengalla Mining Company. Closure of the roads would remove any public safety issues arising from the location of the public roads within the Bengalla project area, mining leases and colliery holding. Once closed the roads cease to be a public road and the rights of passage and access that previously existed in relation to the road are extinguished.



### 11.3 CROWN ROADS PROPOSED TO BE CLOSED AT WYBONG

<b>Attachments:</b>	<b>A. Crown Road Closure Wybong Map</b>
<b>Responsible Officer:</b>	<b>Derek Finnigan - Deputy General Manager</b>
<b>Author:</b>	<b>Imelda Williams - Technical Officer - Traffic &amp; Roads</b>
<b>Community Plan Issue:</b>	<b><i>Our community's infrastructure is planned well, is safe and reliable and provides required levels of service</i></b>
<b>Community Plan Goal:</b>	<b><i>Facilitate investment in high quality community infrastructure necessary to a regional centre.</i></b>
<b>Community Plan Strategy:</b>	<b><i>Investigate and recommend appropriate management treatments for road safety and traffic management.</i></b>

#### PURPOSE

To recommend that Council advises the Department of Planning, Industry and Environment (DPIE) that Council does not support the closure of the Crown Road that provides access to Lot 224 DP750968 and that legal access is to be maintained to this parcel of Crown Land.

#### OFFICER'S RECOMMENDATION

Council advises the Department of Planning, Industry and Environment that Council does not support the closure of Crown Road providing access to the Crown Land being Lot 224 DP750968 and that due consideration be given to the matters raised by the local Aboriginal Groups consulted in relation to the matter.

**Moved:** \_\_\_\_\_ **Seconded:** \_\_\_\_\_

#### BACKGROUND

A report, entitled 'Crown Roads proposed to be closed at Wybong' was submitted to the 30 June 2020 Ordinary Council Meeting. Council's resolution in relation to the report was:

#### 11.1 CROWN ROADS PROPOSED TO BE CLOSED AT WYBONG

*RESOLVED on the motion of Crs Rush and Eades that:*

*The matter be deferred for consideration at the July Council Meeting following discussion with the Wanaruah Local Aboriginal Land Council and other Aboriginal Groups.*

This report provides updated information and advice following consultation with the Wanaruah Local Aboriginal Land Council and other Aboriginal Groups.

#### CONSULTATION

Wanaruah Local Aboriginal Land Council

NTSCorp representing the Gomeroi Application for Native title Determination application NSD37/2019-

Maeve Rose Parker (Solicitor)

Hunter Valley Aboriginal Corporation

Tocomwall Pty Ltd

**CONSULTATION WITH COUNCILLOR SPOKESPERSON**

A copy of the report has been forwarded to the Mayor, Councillor Rush, the Deputy Mayor, Councillor Scholes, and the Councillor Spokesperson for Infrastructure, Councillor Woodruff, for review.

**REPORT**

In accordance with Council's resolution arising from the 30 June 2020 Ordinary Council meeting, staff requested a further extension of time from the Department of Planning Industry & Environment (DPIE) to allow for the matter to be deferred to the July meeting following consultation with the Wanaruah Local Aboriginal Land Council and local Aboriginal Groups.

Council has carried out consultation with the Wanaruah Local Aboriginal Land Council and local Aboriginal Groups, a summary of which is provided below.

Wanaruah Aboriginal Land Council

The Chief Executive Officer of Wanaruah Local Aboriginal Land Council (Wanaruah LALC) advised that Wanaruah LALC did not support the closure of roads that will deny access to any parcel of land that they own or have under claim. At present they have made a number of undetermined claims on the Crown Parcels of land to the north of this site.

NTSCorp

NTS Corp represent the Gomeroi Application in their native title determination NSD37/2019. They have advised that the closure or sale of a Crown Road which provides access to native title land, or itself is subject to native title rights is a future act under the provisions of the *Native Title Act 1993 (Cth)*, unless native title has been extinguished. As registered native title claimants, the Gomeroi Applicant is afforded certain procedural rights in relation to future acts, including the right to be notified and the right to provide comments.

Accordingly, Council forwarded the Notice from DPIE for their consideration. NTSCorp have advised that they will contact DPIE directly in this matter and that Council take no further action on their behalf.

Tocomwall Pty Ltd

The advice provided by Tocomwall Pty Ltd cites the PCWP Aboriginal Corporation (ICN7800), which is registered with the Federal Government under the Aboriginal Association Act. The Corporation advised they cannot support the application to close the Crown Road. The PCWP Aboriginal Corporation has requested that Council consider the actions of the mining company in the following light: If Council were to support the request to the Crown Road closure it would place the traditional owners of the area in a difficult position. The Crown Road leads to a Travelling Stock Route (TSR) that will be landlocked by the closure of the Crown Roads and in affect will allow the mining company to avoid Native Title negotiations under Section 29, the negotiation of access to the TSR that will allow the group open movement to collect resources and material for ceremonies and retain areas of land that contain important cultural areas.

**OPTIONS**

Council has the option to advise DPIE that Council does not support the closure of the Crown Roads that provide legal access to the Crown Land (Lot 224 in DP 750968) for the reasons outlined in the report and raised by the Aboriginal Groups consulted. Council could forward the correspondence received from the interested Aboriginal Groups and request that DPIE give due consideration to the matters raised by the interested parties.

**CONCLUSION**

As Council is not the road authority in this matter, it is recommended that Council advise the DPIE that it does not support the road closure as outlined above, and requests that legal access is to be maintained to the Crown Parcels of Land and that the matters addressed in the correspondence provided be given due consideration by the Department.

**SOCIAL IMPLICATIONS**

By removing access to Crown Lands this affects the traditional owners of the area by removing their right for free movement to collect resources and material for ceremonies from the lands.

**FINANCIAL IMPLICATIONS**

Not applicable.

**POLICY IMPLICATIONS**

Nil known.

**STATUTORY IMPLICATIONS**

Nil known

**LEGAL IMPLICATIONS**

The process is undertaken in accordance with the provisions of Division 2 Part 4 of the *Roads Act 1993*.

The roads under consideration are non-Council public roads, therefore Council is not the Roads Authority in this matter.

**OPERATIONAL PLAN IMPLICATIONS**

Not applicable.

**RISK MANAGEMENT IMPLICATIONS**

In accordance with the *Roads Act 1993*, once closed, the Crown roads will cease to be a public road and the rights of passage and access that previously existed in relation to the roads are extinguished.



Case Officer: TAYLORDR Date: 15/04/2020 File ref: 19/07092

# Proposed Closure of Public Roads Cluster 618240

LGA: MUSWELLBROOK  
LOCALITY : WYBONG  
PARISH : WYBONG  
COUNTY : BRISBANE

**Legend**

MANGOOLA COAL OPERATIONS PTY LIMITED

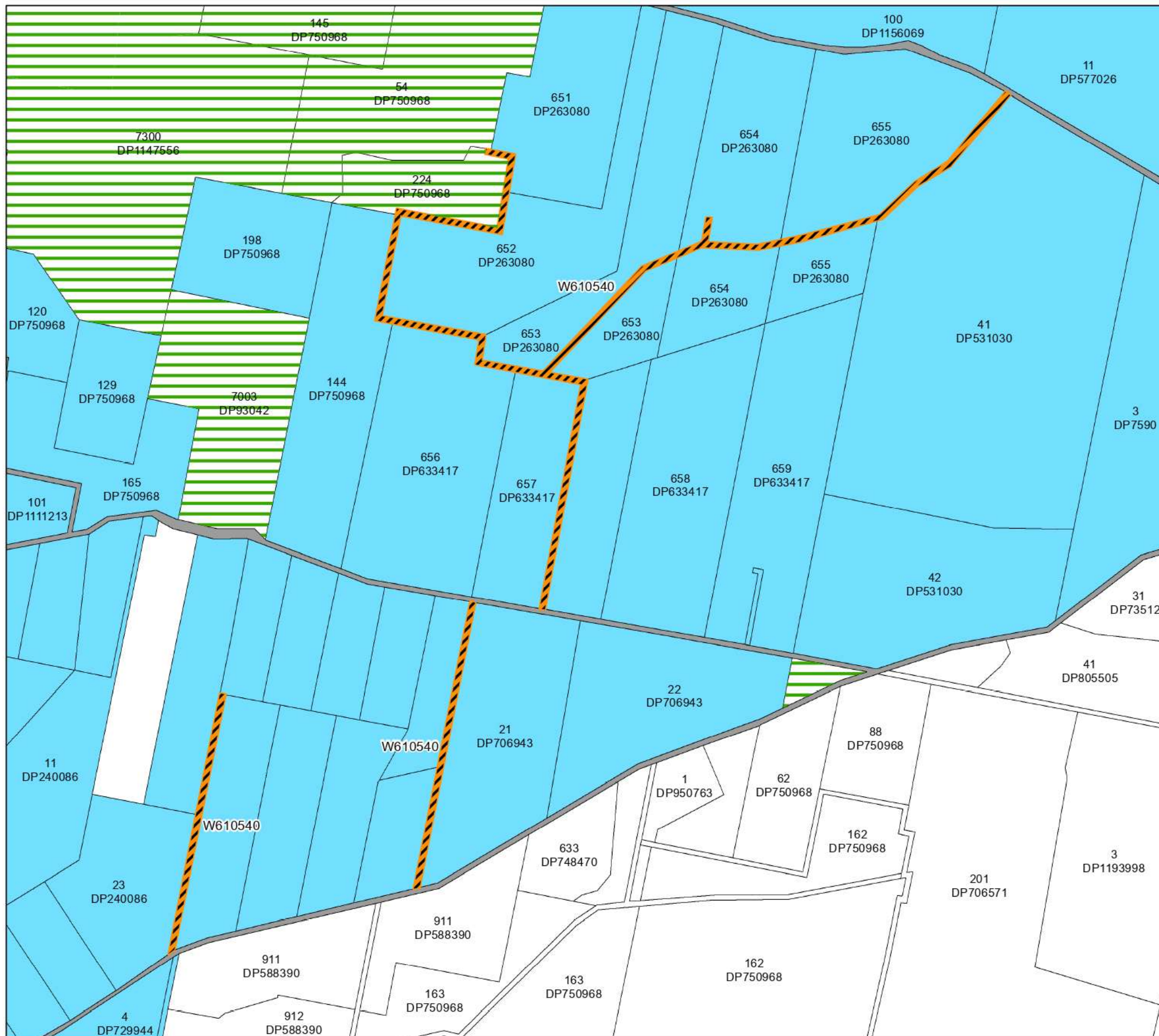
CROWN ROADS UNDER APPLICATION & PROPOSED FOR CLOSURE

PUBLIC ROAD

NATIONAL PARK

STATE FOREST

CROWN LAND



Planning,  
Industry &  
Environment

Disclaimer: The information contained in this map has been provided in good faith. Whilst all effort has been made to ensure the accuracy and completeness of this information, the data providers take no responsibility for errors or omissions nor any loss or damage that may result from the use of this information.

Copyright: Department of Planning, Industry and Environment - Crown Lands, 2020

**11.4 MT PLEASANT MODIFICATION 4 - RAIL LOOP S138 ROAD ACT 1993  
PERMIT AMENDMENTS**

<b>Attachments:</b>	<b>A. MACH Energy Mod 4 Rail Loop Revised Special Conditions</b>
<b>Responsible Officer:</b>	<b>Derek Finnigan - Deputy General Manager</b>
<b>Author:</b>	<b>Imelda Williams - Technical Officer - Traffic &amp; Roads</b>
<b>Community Plan Issue:</b>	<b><i>Our community's infrastructure is planned well, is safe and reliable and provides required levels of service</i></b>
<b>Community Plan Goal:</b>	<b><i>Facilitate investment in high quality community infrastructure necessary to a regional centre.</i></b>
<b>Community Plan Strategy:</b>	<b><i>Investigate and recommend appropriate management treatments for road safety and traffic management.</i></b>

**PURPOSE**

To request Council's endorsement of the amendments to the Special Conditions for the s.138 Roads Act 1993 permit approved by Council at the 28 May Ordinary Meeting.

**OFFICER'S RECOMMENDATION**

**Council delegates to the General Manager authority to sign the amended s.138 Roads Act 1993 approval generally in accordance with the revised Special Conditions set out in the report.**

**Moved:** \_\_\_\_\_ **Seconded:** \_\_\_\_\_

**BACKGROUND**

A report went to the May Ordinary meeting of Council detailing the works proposed by MACH Energy Australia Pty Ltd for the Mount Pleasant Operations Modification 4: Stage 2 Rail Loop works within the road reserves of Wybong Road, Skippens Road, Overton Road and Logues Lane. Council authorised the works within Council's road reserves under s.138 Roads Act 1993 approval. MACH Energy have requested amendment to some of the Special Conditions. In consideration of MACH Energy's request, a number of changes have been made to the Special Conditions and these changes are set before Council for consideration.

**CONSULTATION**

Council's Corporate Lawyer

MACH Energy General Manager

Manager Roads, Drainage & Technical Services

**CONSULTATION WITH COUNCILLOR SPOKESPERSON**

A copy of the report has been forwarded to Council's Councillor Spokesperson for Infrastructure, Councillor Woodruff, for review.

**REPORT**

A report, entitled 'Mt Pleasant Mod 4 Rail Loop – S138 Road Opening' was submitted to the 26 May 2020 Ordinary Council meeting. Council's resolution in relation to the report was::



**11.2 MT PLEASANT MOD 4 RAIL LOOP - S138 ROAD OPENING****1 RESOLVED** on the motion of Crs Scholes and Eades that:

1. Council delegates to the General Manager authority to sign the S.138 Roads Act 1993 approval generally in accordance with the draft Special Conditions set out in the report
2. The Consent be provisional on a suitable form of security to be agreed with the General Manager prior to the commencement of construction of the mine owned asset infrastructure within Council road reserves and the use of Council road reserves to access construction sites.
3. As outlined in the draft Special Conditions, insurance, detailed design and Traffic Management Plans are to be submitted and certified by suitably qualified consultants and accepted by the General Manager.

At the request of MACH Energy, a number of changes were considered by staff in consultation with their representatives and with the provision of preliminary design drawings. The amended conditions are attached to this report for the consideration of Council.

**OPTIONS**

Council may decide to:

1. endorse the delegation to the General Manager to approve the changes to the consent for works under Section 138 Roads Act 1993 as outlined in the amended "Special Conditions" attached to this report;
2. request modification to the conditions; or
3. refuse the delegation.

**CONCLUSION**

It is recommended that option 1 listed above be the preferred option.

**SOCIAL IMPLICATIONS**

There will be negative impacts for the users of Wybong Road east due to disruption to the normal use of the road by construction vehicles and controlled traffic conditions.

**FINANCIAL IMPLICATIONS**

The cost of all works undertaken will be the responsibility of the applicant. Council will cover its costs through the s.138 Road Act 1993 Consent Condition fees.

**POLICY IMPLICATIONS**

Nil known.

**STATUTORY IMPLICATIONS**

Council is the Roads Authority under the *Roads Act 1993* and is acting within its powers.

**LEGAL IMPLICATIONS**

Council is the Roads Authority under the *Roads Act 1993* and is acting within its powers.

**OPERATIONAL PLAN IMPLICATIONS**

Submission of the report complies in principle with item 19.3.4 of the Operational Plan: Investigate and recommend appropriate management treatments for road safety and traffic management'.

**RISK MANAGEMENT IMPLICATIONS**

Risks will be managed through a condition requiring the applicant to enter into a Deed of Agreement for the rail over bridge crossing Wybong Road, where it will be specified that only appropriately qualified and experienced consultants may submit and certify designs for infrastructure to be placed within Council's road reserves and Council staff acceptance of these designs and oversight of the construction. Works are required to comply with appropriate Standards, particularly Austroad standards and RMS guidelines 'Traffic Control at Work Sites' traffic management guidelines. A construction bond and removal will be imposed under the conditions of consent for the works at the time of the approval of a s.138 *Roads Act 1993* application.

**Draft Special Conditions Mod. 4 Rail Loop Construction**

1. Prior to commencement of any construction activity, all proposed temporary construction and permanent accesses are to be located, designed and constructed to meet minimum requirements of Muswellbrook Shire Council's rural property access drawing 0041/1 revision A dated 16/7/2012. Approved accesses are to be in accordance with those stipulated on Table 1 Construction and Permanent Access Gates.
2. All constructed temporary accesses are to be removed and decommissioned upon completion of construction and boundary fencing reinstated and all areas of disturbances within the road reserve to be reestablished to Auspec standard including top soiling, grass seeding, reshaping of table drains and scour protection where required to Council's satisfaction.
3. Gate 2- The use of gate 2 for access by general construction traffic is not permitted. Access to gate 2 from Wybong Road will only be permitted under traffic control for the purposes of facilitating access for large loads such as the delivery of train load out bin segments. The applicant is to notify Council staff of any proposed access.
4. Any oversize/overmass loads must have gained prior approval either through NHVR or directly to Council to travel on any local road.
5. The consent holder is required to facilitate other OSOM loads that have gained Council approval to travel on Wybong Road.
6. An approved Construction Traffic Management Plan (and Vehicle Movement Plan) has been approved by DPIE, post consultation with council. The permit holder is to discuss details, including construction traffic volumes, staging, timing and Traffic Guidance Scheme with Council's Technical Officer, Roads & Drainage. Information to be provided and accepted by Council prior to commencement of construction.
7. All works impacting public roads will require a Traffic Guidance Scheme (TGS). All TGS plans are to be developed as per RMS "Traffic Control at Worksites Conditions" and to be accepted by Council prior to use. Any changes to the TGS's will require submission to council prior to the implementation of the changes. MACH Energy are to coordinate with other adjacent MSC Section 138 permit holders for access requirements and coordination of TGS's, to Council's Satisfaction.
8. Maintenance Management Plan: The applicant must, at its sole risk and expense, carry out a maintenance regime, in accordance with its RMP (as modified) and to Council's satisfaction, for:
  - a. Wybong Road from Gates 20, 21 to 150m east of Logues Lane; and
  - b. The full length of Logues Lane;during construction of the works authorised by this consent and during any period in which any works remain in, above or under Council's public roads.

The Council may waive or alter the above requirements should the Council be satisfied that one or more of those requirements (in whole or in part) are no longer required.

8A. The applicant must maintain Overton Road during the period of construction of the works authorised by this approval to the satisfaction of Council.

9. Dilapidation report: A dilapidation report which records the conditions of the road pavement, drainage structures and other road related infrastructure must be compiled in consultation with Council's Manager Roads, Drainage & Technical Services for Wybong Road west of the main access of the MTP to Logues Lane, including Overton Road & Logues Lane prior to commencement of any site work. On practical completion of the project, the dilapidation report must be undertaken again and all defects identified. Any defects identified will be rectified to the pre-work conditions or better at the full cost to the applicant or in accordance with clause 14 of the general condition of the s.138 approval covered under "Restoration Fees". MACH Energy are to coordinate with other adjacent MSC s. 138 permit holders for dilapidation management of Council assets affected by both work parties, to Council's Satisfaction.
10. Time frames: A schedule of works will be provided to Council in the form of a Gant chart detailing individual key work activities associated with the overall work project and completion of work stages. Specifically, where this works impacts Council's road reserve. This is to be updated every 4 weeks and made available to Council. A liaison meeting should be scheduled as required.
11. To facilitate access to the interface of the rail, the applicant is required to construct Access 11 off Logues Lane. Prior to undertaking any construction of the access, the applicant will submit a design which addresses the following to Council's satisfaction:
  - a) confirms the appropriate location of the access to conform with Austroads standards for sight distance and Council's standard drawing for Rural Road Access,
  - b) stormwater drainage at the location.
12. Bonds for the construction and removal of the infrastructure within Skippens Road, Wybong Road and Logues Lane.  
 Prior to the commencement of any construction works or ancillary works within any of Council's road reserves, the consent holder must provide to Council unconditional bank guarantee(s) that are:
  - (i) in favour of Muswellbrook Shire Council;
  - (ii) for the amount(s) specified in the Schedule;
  - (iii) unlimited in time;
  - (iv) issued by a bank licensed to carry out business in Australia; and
  - (v) otherwise to Council's satisfaction.

The bank guarantees provided pursuant to this condition in respect of Logues Lane and Skippens Road will be held until such time as the road is legally closed and transferred to the applicant or the infrastructure is removed from the road reserve to Council's satisfaction. The bank guarantee for Wybong Road rail bridge infrastructure will be held

until such time as the infrastructure is removed from the Wybong Road reserve to Council's satisfaction.

13. The Consent Holder is to enter into a Deed of Agreement with Council for;
  - a. the construction of the Rail Over Bridge on Wybong Road and associated infrastructure to be constructed within the road reserve of Wybong and Overton Roads;
  - b. the design and reconstruction of Wybong Road from the MTP entrance to Overton Road to meet the standards of a RS2M road class and to Council's satisfaction; and
  - c. ancillary or incidental matters thereto.
14. School bus times Construction traffic is not to operate on Wybong Road during the school bus times. School bus times are to be confirmed by the applicant in consultation with the relevant bus companies and documented in the CTMP.
15. Inclement weather: Council as the road authority reserves the right to automatically suspend or decrease the construction traffic activity in the event of heavy or prolonged rain affecting their roads. Construction traffic movements are not permitted on public roads when fog conditions prevail and sight distances are compromised.
16. Prior to the use of Access 1 for the movement of Dump Trucks across Wybong Road the applicant is to submit the following to Council's satisfaction;
  - a. Geometrical Design for the temporary Access 1,
  - b. Pavement design to be constructed for the impacted section of Wybong Road to facilitate the road crossing by 40t Articulated Dump Trucks,
  - c. A road maintenance plan to be implemented on the impacted section of Wybong Road at the crossing for the duration of its use for construction purposes.
  - d. A pavement design for the permanent reinstatement of the affected section of Wybong Road at the crossing.
17. Culvert: The use of Wybong Road in relation to the Culvert at 2.7km from Kayuga Road requires a Structural Engineer's assessment of the culvert giving the load capacity of the structure and certifying its suitability for the nominated type and volume of construction traffic proposed.
18. The consent Holder is to advise Council's Technical Officer, Roads & Drainage on 0418 110010 fourteen days prior to commencing work to confirm inspections regime.
19. The Consent Holder is to advise directly affected residents and property owners of the program of Works that may affect access etc. at least one week in advance and incorporate reasonable access needs for those properties. The Consent Holder is to advise the general public of the Works by variable message board in the local area of the Works and advertisement in the local newspaper at least a week prior to site works.

20. The occupation of the Road should be undertaken in a safe manner, with adequate dust control.
21. The Consent Holder shall check for other services and is responsible for any damage to services and Council Infrastructure. Dial before you dig 1100.
22. The Consent does not constitute approval of any environmental impacts and the Consent Holder is responsible for any such impacts and any approval that may be required in relation to the Works.
23. The Consent Holder must ensure that mud and dust from the Works is not carried on to the Road. It is recommended that a device (from the Soil Management book) be used to remove such materials prior to entering the roads.
24. This Consent does not activate until all fees listed on the Schedule of Details and the Bond amounts and terms in the Schedule are specified in writing by Council.
25. Council reserves the right to reconsider and review the special conditions relating to the movement of construction vehicles on public road.

**11.5 CONCEPT DESIGN WILLIAMS BRIDGE MARTINDALE RD**

Attachments:	A. Concept Design - Williams Bridge B. Program
Responsible Officer:	Derek Finnigan - Deputy General Manager
Author:	Kellie Scholes - Manager - Roads, Drainage & Technical Services
Community Plan Issue:	<i>Our community's infrastructure is planned well, is safe and reliable and provides required levels of service</i>
Community Plan Goal:	<i>Maintain and continually improve community infrastructure across the Shire.</i>
Community Plan Strategy:	<i>Ensure all Infrastructure works activities undertake a competent Review of Environmental Factors (REF).</i>

**PURPOSE**

To seek endorsement of the Concept Design for the replacement of Williams Bridge at Ch 17.0km Martindale Road, and to inform Council of the program of work.

**OFFICER'S RECOMMENDATION**

1. Council endorse the Concept Design attached as Attachment A to the report prepared by Waeger Construction for the construction of Williams Bridge at chainage 17.0km on Martindale Road.
2. Council note the submitted program for the works, attached as Attachment B to the report.
3. Council acknowledge the sources of funding received and endorse the amendment of the 2020-21 Capital Budget for Williams Bridge to the amount of \$1,472,232, less amounts spent in 2019-20 financial year, to be funded from the sources nominated in the 'Financial Implication' section of the report.

Moved: \_\_\_\_\_ Seconded: \_\_\_\_\_

**BACKGROUND**

The construction of Williams Bridge is included in Council's 2019-20 Capital Works Program and Operational Plan. The work comprises the demolition, removal and disposal of the existing timber bridge, construction of a new single lane concrete bridge, installation of traffic barrier railing system and road work to construct the approaches. Tenders for the construction of the bridge were received and reported to Council at the 28 April 2020 Ordinary Council meeting, where Council resolved:

**19.2 TENDER 2019-2020-0411 - CONSTRUCTION OF WILLIAMS BRIDGE, MARTINDALE**

*RESOLVED on the motion of Crs Ward and Woodruff that:*

1. *The alternative tender submitted by Waeger Constructions Pty Ltd be accepted subject to a satisfactory financial assessment; and*
2. *The General Manager be delegated authority to sign and affix Council's seal to the Contract 2019-2020-0411.*



In accordance with Council's requirement for a concept design for a project with a value greater than \$75,000 to be reported to Council for endorsement, a design has been prepared and is submitted for Council's consideration.

**CONSULTATION**

Mr Jim Alexander – Structural Engineer Bridge Design Pty Ltd

**CONSULTATION WITH COUNCILLOR SPOKESPERSON**

Cr Scholes

Cr Woodruff

**REPORT**

Council accepted the Tender from Waeger Constructions Pty Ltd for the alternative design and construction of a four (4) 15.0m (approx.) span concrete bridge to replace the existing timber bridge at the existing Williams Bridge site, Martindale Road. In accordance with the engagement, Council has been provided with the alternate design attached as 'A' to the report, which was undertaken by Bridge Design Pty Ltd for Waeger Constructions.

The alternate design complies with the following standards and conforms to the requirements specified by Council;

- Bridge Design to AS 5100-2017
- Traffic loadings SM 1600, HLP400. Design Speed 100kph.
- Design heavy vehicle count 100vpd with a route factor of 0.5.
- Allowance for superimposed dead loads 1.7kPa (serviceability)
- Earthquake design classification BEDC – 2
- Minimum design lateral loading 500kN
- Traffic barriers: tribeam railing,
- Wind loading 40m/s
- Deck width 4.5m
- Deck height is maintained at the same Reduced Level as the existing Timber Bridge to be the 1.0%AEP (1 in 100 year flood height).

The new bridge will be 60.45m long with a clear width of 4.5m between barriers. The new bridge foundations will be driven 610 CHS steel piles with the top 6.0m of each pile hot-dipped galvanised to provide corrosion protection to the exposed pier piles and 1.5m of the buried portion of each pile, founded on the underlying bedrock. The design includes the use of a custom made precast concrete modular deck system which allows for the pre-casting and construction of almost all of the bridge components to be done off site. The Waeger Deck Bridge Modules consist of prestressed, precast concrete bridge girders cast concurrently with a topping slab. This system reduces the onsite construction time compared to traditional cast-in-situ bridge construction methods and has the advantage of reducing the timeframe that the sidetrack needs to be in use.

The horizontal location of the bridge has been moved slightly (1.5m to the south) to avoid the new piles clashing with an existing pier location and to provide a better clear span over the low flow channel. Other than this, the new bridge will be constructed to the same physical location as the existing road alignment.

**CONCLUSION**

The concept drawings provided meet the specifications provided by Council and the performance objectives of the conforming design. Council's endorsement of this concept will allow the contract with Waeger Constructions to proceed to the construction stage of the works, and to meet the delivery times shown on the program provided and attached as 'B' to the report.

**SOCIAL IMPLICATIONS**

Undertaking this work will improve road safety and the useability of Martindale road for the public. And will open up the Martindale Valley for residential, economic, tourism and emergency service purposes.

**FINANCIAL IMPLICATIONS**

The Tendered Lump Sum price from Waeger Constructions Pty Ltd. for the alternate design is \$932,500.00 (exc.GST), with the total cost of the project estimated at \$1,472,232.

The works are to be undertaken within the amounts allocated in the 2019-20 and 2020-21 Capital Expenditure Budgets. The available funding for Williams Bridge for 2020-21 is \$1,101,222 – this represents a shortfall of \$371,010 when compared to the total project cost above. The total project cost is proposed to be funded as follows:

\$1,101,222	Adopted 2019-20 Capital Carryover for Williams Bridge
\$100,000	NSW Bushfire Community Resilience Economic Recovery Fund Phase 1 (not yet budgeted in 2020-21, so it will need to be added to the 2020-21 budget)
\$271,010	Re allocate Council Revenue, currently allocated to Urban Road Renewal in the 2020-21 Budget
<b>\$1,472,232</b>	<b>Total</b>

It is recommended that Council acknowledge the sources of funding received and amend the 2020-21 Capital Budget for Williams Bridge to the amount of \$1,472,232 (less amounts spent in the 2019-20 financial year, with the Carry over amounts pending end of year adjustments with 2019-20 outstanding invoices) to be funded from the sources nominated above.

**POLICY IMPLICATIONS**

Nil Known

**STATUTORY IMPLICATIONS**

Nil Known

**LEGAL IMPLICATIONS**

Nil Known

**OPERATIONAL PLAN IMPLICATIONS**

The submission of the report complies with item 19.4.4 Seek grant funding for replacement of Williams Bridge (a timber bridge), Martindale Road.

**RISK MANAGEMENT IMPLICATIONS**

A risk assessment and contingency plan will be undertaken for implementation in the case of rain events during the on site construction period due to the existing bridge being removed and where the side track will be compromised. This will allow for risks associated with access to be addressed.



PO Box 9140,  
Wyoming, NSW 2250  
ph (02) 4322 0011  
ABN 63 145 429 063

Waeger Constructions  
Attention: Chris Purcell

Document Register/Issue: B2010-DI-01

Date: 26 June 2020

### Williams Bridge over Martindale Creek

Document number	Title	Rev	Issued to Waeger			
			Format	No	Format	No
B2010-D-01	Cover Sheet	P1	A3	pdf		
B2010-D-02	General Arrangement - Sheet A	P1	A3	pdf		
B2010-D-03	General Arrangement - Sheet B	P1	A3	pdf		
B2010-D-04	Substructure - Setout	P1	A3	pdf		
B2010-D-05	Piling - Details					
B2010-D-06	Abutment - Assembly Details	P1	A3	pdf		
B2010-D-07	Abutment Headstock - Reinforced Concrete Details	P1	A3	pdf		
B2010-D-08	Wing Walls - Reinforced Concrete Details	P1	A3	pdf		
B2010-D-09	Pier - Assembly Details	P1	A3	pdf		
B2010-D-10	Pier Headstock - Reinforced Concrete Details	P1	A3	pdf		
B2010-D-11	Deck - Assembly Details - Sheet A	P1	A3	pdf		
B2010-D-12	Deck - Assembly Details - Sheet B	P1	A3	pdf		
B2010-D-13	Deck Units - Concrete Details - Sheet A	P1	A3	pdf		
B2010-D-14	Deck Units - Concrete Details - Sheet B	P1	A3	pdf		

## Williams Bridge over Martindale Creek

Document number	Title	Rev	Issued to Waeger			
			Format	No	Format	No
B2010-D-15	Deck Units - Reinforcement Details - Sheet A	P1	A3	pdf		
B2010-D-16	Deck Units - Reinforcement Details - Sheet B					
B2010-D-17	Barriers, Bearings and Miscellaneous Steelwork Details - Sheet A	P1	A3	pdf		
B2010-D-18	Barriers, Bearings and Miscellaneous Steelwork Details - Sheet B	P1	A3	pdf		
B2010-D-19	Reinforcement - Bar Shapes	P1	A3	pdf		

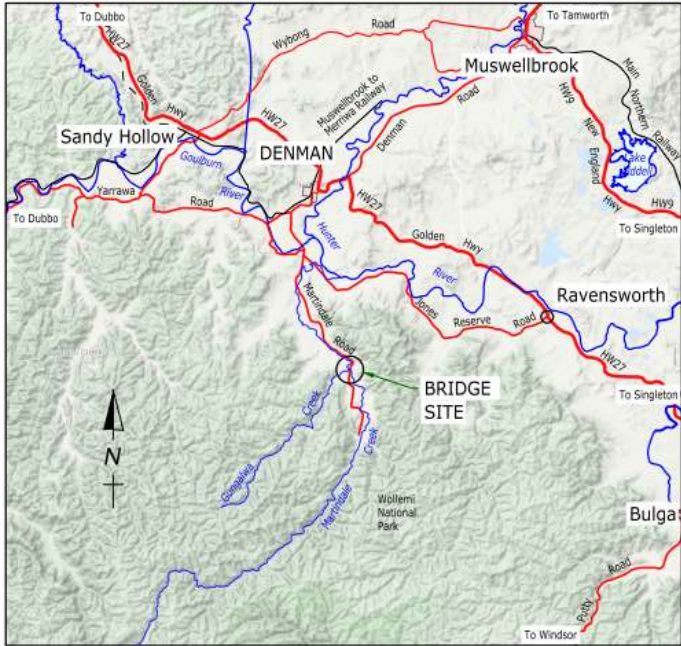
THIS DRAWING IS COPYRIGHT BRIDGE DESIGN PTY LTD. THE DRAWING AND THE ORIGINAL DESIGN CONCEPTS CONTAINED THEREIN SHALL NOT BE USED OR REPRODUCED IN ANY FORM OTHER THAN THE FORM SUPPLIED AND FOR THE PURPOSES OF CONSTRUCTION OF THE WORKS FOR WHICH IT WAS SPECIFICALLY PREPARED. UNAUTHORIZED TRANSMISSION TO ANY OTHER PARTY FOR THE PURPOSES OF APPROVAL, AND CONSTRUCTION OF THE WORKS IS PROHIBITED.

20/06/2020 2:25:50 PM jlm

\\drilling\2010\01 Cover.dgn

# WILLIAMS BRIDGE OVER MARTINDALE CREEK ON MARTINDALE ROAD AT 21 km SOUTH OF DENMAN

AUTHORITY - MUSWELLBROOK SHIRE COUNCIL



## LOCATION

1:600 000

Latitude: -32.5315°S  
Longitude: 150.7154°E

## DRAWING LIST

- D01 COVER SHEET
- D02 GENERAL ARRANGEMENT - SHEET A
- D03 GENERAL ARRANGEMENT - SHEET B
- D04 SUBSTRUCTURE - SETOUT
- D05 PILING - DETAILS
- D06 ABUTMENT - ASSEMBLY DETAILS
- D07 ABUTMENT HEADSTOCK - REINFORCED CONCRETE DETAILS
- D08 WING WALLS - REINFORCED CONCRETE DETAILS
- D09 PIER - ASSEMBLY DETAILS
- D10 PIER HEADSTOCK - REINFORCED CONCRETE DETAILS
- D11 DECK - ASSEMBLY DETAILS - SHEET A
- D12 DECK - ASSEMBLY DETAILS - SHEET B
- D13 DECK UNITS - CONCRETE DETAILS - SHEET A
- D14 DECK UNITS - CONCRETE DETAILS - SHEET B
- D15 DECK UNITS - REINFORCEMENT DETAILS - SHEET A
- D16 DECK UNITS - REINFORCEMENT DETAILS - SHEET B
- D17 BARRIERS, BEARINGS AND MISCELLANEOUS STEELWORK DETAILS - SHEET A
- D18 BARRIERS, BEARINGS AND MISCELLANEOUS STEELWORK DETAILS - SHEET B
- D19 REINFORCEMENT - BAR SHAPES

Replacement bridge - 2020.  
Bridge design to AS 5100-2017.  
Traffic loadings SM1600, HLP400. Design speed 100kph.  
Design heavy vehicle count 100 vpd. Route factor 0.5.  
Allowance for superimposed dead loads 1.7 kPa (serviceability).  
Earthquake design classification BEDC-2.  
Minimum design lateral loading 500kN.  
Traffic barriers: Kerb only.  
Wind Loading 40m/s.  
Waterway data:

Average Exceedance Probability	Flow m <sup>3</sup> /s	Velocity m/s	Water level (inc afflux)
1%	x	x	x
0.05%	x	x	x

### SAFETY IN DESIGN

The design of the works shown on these drawings does not present hazards that are not usually encountered in this type of construction.

~~The design of the works shown on these drawings does present hazards that are not usually encountered in this type of construction. Refer to the design report for details of these hazards.~~

PRELIMINARY DRAWING  
NOT FOR CONSTRUCTION

DRAWING SCALES TRUE AT A3



DESIGN: JRA  
VERIFIED: WJS  
DRAWING: WJS  
CHECKED:

MANAGER, BRIDGE DESIGN PTY LTD  
THIS DRAWING IS NOT APPROVED FOR CONSTRUCTION UNLESS SIGNED DIGITALLY ON THIS SHEET OR THE FIRST SHEET IN THIS SET OF DRAWINGS.

Martindale Road  
WILLIAMS BRIDGE OVER MARTINDALE CREEK  
at 21 km south of Denman  
COVER SHEET

Shire of Muswellbrook  
JOB NUMBER B2010  
DOCUMENT NUMBER D01  
SHEET 1 OF 18  
REVISION P1

100mm on original

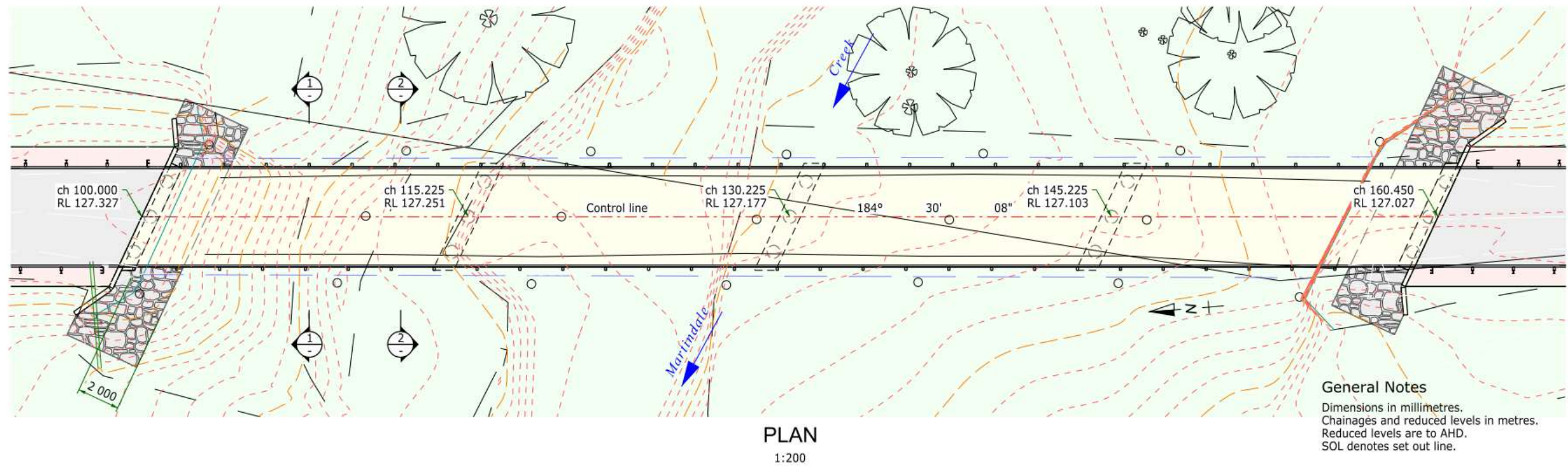
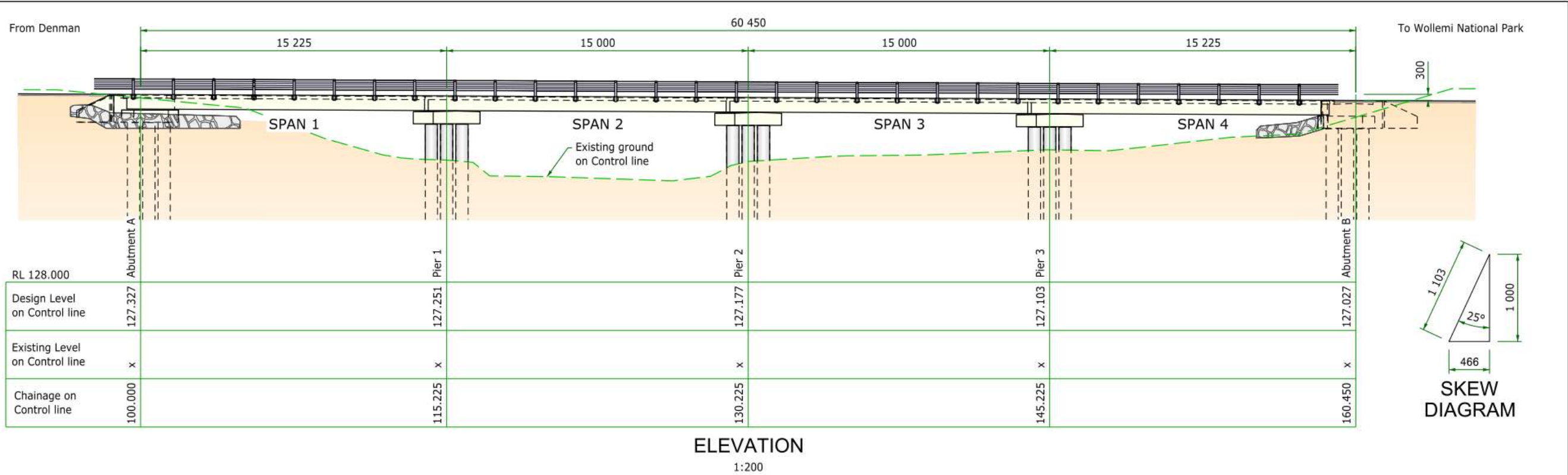
200mm on original



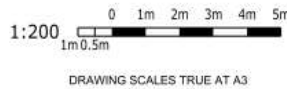
THIS DRAWING IS COPYRIGHT BRIDGE DESIGN PTY LTD. THE DRAWING AND THE ORIGINAL DESIGN CONCEPTS CONTAINED HEREIN SHALL NOT BE USED OR REPRODUCED IN ANY FORM OTHER THAN THE FORM SUPPLIED AND FOR THE PURPOSES OF CONSTRUCTION OF THE WORKS FOR WHICH IT WAS SPECIFICALLY PREPARED. UNAUTHORIZED TRANSMISSION TO ANY OTHER PARTY FOR THE PURPOSE OF APPROVAL AND CONSTRUCTION OF THE WORKS IS PROHIBITED.

20/06/2020 4:16:00 PM jlm

182010 D02 CA Sheet A.dgn



PRELIMINARY DRAWING  
NOT FOR CONSTRUCTION



PO Box 9140, Wyoming,  
NSW 2250  
ph 02 4322 0011  
ABN 63 145 429 063

DESIGN: JRA  
VERIFIED: WJS  
DRAWING: WJS  
CHECKED:

MANAGER, BRIDGE DESIGN PTY LTD  
THIS DRAWING IS NOT APPROVED FOR  
CONSTRUCTION UNLESS SIGNED  
DIGITALLY ON THIS SHEET OR THE FIRST  
SHEET IN THIS SET OF DRAWINGS.

Martindale Road  
WILLIAMS BRIDGE OVER MARTINDALE CREEK  
at 21 km south of Denman  
GENERAL ARRANGEMENT  
SHEET A

Shire of Muswellbrook  
JOB NUMBER B2010  
DOCUMENT NUMBER D02  
SHEET 2 OF 18  
REVISION P1

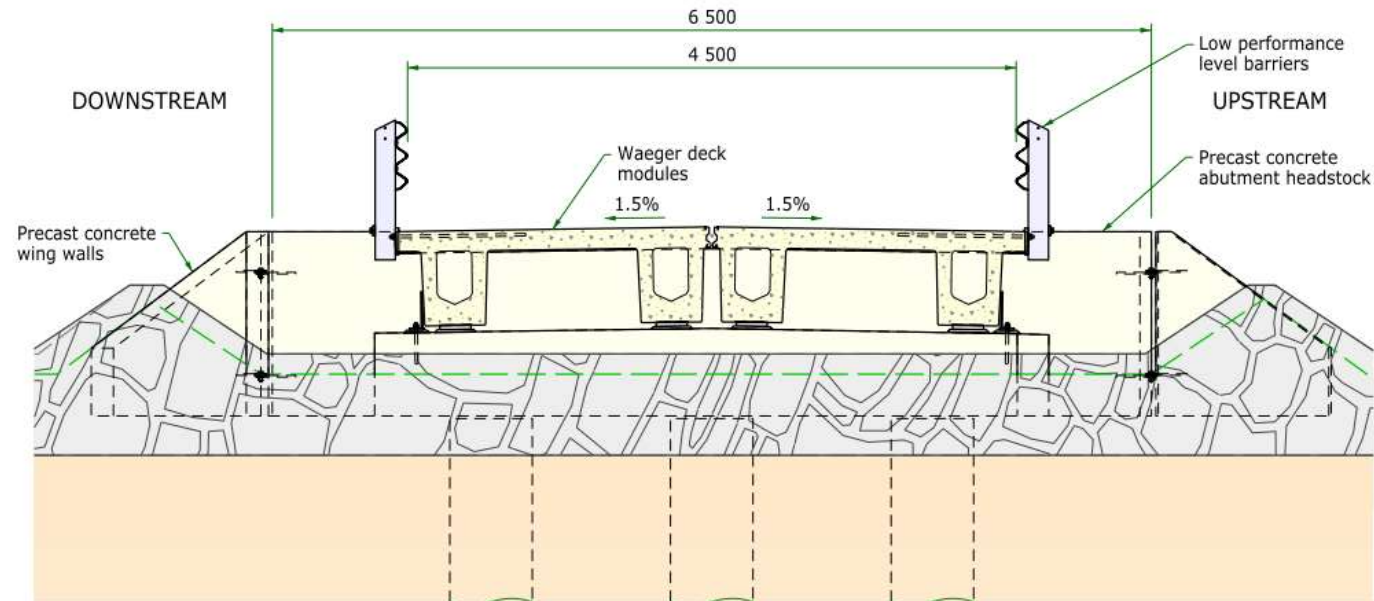
100mm on original

200mm on original

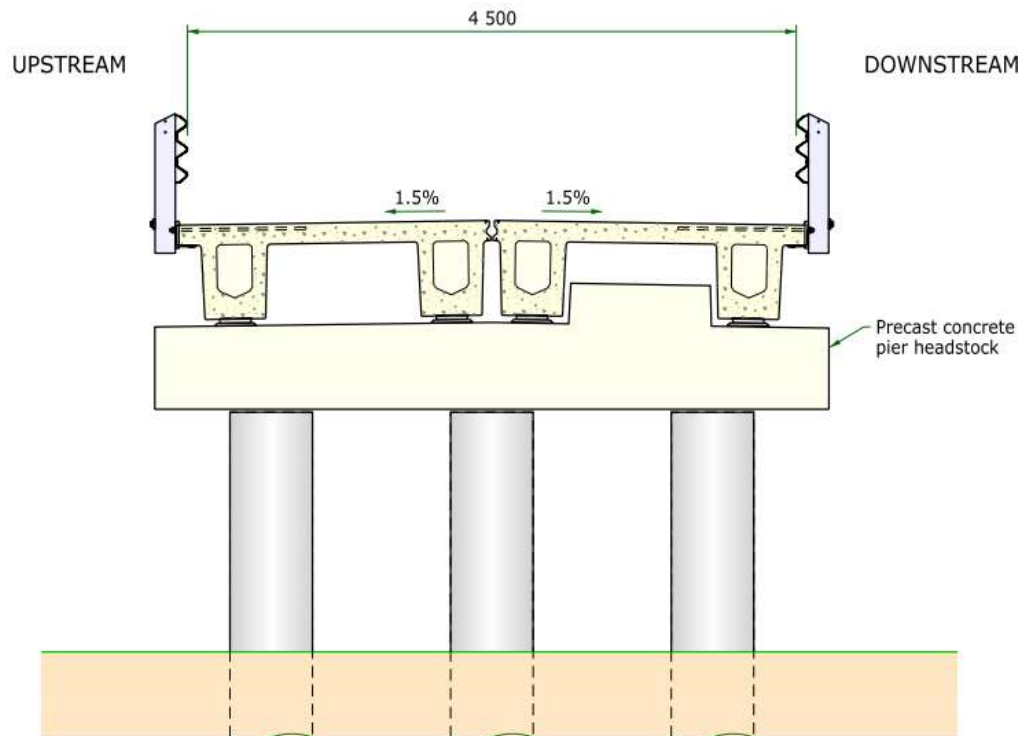
THIS DRAWING IS COPYRIGHT BRIDGE DESIGN PTY LTD. THE DRAWING AND THE ORIGINAL DESIGN CONCEPTS CONTAINED THEREIN SHALL NOT BE USED OR REPRODUCED IN ANY FORM OTHER THAN THE FORM SUPPLIED AND FOR THE PURPOSES OF CONSTRUCTION OF THE WORKS FOR WHICH IT WAS SPECIFICALLY PREPARED. UNAUTHORIZED TRANSMISSION TO ANY OTHER PARTY FOR THE PURPOSE OF APPROVAL AND CONSTRUCTION OF THE WORKS IS PROHIBITED.

20/06/2020 3:44:06 PM jlm

182010 D03 CA Sheet B.dgn



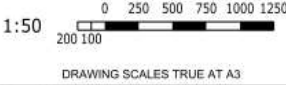
SECTION ①  
1:50



SECTION ②  
1:50

For General Notes - see sheet 2

PRELIMINARY DRAWING  
NOT FOR CONSTRUCTION



PO Box 9140, Wyoming,  
NSW 2250  
ph 02 4322 0011  
ABN 63 145 429 063

DESIGN: JRA  
VERIFIED: WJS  
DRAWING: WJS  
CHECKED:

MANAGER, BRIDGE DESIGN PTY LTD  
THIS DRAWING IS NOT APPROVED FOR  
CONSTRUCTION UNLESS SIGNED  
DIGITALLY ON THIS SHEET OR THE FIRST  
SHEET IN THIS SET OF DRAWINGS.

Martindale Road  
WILLIAMS BRIDGE OVER MARTINDALE CREEK  
at 21 km south of Denman  
GENERAL ARRANGEMENT  
SHEET B

Shire of Muswellbrook  
JOB NUMBER B2010  
DOCUMENT NUMBER D03  
SHEET 3 OF 18  
REVISION P1

100mm on original

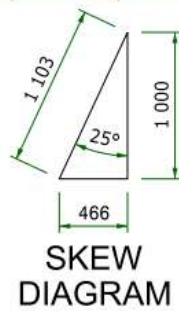
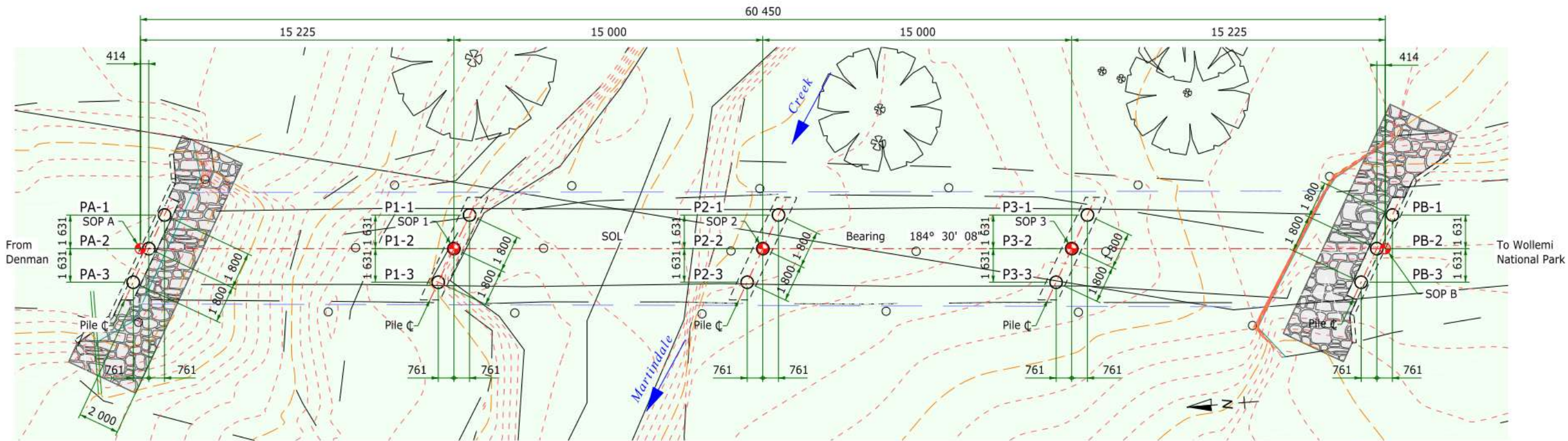
200mm on original



THIS DRAWING IS COPYRIGHT BRIDGE DESIGN PTY LTD. THE DRAWING AND THE ORIGINAL DESIGN CONCEPTS CONTAINED HEREIN SHALL NOT BE USED OR REPRODUCED IN ANY FORM OTHER THAN THE FORM SUPPLIED AND FOR THE PURPOSES OF CONSTRUCTION OF THE WORKS FOR WHICH IT WAS SPECIFICALLY PREPARED. UNAUTHORIZED TRANSMISSION TO ANY OTHER PARTY FOR THE PURPOSES OF APPROVAL AND CONSTRUCTION OF THE WORKS IS PROHIBITED.

3:48:55 PM 20/06/2020

...\\Drawing\\2010 D04 Setout.dgn



	Easting	Northing	RL	Notes
SOP A	X			
SOP 1				
SOP 2				
SOP 3				
SOP B				
STN 101				
STN 102				

TABLE 1 - SETOUT

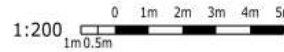
Pile No.	Easting	Northing	Ultimate Design Loads			Anticipated Toe Level	Cut off Level	Max Toe Level
			Vertical (kN)	Lateral (kN)	Bending (kNm)			
PA-1	X		x	x	x	x	X	x
PA-2								
PA-3								
P1-1								
P1-2								
P1-3								
P2-1								
P2-2								
P2-3								
P3-1								
P3-2								
P3-3								
PB-1								
PB-2								
PB-3								

TABLE 2 - PILE DETAILS

**General Notes**  
Dimensions in millimetres.  
Chainages and reduced levels in metres.  
Reduced levels are to AHD.  
Coordinates are to MGA.  
SOL denotes set out line.  
SOP denotes set out point.

**Foundation Notes**  
x

PRELIMINARY DRAWING  
NOT FOR CONSTRUCTION



DESIGN: JRA  
VERIFIED: WJS  
DRAWING: WJS  
CHECKED:

MANAGER, BRIDGE DESIGN PTY LTD.  
THIS DRAWING IS NOT APPROVED FOR CONSTRUCTION UNLESS SIGNED DIGITALLY ON THIS SHEET OR THE FIRST SHEET IN THIS SET OF DRAWINGS.

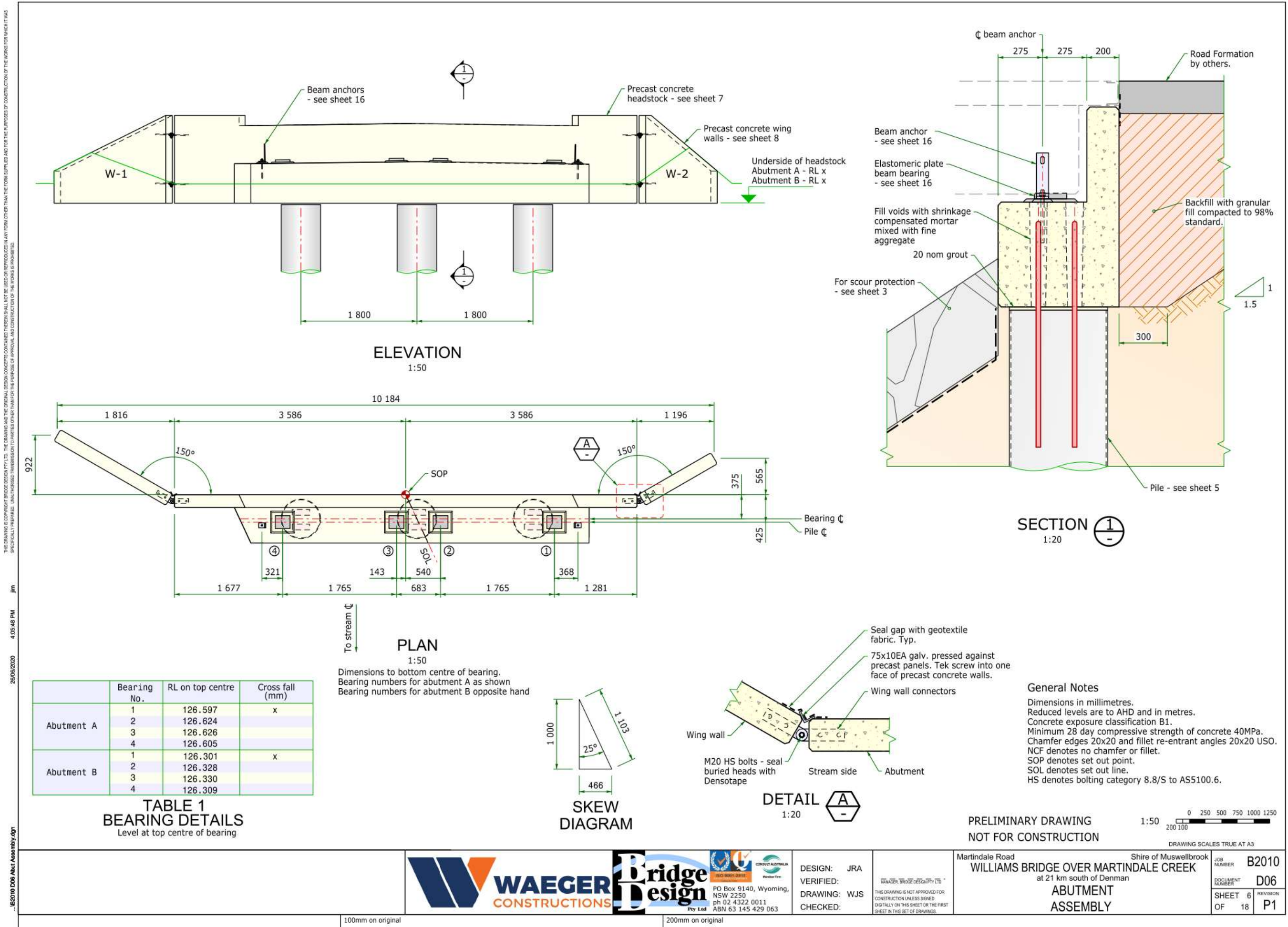
Martindale Road  
WILLIAMS BRIDGE OVER MARTINDALE CREEK  
at 21 km south of Denman  
SUBSTRUCTURE  
SETOUT

Shire of Muswellbrook  
JOB NUMBER B2010  
DOCUMENT NUMBER D04  
SHEET 4 OF 18  
REVISION P1

100mm on original

200mm on original

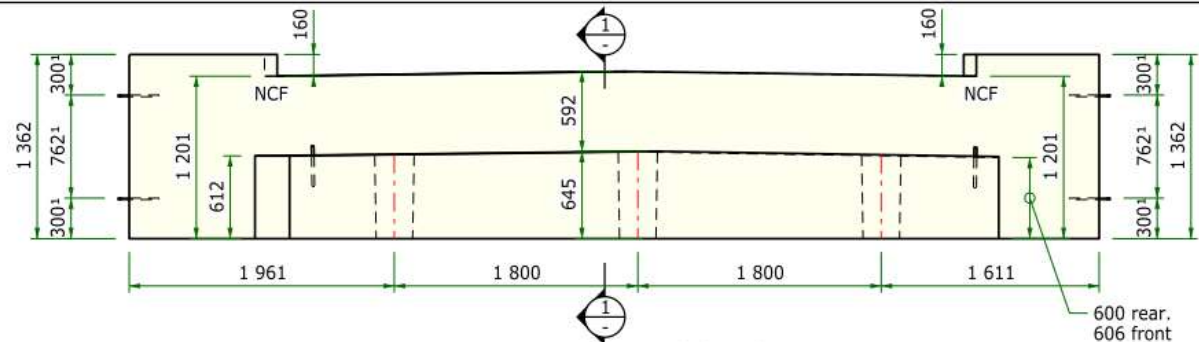






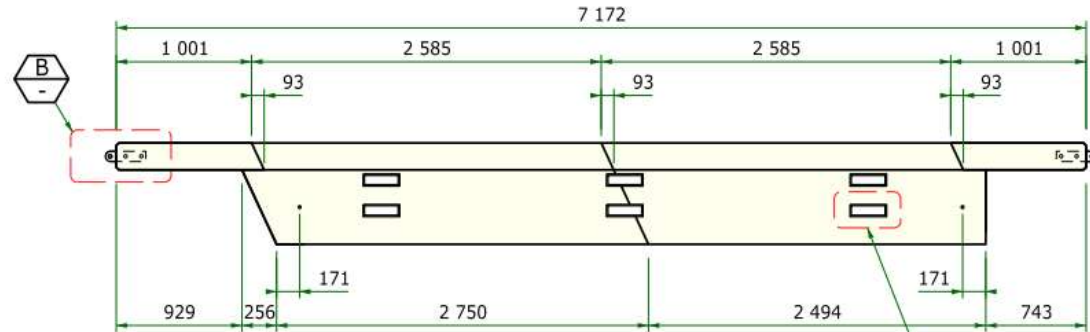
THIS DRAWING IS COPYRIGHT BRIDGE DESIGN PTY LTD. THE DRAWING AND THE ORIGINAL DESIGN CONCEPTS CONTAINED THEREIN SHALL NOT BE USED OR REPRODUCED IN ANY FORM OTHER THAN THE FORM SUPPLIED AND FOR THE PURPOSES OF CONSTRUCTION OF THE WORKS FOR WHICH IT WAS SPECIFICALLY PREPARED. UNAUTHORIZED TRANSMISSION TO ANY OTHER PARTY FOR THE PURPOSES OF APPROVAL, AND CONSTRUCTION OF THE WORKS IS PROHIBITED.

20/06/2020 4:00:08 PM JB2010 D07 Abut Headstock RC.dgn



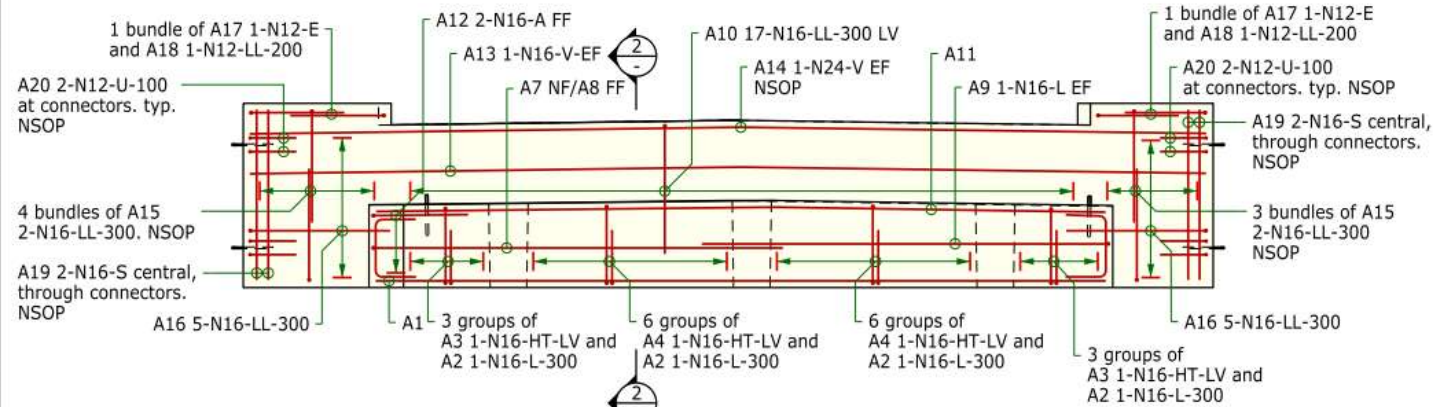
ELEVATION - CONCRETE

1:50  
1 Dimensions to top of wing wall connector plates



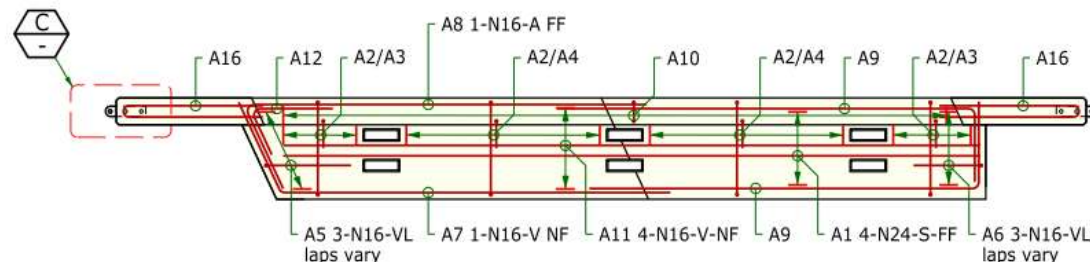
PLAN - CONCRETE

1:50



ELEVATION - REINFORCEMENT

1:50

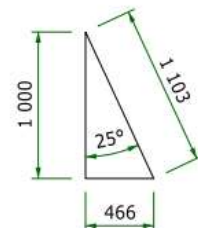


PLAN - REINFORCEMENT

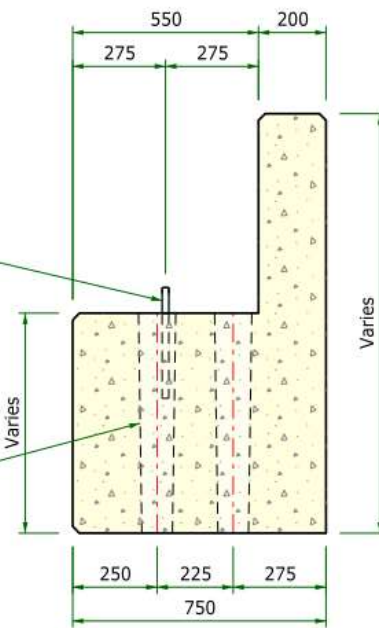
1:50

M20 HS threaded rod x 300 long. Cast in or drilled on site. 100 projection

1 in 60 void taper. Coat void formers with retarder and roughen with pressure spray

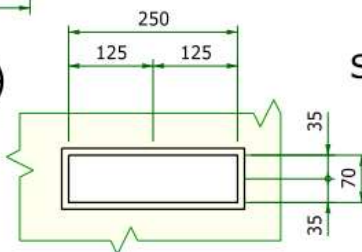


SKEW DIAGRAM



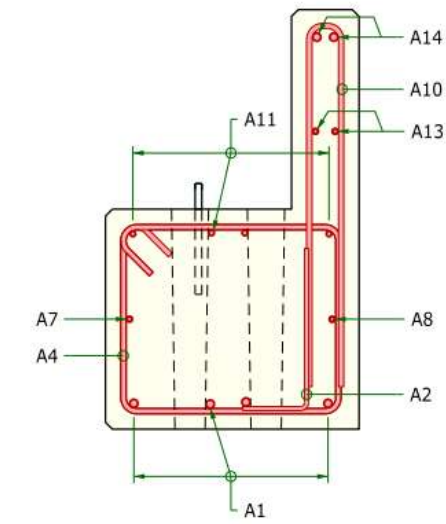
SECTION 1

1:20



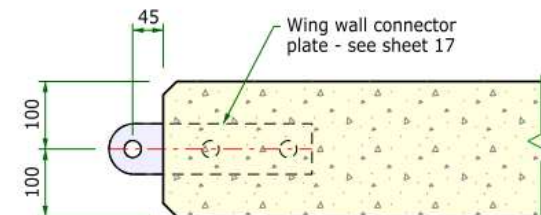
DETAIL A

1:10



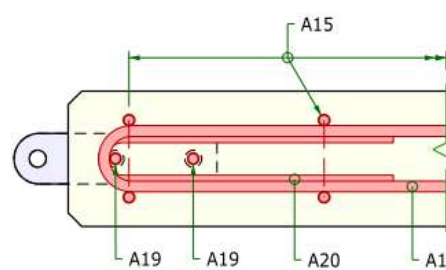
SECTION 2

1:20



DETAIL B

1:10



DETAIL C

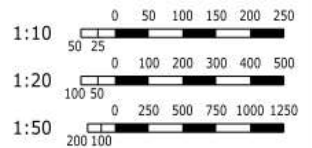
1:10

General Notes

Dimensions in millimetres.  
Concrete exposure classification B1.  
Minimum 28 day compressive strength of concrete 40MPa.  
Chamfer edges 20x20 and fillet re-entrant angles 20x20 USO.  
NCF denotes no chamfer or fillet.  
Nominal cover to reinforcement nearest to the concrete surface 45mm USO.  
FF far face NF near face  
EF each face LV length varies.  
USO unless specified otherwise.  
NSOP not shown on plan.  
Lap lengths to be as follows USO:

Bar size	N12	N16	N20	N24	N28	N32
Horizontal bars with >300mm of concrete cast below the bar	450	600	...	...	1570	...
Other bars	350	470	...	...	1210	...

Development length to be 100% of the lap length above USO.  
Reinforcement may be displaced slightly to clear embedded objects, holes and recesses.



DRAWING SCALES TRUE AT A3

PRELIMINARY DRAWING  
NOT FOR CONSTRUCTION



DESIGN: JRA  
VERIFIED: WJS  
DRAWING: WJS  
CHECKED:

MANAGER, BRIDGE DESIGN PTY LTD  
THIS DRAWING IS NOT APPROVED FOR CONSTRUCTION UNLESS SIGNED DIGITALLY ON THIS SHEET OR THE FIRST SHEET IN THIS SET OF DRAWINGS.

Martindale Road  
WILLIAMS BRIDGE OVER MARTINDALE CREEK  
at 21 km south of Denman  
ABUTMENT HEADSTOCK  
REINFORCED CONCRETE DETAILS

Shire of Muswellbrook  
JOB NUMBER B2010  
DOCUMENT NUMBER D07  
SHEET 7 OF 18  
REVISION P1

100mm on original

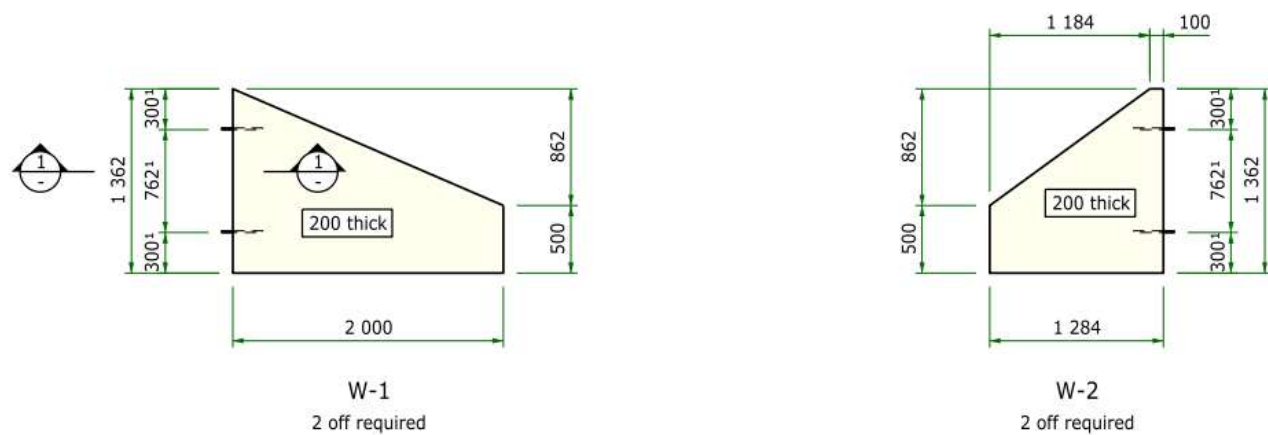
200mm on original



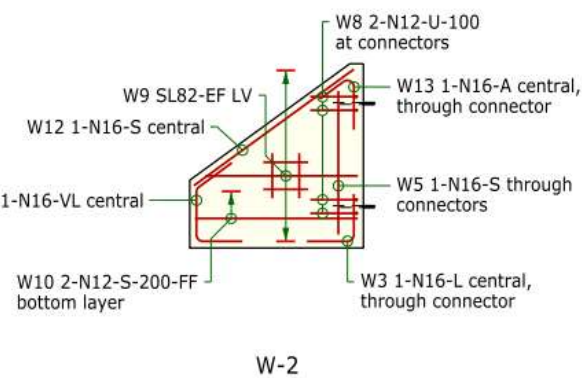
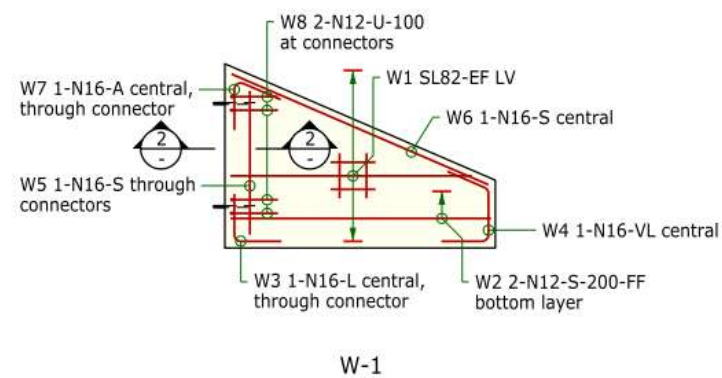
THIS DRAWING IS COPYRIGHT BRIDGE DESIGN PTY LTD. THE DRAWING AND THE ORIGINAL DESIGN CONCEPTS CONTAINED THEREIN SHALL NOT BE USED OR REPRODUCED IN ANY FORM OTHER THAN THE FORM SUPPLIED AND FOR THE PURPOSES OF CONSTRUCTION OF THE WORKS FOR WHICH IT WAS SPECIFICALLY PREPARED. UNAUTHORIZED TRANSMISSION TO ANY OTHER PARTY FOR THE PURPOSE OF APPROVAL AND CONSTRUCTION OF THE WORKS IS PROHIBITED.

20/06/2020 4:06:30 PM jfm

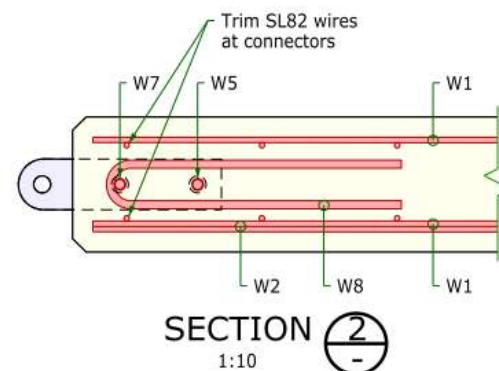
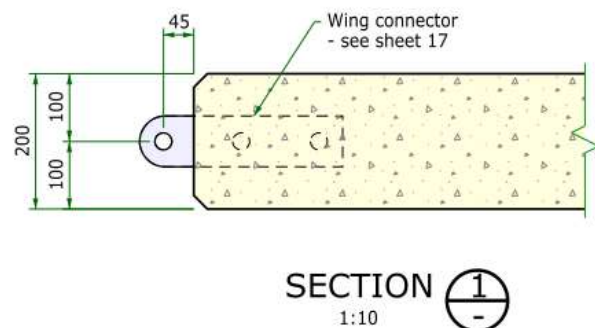
182010 D08 Wing walls RC.dgn



CONCRETE  
1:50



REINFORCEMENT  
1:50



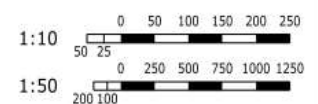
ALL PANELS ARE SHOWN OUTER  
FACE DOWN, IN CASTING  
POSITION.  
ALL PANELS TO BE CLEARLY  
LABELLED ON INSIDE FACES

General Notes

Dimensions in millimetres.  
Concrete exposure classification B1.  
Minimum 28 day compressive strength of concrete 40 MPa.  
Chamfer edges 20x20 and fillet re-entrant angles 20x20 USO.  
NCF denotes no chamfer or fillet.  
USO unless specified otherwise.  
1 Dimensions to bottom of connector plates.  
HS denotes grade 8.8/s to AS5100.6.  
Nominal cover to reinforcement nearest to the concrete surface 35mm USO. Use rigid forms and intense compaction.  
FF far face NF near face  
EF each face LV length varies.  
USO unless specified otherwise.  
NSOP not shown on plan.  
Lap lengths to be as follows USO:

Bar size	N12	N16	N20	N24	N28	N32
Horizontal bars with >300mm of concrete cast below the bar	460	610	...	...	...	...
Other bars	350	470	...	...	...	...

Development length to be 100% of the lap length above USO.  
Reinforcement may be displaced slightly to clear embedded objects, holes and recesses.



PRELIMINARY DRAWING  
NOT FOR CONSTRUCTION

DRAWING SCALES TRUE AT A3

**WAEGER CONSTRUCTIONS**

**Bridge Design**

PO Box 9140, Wyoming, NSW 2250  
ph 02 4322 0011  
ABN 63 145 429 063

DESIGN: JRA  
VERIFIED: JRA  
DRAWING: WJS  
CHECKED: WJS

MANAGER, BRIDGE DESIGN PTY LTD

THIS DRAWING IS NOT APPROVED FOR CONSTRUCTION UNLESS SIGNED DIGITALLY ON THIS SHEET OR THE FIRST SHEET IN THIS SET OF DRAWINGS.

Martindale Road  
**WILLIAMS BRIDGE OVER MARTINDALE CREEK**  
at 21 km south of Denman  
**WING WALLS**  
**REINFORCED CONCRETE DETAILS**

Shire of Muswellbrook

JOB NUMBER: B2010  
DOCUMENT NUMBER: D08  
SHEET 8 OF 18  
REVISION: P1

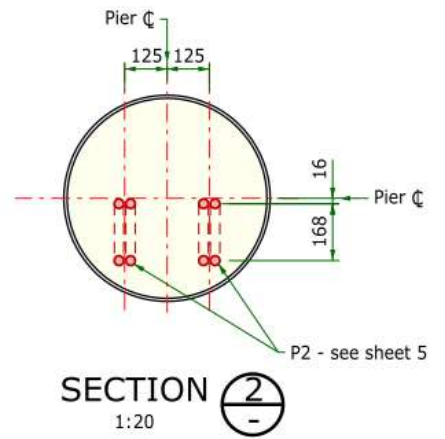
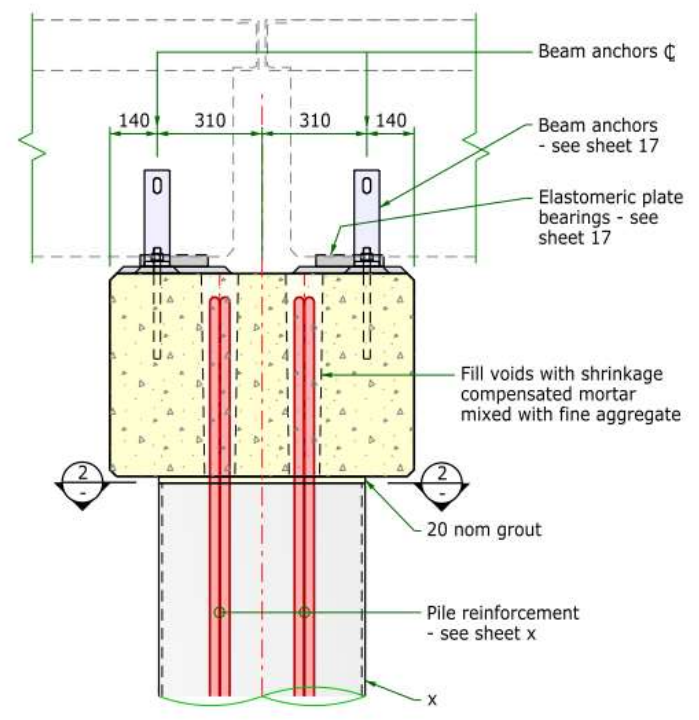
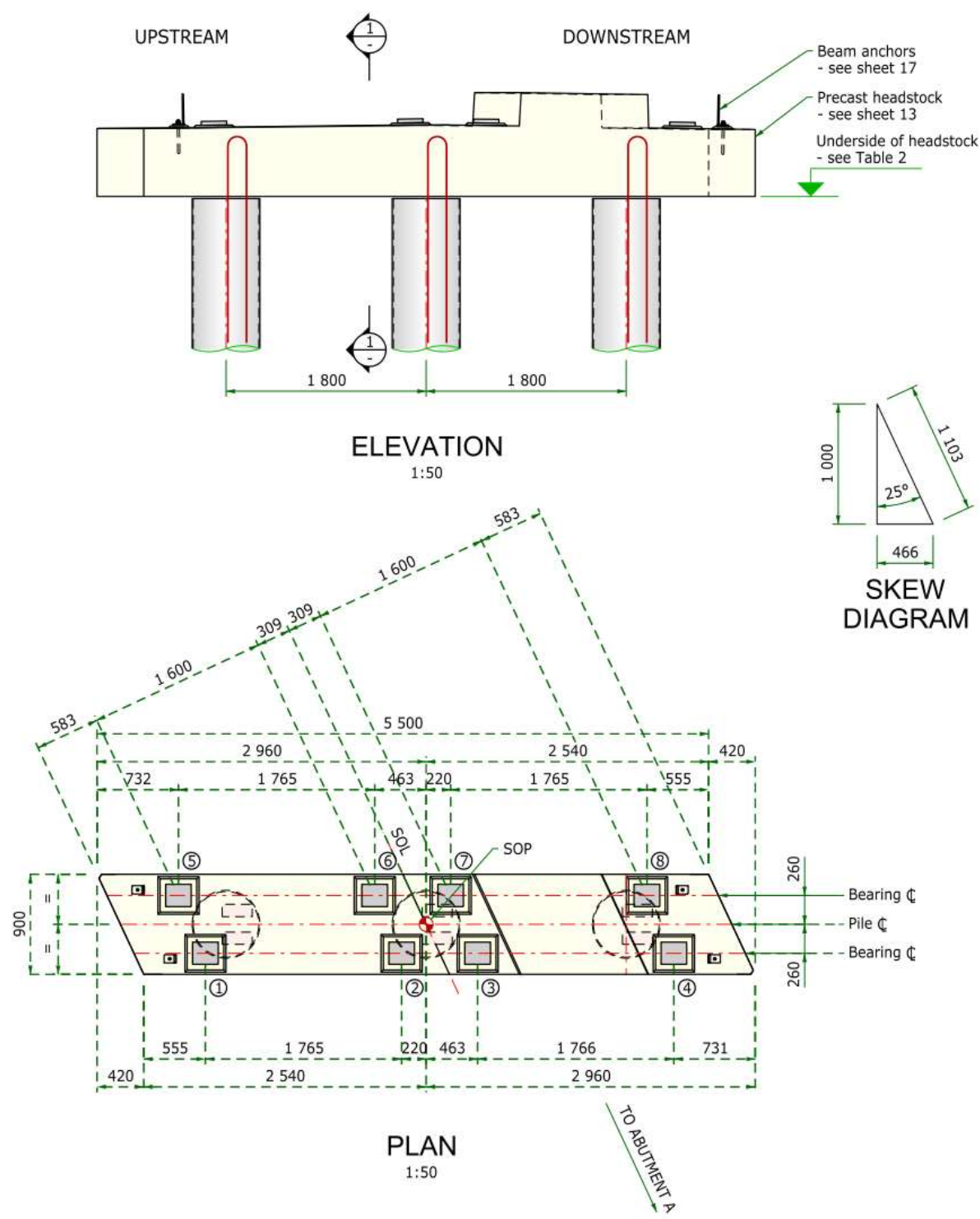
100mm on original

200mm on original

THIS DRAWING IS COPYRIGHT BRIDGE DESIGN PTY LTD. THE DRAWING AND THE ORIGINAL DESIGN CONCEPTS CONTAINED THEREIN SHALL NOT BE USED OR REPRODUCED IN ANY FORM OTHER THAN THE FORM SUPPLIED AND FOR THE PURPOSES OF CONSTRUCTION OF THE WORKS FOR WHICH IT WAS SPECIFICALLY PREPARED. UNAUTHORIZED TRANSMISSION TO ANY OTHER PARTY FOR THE PURPOSES OF APPROVAL AND CONSTRUCTION OF THE WORKS IS PROHIBITED.

20/06/2020 4:08:51 PM jlm

18/02/2010 D09 Pier Assembly.dgn



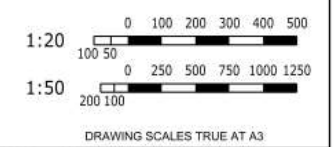
	Bearing No.	RL on top centre	Cross fall (mm)
Pier 1	1	X	X
	2		
	3		
	4		
	5		
	6		
	7		
	8		
Pier 2	1		
	2		
	3		
	4		
	5		
	6		
	7		
	8		
Pier 3	1		
	2		
	3		
	4		
	5		
	6		
	7		
	8		

TABLE 1  
BEARING DETAILS  
Level at top centre of bearing

Pier No.	Underside of headstock RL 'X'
1	X
2	
3	

TABLE 2  
PIER HEADSTOCK LEVELS  
Level at top centre of bearing

**General Notes**  
Dimensions in millimetres.  
Concrete exposure classification B1.  
Minimum 28 day compressive strength of concrete and mortar 40MPa.  
Chamfer edges 20x20 and fillet re-entrant angles 20x20 USO.  
NCF denotes no chamfer or fillet



PRELIMINARY DRAWING  
NOT FOR CONSTRUCTION

DRAWING SCALES TRUE AT A3



DESIGN: JRA  
VERIFIED: WJS  
DRAWING: WJS  
CHECKED:

Martindale Road  
WILLIAMS BRIDGE OVER MARTINDALE CREEK  
at 21 km south of Denman  
PIER  
ASSEMBLY DETAILS

JOB NUMBER	B2010
DOCUMENT NUMBER	D09
SHEET OF	9 18
REVISION	P1

100mm on original

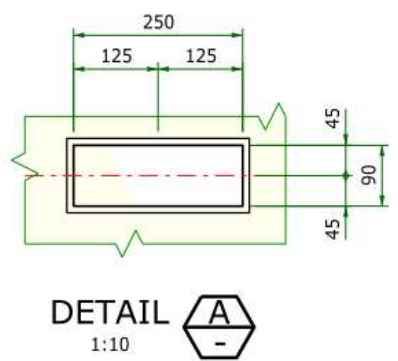
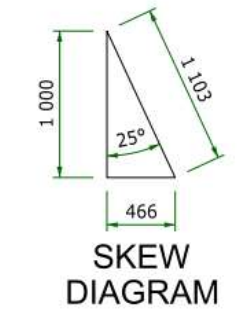
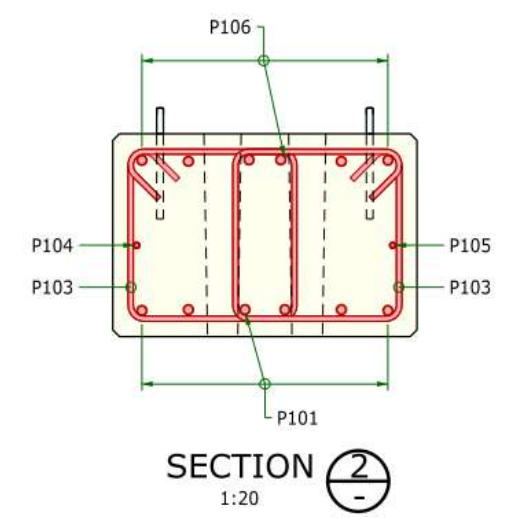
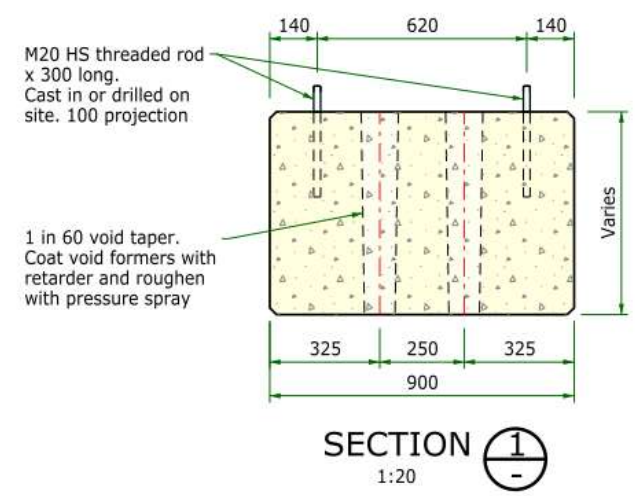
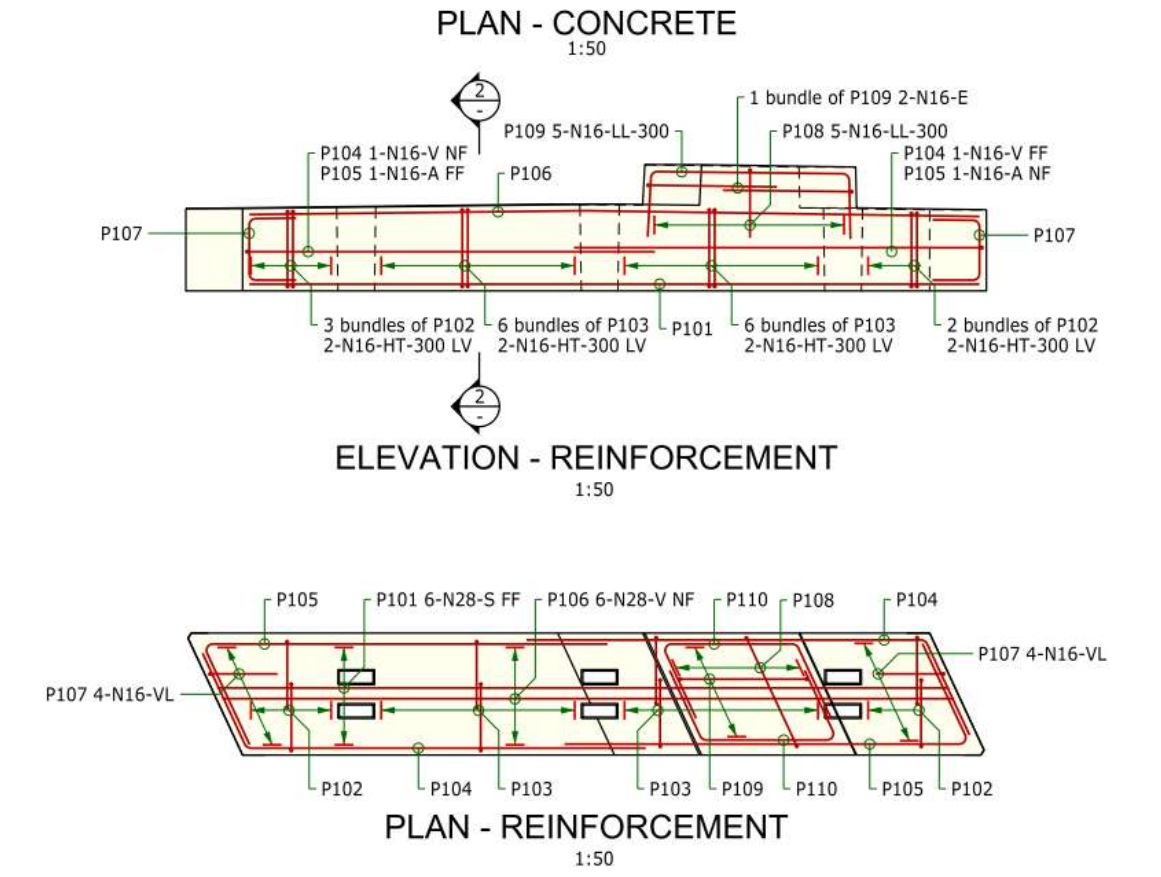
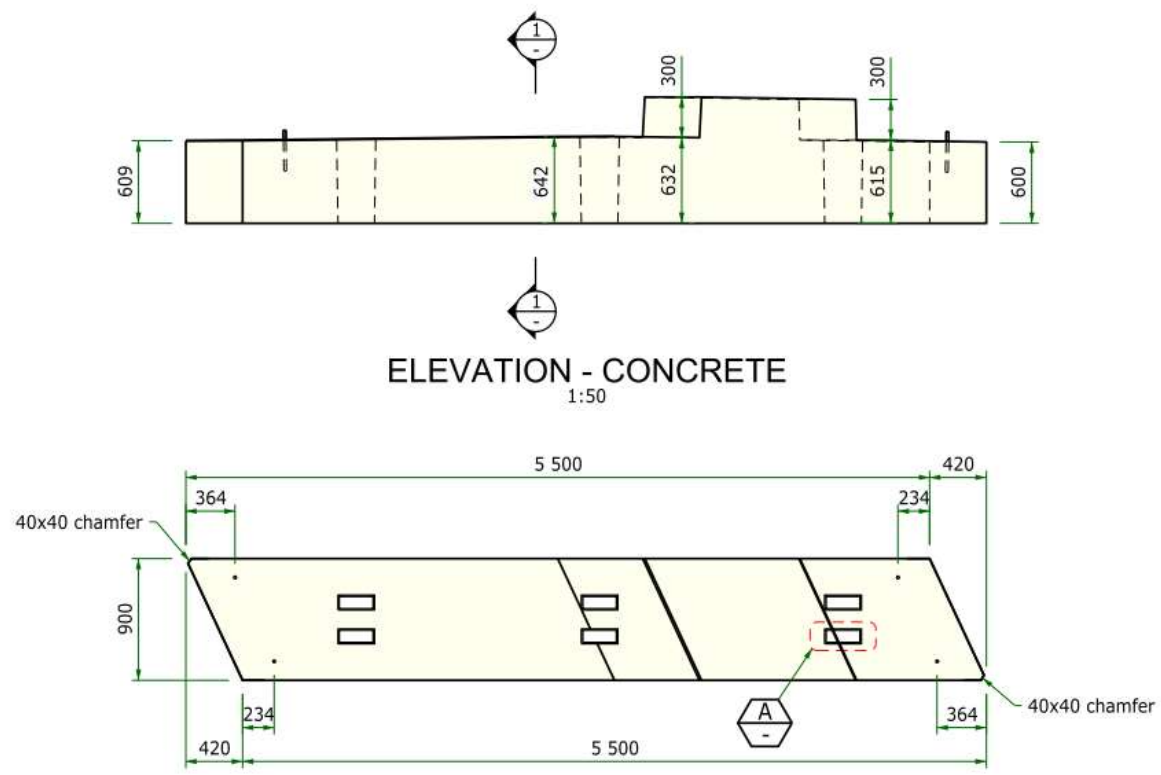
200mm on original



THIS DRAWING IS COPYRIGHT BRIDGE DESIGN PTY LTD. THE DRAWING AND THE ORIGINAL DESIGN CONCEPTS CONTAINED THEREIN SHALL NOT BE USED OR REPRODUCED IN ANY FORM OTHER THAN THE FORM SUPPLIED AND FOR THE PURPOSES OF CONSTRUCTION OF THE WORKS FOR WHICH IT WAS SPECIFICALLY PREPARED. UNAUTHORIZED TRANSMISSION TO ANY OTHER PARTY FOR THE PURPOSES OF APPROVAL AND CONSTRUCTION OF THE WORKS IS PROHIBITED.

20/06/2020 4:00:01 PM jrm

...\\B2010 D10 Pier Headstock RC.dgn

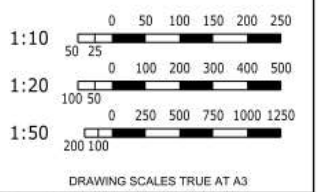


**General Notes**

Dimensions in millimetres.  
Concrete exposure classification B1.  
Minimum 28 day compressive strength of concrete 40MPa.  
Chamfer edges 20x20 and fillet re-entrant angles 20x20 USO.  
NCF denotes no chamfer or fillet.  
Nominal cover to reinforcement nearest to the concrete surface 45mm USO.  
FF far face NF near face  
EF each face LV length varies.  
USO unless specified otherwise.  
NSOP not shown on plan.  
Lap lengths to be as follows USO:

Bar size	N12	N16	N20	N24	N28	N32
Horizontal bars with >300mm of concrete cast below the bar	450	600	...	...	1570	...
Other bars	350	470	...	...	1210	...

Development length to be 100% of the lap length above USO.  
Reinforcement may be displaced slightly to clear embedded objects, holes and recesses.



PRELIMINARY DRAWING  
NOT FOR CONSTRUCTION

**WAEGER CONSTRUCTIONS**

**Bridge Design**

PO Box 9140, Wyoming, NSW 2250  
ph 02 4322 0011  
ABN 63 145 429 063

DESIGN: JRA  
VERIFIED: WJS  
DRAWING: WJS  
CHECKED:

MANAGER, BRIDGE DESIGN PTY LTD.  
THIS DRAWING IS NOT APPROVED FOR CONSTRUCTION UNLESS SIGNED DIGITALLY ON THIS SHEET OR THE FIRST SHEET IN THIS SET OF DRAWINGS.

Martindale Road WILLIAMS BRIDGE OVER MARTINDALE CREEK at 21 km south of Denman PIER HEADSTOCK REINFORCED CONCRETE DETAILS	Shire of Muswellbrook	JOB NUMBER B2010
	DOCUMENT NUMBER D10	
	SHEET 10 OF 18	REVISION P1

100mm on original

200mm on original

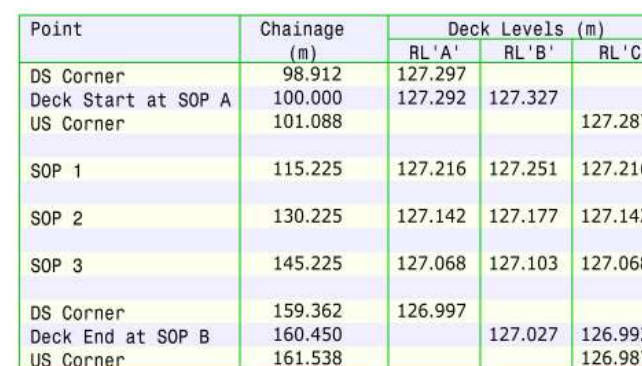
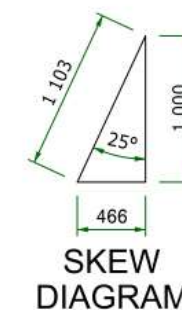
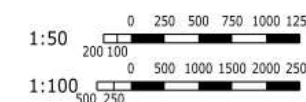


TABLE 1 - DECK LEVELS



### General Notes

Dimensions in millimetres.  
Reduced levels in metres and to AHD.  
Grout minimum 40MPa at 7 days.  
Bolting category 4.6/S to AS5100.6 USO.  
HS denotes bolting category 8.8/S to AS5100.  
NCF denotes no chamfer or fillet.



PRELIMINARY DRAWING  
NOT FOR CONSTRUCTION

DRAWING SCALES TRUE AT A3



**Bridge Design**

PO Box 9140, Wyoming  
NSW 2250  
ph 02 4322 0011  
fax 02 4322 0062

DESIGN: JRA  
VERIFIED:  
DRAWING: WJS  
CHECKED:

MANAGER, BRIDGE DESIGN PTY LTD

Martindale Road Shire of Muswellbrook  
**WILLIAMS BRIDGE OVER MARTINDALE CREEK**  
 at 21 km south of Denman  
**DECK**  
**ASSEMBLY DETAILS - SHEET A**

JOB NUMBER	B2010	
DOCUMENT NUMBER	D11	
SHEET 11 OF 18	REVISION P1	

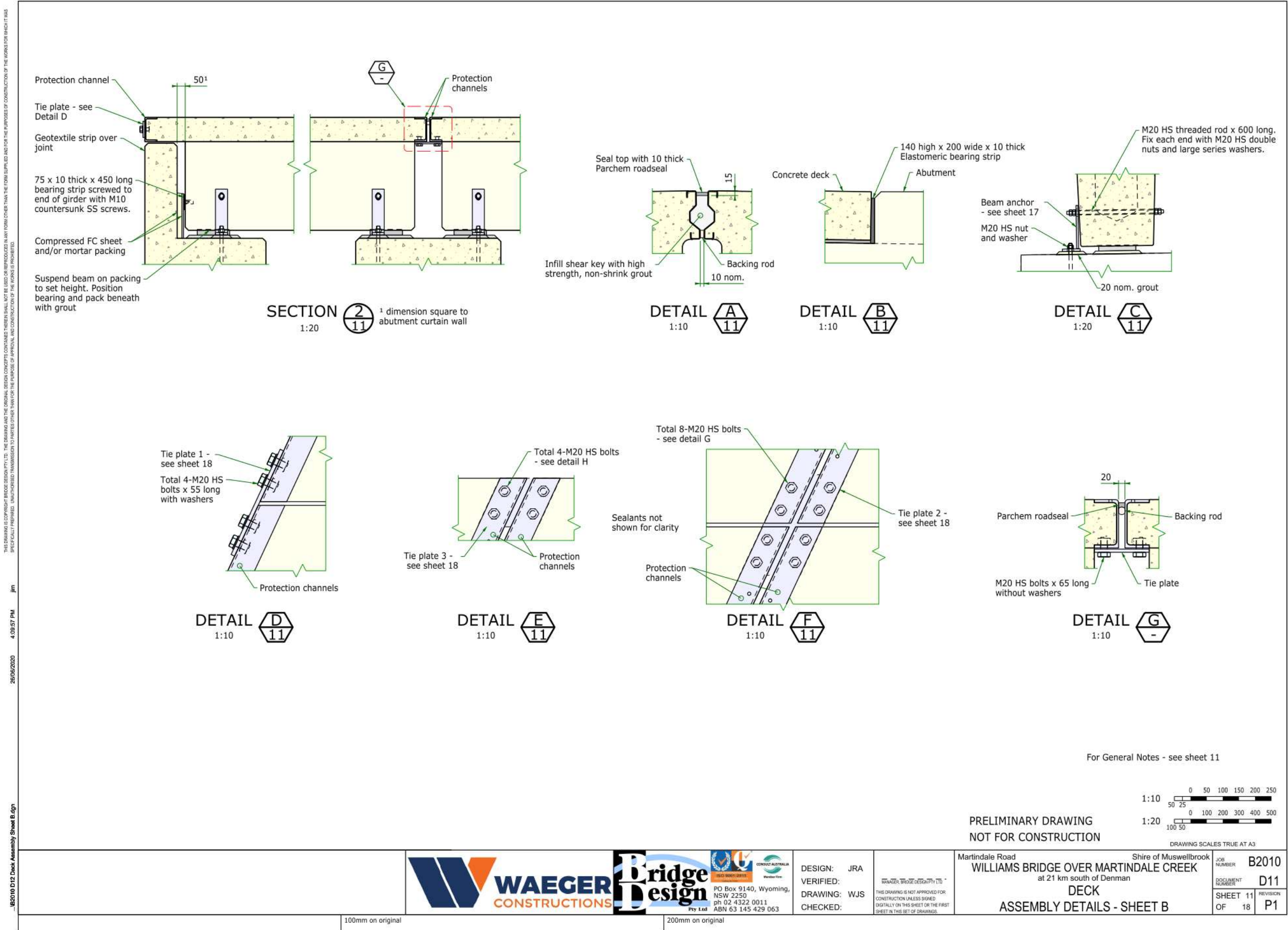
100mm on original

200mm on original

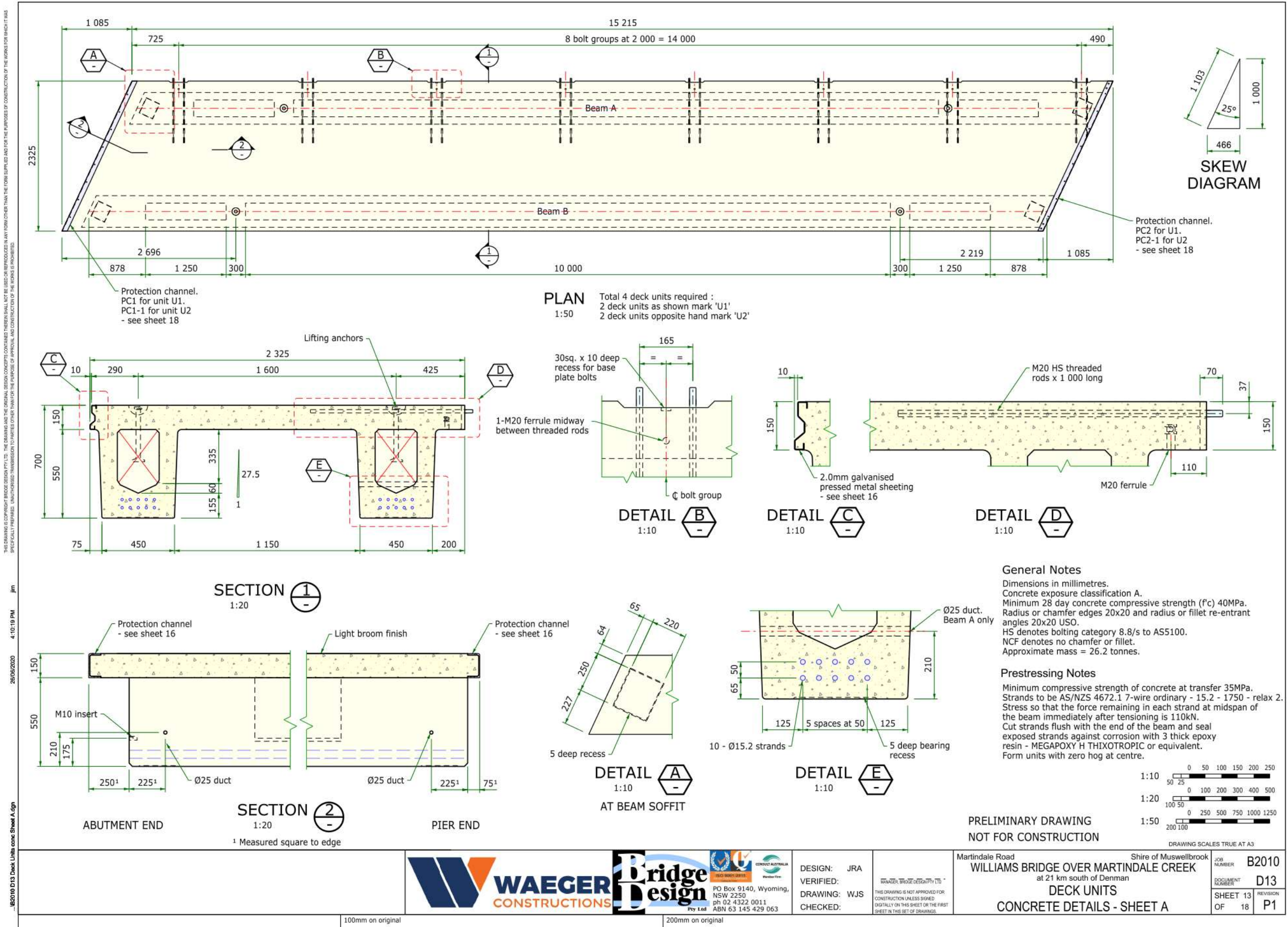
THIS DRAWING IS COPYRIGHT BRIDGE DESIGN SYSTEM, LTD. THE DRAWING AND THE ORIGINAL DESIGN CONCEPTS CONTAINED HEREIN SHALL NOT BE USED OR REPRODUCED IN ANY FORM OTHER THAN THE FORM SUBMITTED AND FOR THE PURPOSES OF CONSTRUCTION OF THE WORKS FOR WHICH IT WAS SPECIFICALLY PREPARED. UNAUTHORIZED TRANSMISSION TO PARTIES OTHER THAN THE DESIGNER OF APPROVAL AND CONTRACTOR SHALL BE PROHIBITED.

26/06/2020 4:08:23 PM  
 V2010 D11 Deck Assembly Sheet A.dgn  
 10m

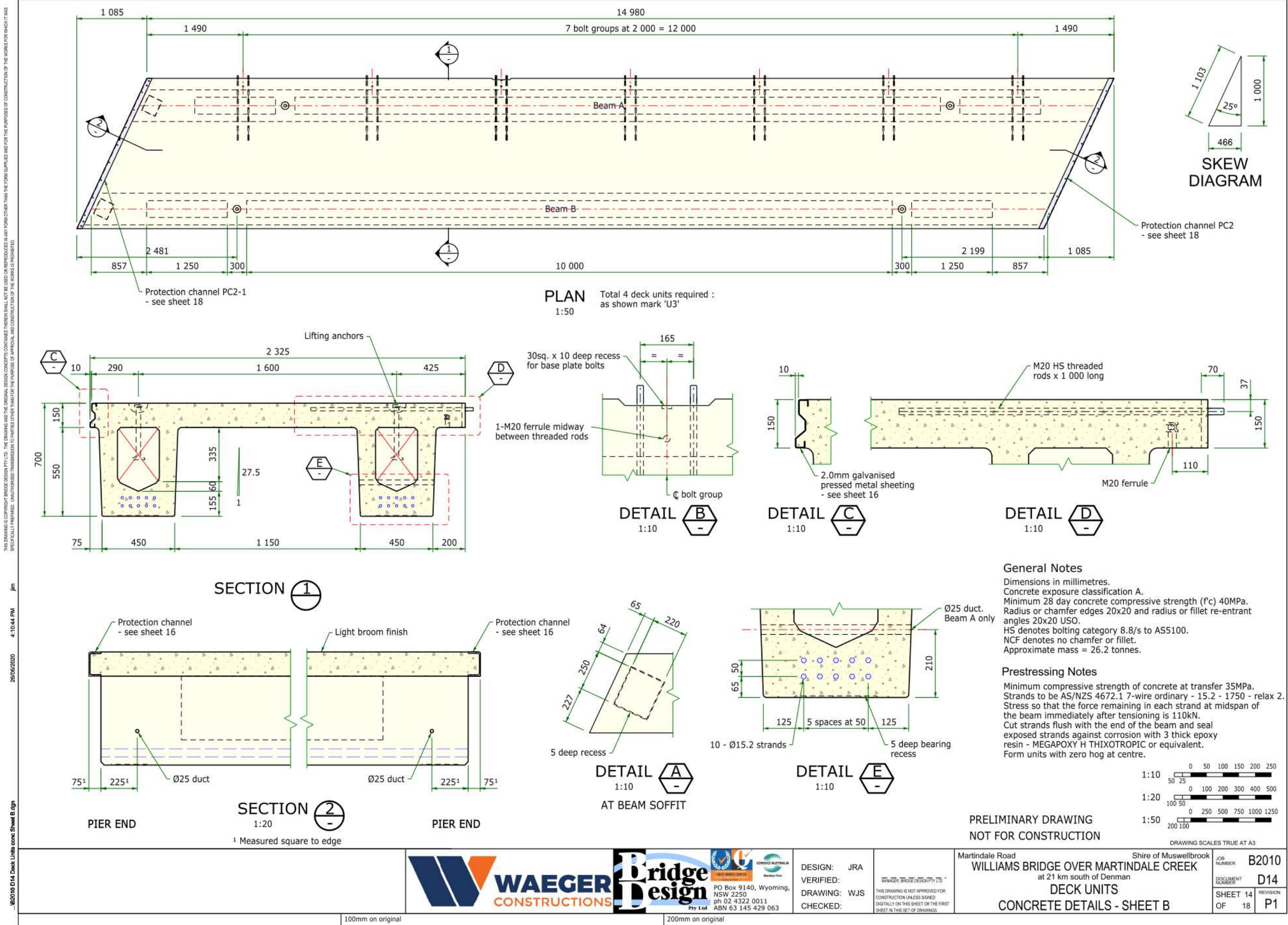














...\\B2010 D15 Deck Units reinf Sheet A.dgn

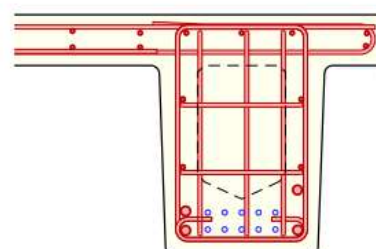


TABLE 1 -  
REINFORCEMENT SCHEDULE

Development length to be 100% of the lap length above USO.  
Reinforcement may be displaced slightly to clear embedded  
objects, holes and recesses.



Martindale Road		Shire of Muswellbrook	
WILLIAMS BRIDGE OVER MARTINDALE CREEK			
at 21 km south of Denman			
DECK UNITS			
REINFORCEMENT DETAILS			
JOB NUMBER		B2010	
DOCUMENT NUMBER		D15	
SHEET 15		REVISION	
OF 18		P1	



ISO 9001:2015  
CONSULT AUSTRALIA  
Member Firm

PO Box 9140, Wyoming  
NSW 2250  
ph 02 4322 0011  
ABN 63 145 429 063

DESIGN: JRA  
VERIFIED:  
DRAWING: WJS  
CHECKED:

MANAGER, BRIDGE DESIGN PTY LTD

Martindale Road		Shire of Muswellbrook	
WILLIAMS BRIDGE OVER MARTINDALE CREEK			
at 21 km south of Denman			
DECK UNITS			
REINFORCEMENT DETAILS			
JOB NUMBER		B2010	
DOCUMENT NUMBER		D15	
SHEET 15		REVISION	
OF 18		P1	

100mm on original

200mm on original



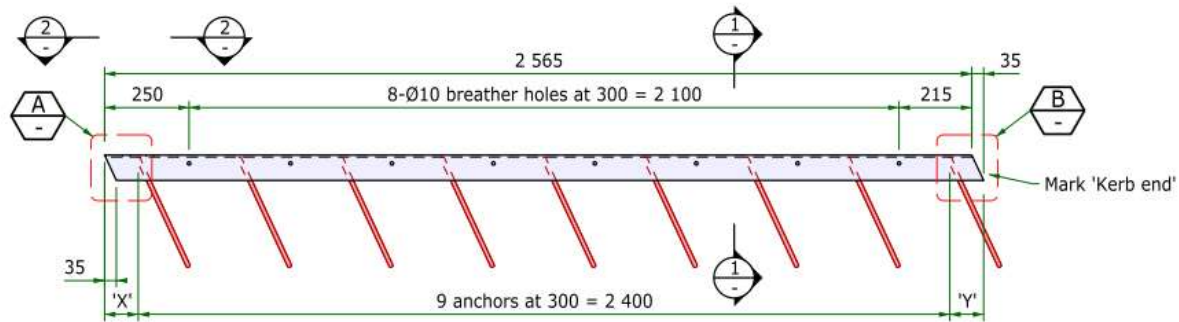




THIS DRAWING IS COPYRIGHT BRIDGE DESIGN PTY LTD. THE DRAWING AND THE ORIGINAL DESIGN CONCEPTS CONTAINED THEREIN SHALL NOT BE USED OR REPRODUCED IN ANY FORM OTHER THAN THE FORM SUPPLIED AND FOR THE PURPOSES OF CONSTRUCTION OF THE WORKS FOR WHICH IT WAS SPECIFICALLY PREPARED. UNAUTHORIZED TRANSMISSION TO ANY OTHER PARTY FOR THE PURPOSES OF APPROVAL AND CONSTRUCTION OF THE WORKS IS PROHIBITED.

20/06/2020 4:11:51 PM jlm

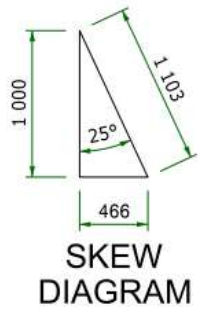
102010 D18 Misc Sheet B.dgn



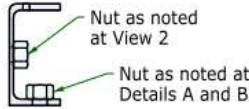
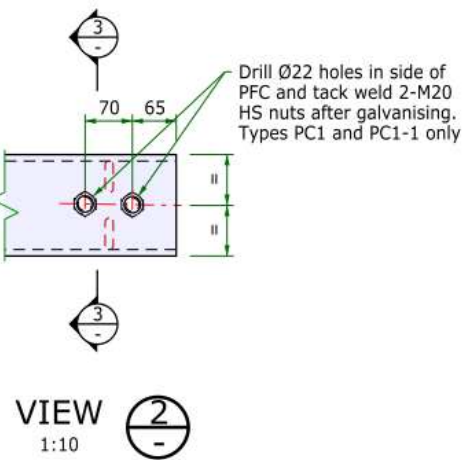
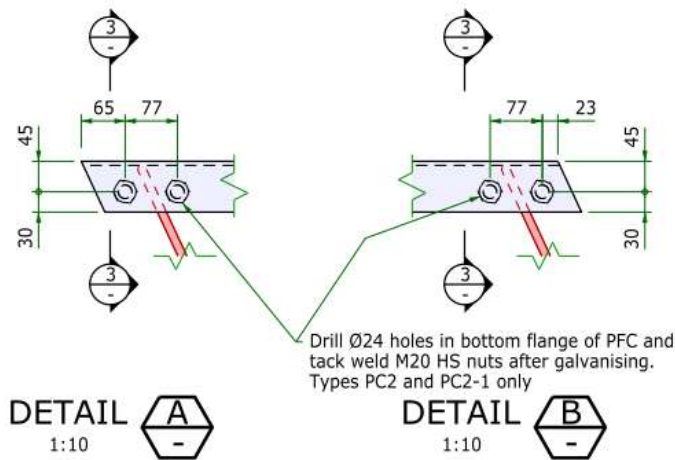
**PROTECTION CHANNEL**  
1:20  
12 total off required  
2 off type PC1, as shown and noted  
2 off type PC1-1, opposite hand and noted  
4 off type PC2, opposite hand and noted  
4 off type PC2-1, as shown and noted

Protection Channel No.	'X'	'Y'
PC1	100	100
PC1-1	100	100
PC2	90	110
PC2-1	90	110

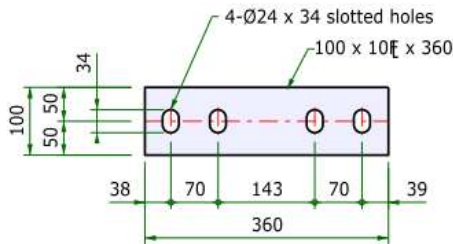
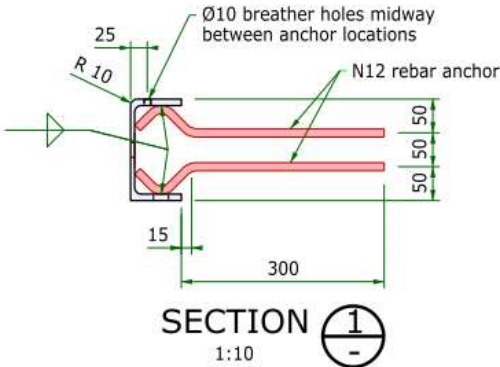
TABLE 1



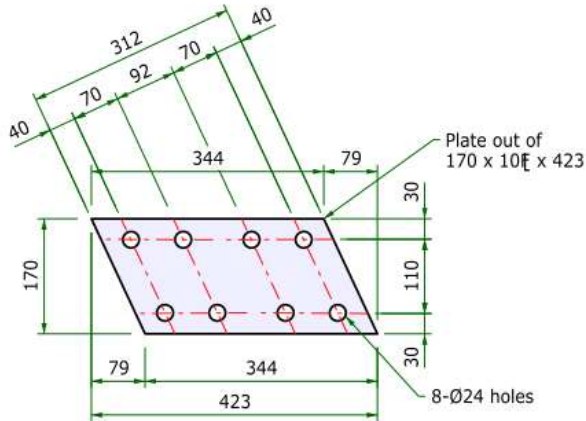
SKEW  
DIAGRAM



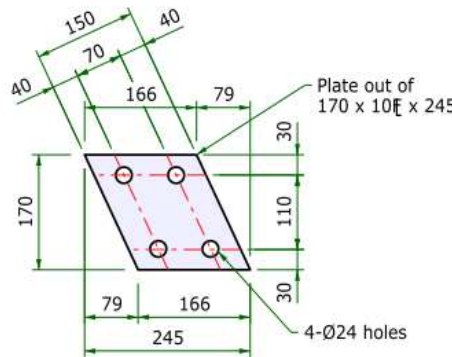
SECTION 3  
1:10



TIE PLATE 1  
1:10  
2 off required

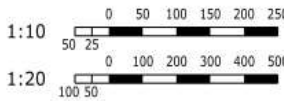


TIE PLATE 2  
1:10  
3 off required



TIE PLATE 2  
1:10  
6 off required

For General Steelwork Notes - see sheet 17



PRELIMINARY DRAWING  
NOT FOR CONSTRUCTION

DRAWING SCALES TRUE AT A3



PO Box 9140, Wyoming,  
NSW 2250  
ph 02 4322 0011  
ABN 63 145 429 063

DESIGN: JRA  
VERIFIED: WJS  
DRAWING: WJS  
CHECKED:

MANAGER, BRIDGE DESIGN PTY LTD.  
THIS DRAWING IS NOT APPROVED FOR  
CONSTRUCTION UNLESS SIGNED  
DIGITALLY ON THIS SHEET OR THE FIRST  
SHEET IN THIS SET OF DRAWINGS.

Martindale Road  
WILLIAMS BRIDGE OVER MARTINDALE CREEK  
at 21 km south of Denman  
BARRIERS, BEARINGS AND MISCELLANEOUS  
STEELWORK DETAILS - SHEET B

Shire of Muswellbrook  
JOB NUMBER B2010  
DOCUMENT NUMBER D18  
SHEET 18 OF 18  
REVISION P1

100mm on original

200mm on original



THIS DRAWING IS COPYRIGHT BRIDGE DESIGN PTY LTD. THE DRAWING AND THE ORIGINAL DESIGN CONCEPTS CONTAINED THEREIN SHALL NOT BE USED OR REPRODUCED IN ANY FORM OTHER THAN THE FORM SUPPLIED AND FOR THE PURPOSES OF CONSTRUCTION OF THE WORKS FOR WHICH IT WAS SPECIFICALLY PREPARED. UNAUTHORIZED TRANSMISSION TO ANY OTHER PARTY FOR THE PURPOSES OF APPROVAL AND CONSTRUCTION OF THE WORKS IS PROHIBITED.

20/06/2020 4:12:21 PM jlm

182010 D19 Bar shapes.dgn

Shape Code	Australian Standard Bar Shape	Shape Code	Australian Standard Bar Shape	Shape Code	Australian Standard Bar Shape	Shape Code	Non standard shape - RMS Code	Shape Code	Non standard shape
S		RC		LH		Q			
L		VL		SP		W			
LL		VV				Y			
H		J		B					
HH		LJ		C		NS1			
F		JJ		E					
V		A		G					
U		R		K					
T		HT		M					
SH		XT		N					
CC		CT		P					

**GENERAL NOTES**

Australian standard bar shapes are in accordance with AS1100.501. Bar size is the nominal diameter in millimetres or the AS4671 fabric size.

Bars shall be deformed bar grade D500N to AS4671 USO.

W denotes plain bar grade D500L to AS4671.  
R denotes plain bar grade R250N to AS4671.  
RL and SL denotes welded reinforcing fabric, rectangular and square respectively.

**Bar Marking**

The reinforcement on the drawings is described by the following notation:  
A2 10-N16-S-300FF

Placing information  
Spacing along limit line  
Bar shape code  
Bar type and size in millimetres  
Number of bars in the set  
Bar number in tag sequence  
Structure element notation

**Structure element notations:**

Q - Piles  
A - Abutments/ Abutment A  
B - Abutment B  
P - Pier  
G - Girder  
D - Deck

**Welding of Reinforcement**

Welding required to develop full strength laps to AS1554.3 Section 3.2 and Table F4.  
Weld material; E48xx, W50x or W50xx minimum.  
Throat depth is minimum and includes "S" plus reinforcement. For "S" and "W" refer to AS1554.3  
Testing - 100% visual. Inspect each layer of multi-pass welds.

PRELIMINARY DRAWING  
NOT FOR CONSTRUCTION

DRAWING SCALES TRUE AT A3



PO Box 9140, Wyoming, NSW 2250  
ph 02 4322 0011  
ABN 63 145 429 063

DESIGN: JRA  
VERIFIED: WJS  
DRAWING: WJS  
CHECKED:

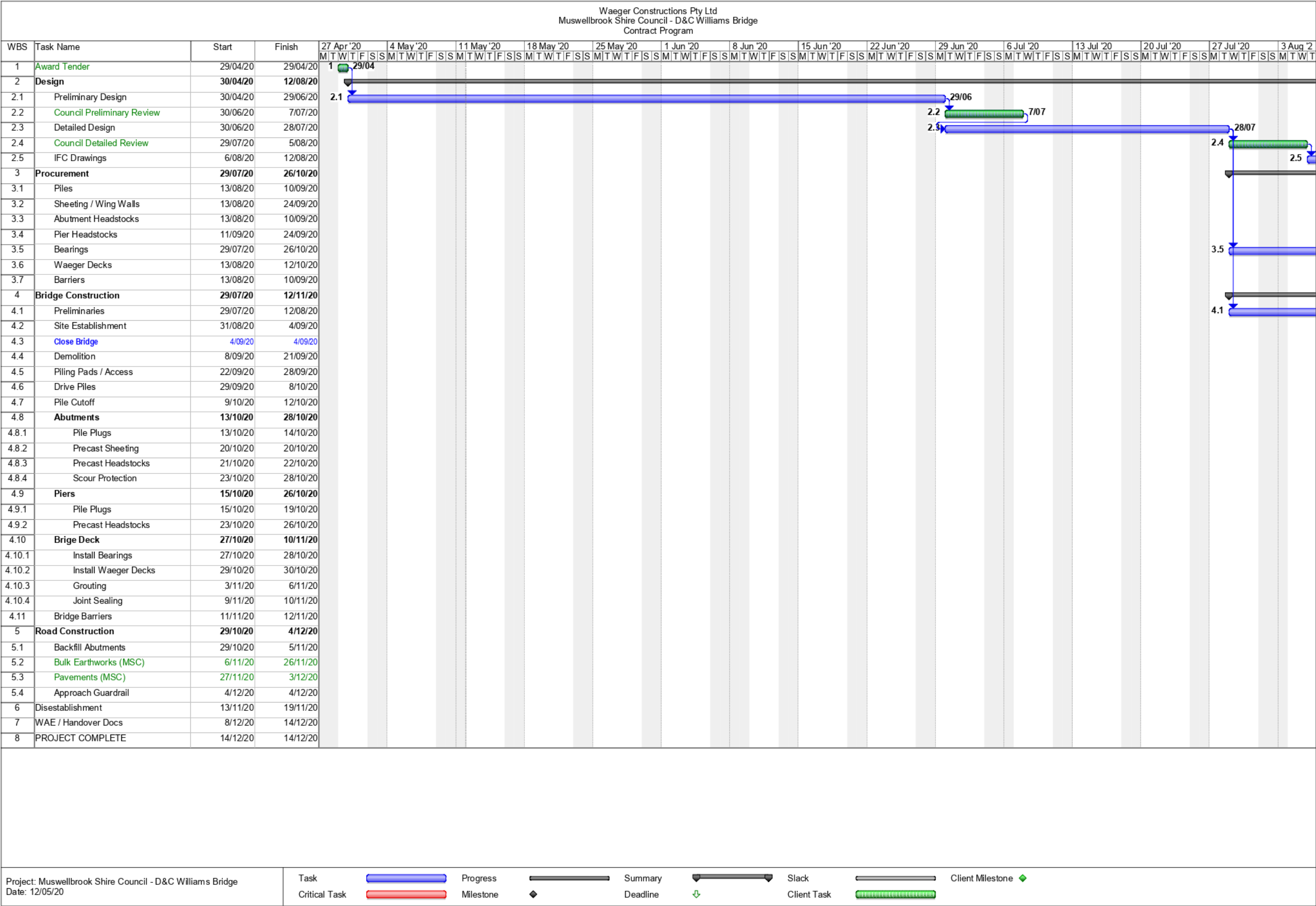
MANAGER, BRIDGE DESIGN PTY LTD  
THIS DRAWING IS NOT APPROVED FOR CONSTRUCTION UNLESS SIGNED DIGITALLY ON THIS SHEET OR THE FIRST SHEET IN THIS SET OF DRAWINGS.

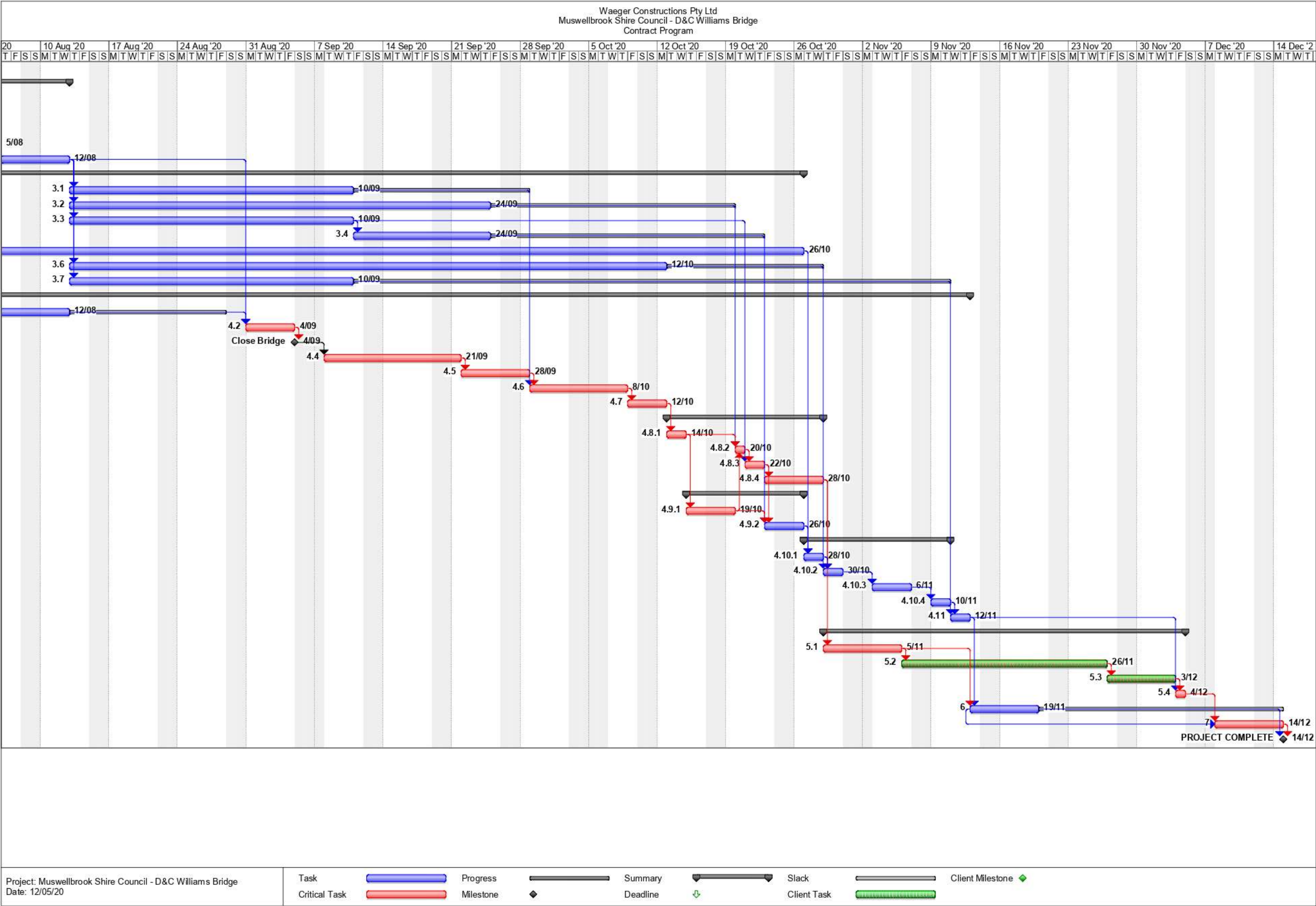
Martindale Road  
WILLIAMS BRIDGE OVER MARTINDALE CREEK  
at 21 km south of Denman  
REINFORCEMENT  
BAR SHAPES

JOB NUMBER	B2010
DOCUMENT NUMBER	D19
SHEET OF	19 OF 18
REVISION	P1

100mm on original

200mm on original







**11.6 2020-21 CAPITAL WORKS PROGRAM ROADS AND DRAINAGE**

Attachments:	A. Footpath Master Renewal B. Pram Ramps C. Kerb and Gutter Master D. Reseal program E. New Footpath Master F. 2020-2021 Urban Rehab G. Heavy Patching Master H. Road Safety Devices Master List I. Carpark Inspections
Responsible Officer:	Derek Finnigan - Deputy General Manager
Author:	Kellie Scholes - Manager - Roads, Drainage & Technical Services Kim Morris - Administration Officer
Community Plan Issue:	<i>Our community's infrastructure is planned well, is safe and reliable and provides required levels of service</i>
Community Plan Goal:	<i>Maintain and continually improve community infrastructure across the Shire.</i>
Community Plan Strategy:	<i>Implement prioritised (but responsive) rolling works program of maintenance.</i>

**PURPOSE**

To provide Council with prioritised lists of proposed and alternative projects for funding under various Capital Expenditure Road and Drainage programs for the 2020-21 financial year.

**OFFICER'S RECOMMENDATION**

1. Council endorses the following priority lists of work and the undertaking of the 2020-21 Capital Works Programs to the extent of funding under the various programs:  
Footpath Renewals as per attachment A and Pram Ramps as per attachment B;  
Kerb and Gutter Renewals as per attachment C;  
Road Resealing as per attachment D;  
New Footpath construction as per attachment E with the missing link in Maitland St from Bell to Thompson St, to be constructed using the current allocation;  
Urban Road Renewal Program as per attachment F with the rehabilitation of Brook St (King to Carl) undertaken as priority 1;  
Heavy Patching as per attachment G;  
Safety Devices as per attachment H; and  
Carparks as per attachment I with works in Hill St carpark undertaken as priority 1.
2. Council endorses the reallocation of already budgeted amounts and sources of funding for Council's co contributions for grant funded projects as detailed in the "Financial Implications" section of the report.

Moved: \_\_\_\_\_ Seconded: \_\_\_\_\_

**CONSULTATION**

Asset Management Engineer and Roads and Drainage Staff  
Manager Works, Property & Building  
Senior Works Coordinator  
Business Systems Accountant

**CONSULTATION WITH COUNCILLOR SPOKESPERSON**

Mayor Rush  
Councillor Scholes

**REPORT**

At the June 2020 ordinary meeting, Council adopted a budget containing a number of Capital Works budgets for Roads and Drainage works.

The Operational Plan requires Council to be provided with a list of proposed and alternative projects prior to construction. The attachments provide prioritised lists of works for Council's consideration and endorsement. These lists exceed the available funding in any single year, and therefore it is proposed that funds be applied to these priorities to the extent of funding under the various funding programs.

Footpath / Cycleway Renewal Program - \$:150,000

A list of footpath defects is provided as Attachment A. The list provides the locations of the defect, the extent of the area of paths which require renewal and the priority ranking. The estimated cost of Priority 1 renewals is \$126,282. The estimated cost of Priority 2 is \$367,575. The 2020-21 budget allocation will achieve the renewal of all priority 1 defects.

Further to the above list of footpath defects, a list of kerb ramps that do not meet the current standard for access is provided as Attachment B. By modifying the existing ramps, these ramps can be brought up to current standards in some locations without major associated works. However, in some locations major works such as relocation of utility services pits, and/or road works have to be carried out in order to provide compliant access.

It is proposed to carry out a combination of footpath renewals and modification of kerb ramps utilising the funds available in the footpath renewal allocation.

Kerb and Gutter Replacement Program - \$130,000

A prioritised list of kerb and gutter which requires renewal is provided as Attachment C.

Resealing Programs \$500,000:

Proposed urban and rural road segments that are recommended to be resurfaced are listed in Attachment D.

It is proposed that the programs are to be developed from the lists in the order of priority and on an as need basis so as to achieve the best value for money outcome.

New Footpaths and Cycleway Program \$150,000

A prioritised footpath and cycle way program is attached as Attachment E.

The footpath link along Maitland St, between Thompson Street and Bell Street has the highest priority ranking. It is recommended that this link be considered for construction to facilitate improved and safe access to the proposed installation of traffic signals at the Thompson Street/NE Highway intersection. The missing link in Turtle St is nominated as priority 2, however this would be best considered after the construction of kerb and gutter in future programs. The next priority aligned with the budget is Thompson St, New England Hwy to Kamilaroi Ave. This section would link the residential areas to the crossing of the New England Hwy at the proposed signals at Thompson St. The construction of the missing link in Ironbark Road (Adams to Rutherford Road) would exceed the current budget allocation, however this project may be considered favourably in future funding grant applications.

---

Urban Road Renewal Program \$ 400,000

Following is a list of urban road segments which are in poor condition (condition 5), required to be rehabilitated: Attachment F provides the details.

- Brook St - Westbound lane from George St to Carl St
- Hill St - Bridge St to rail crossing
- Lorne St - Mitchell St to NE Hwy
- Brook St - Bridge St to rail crossing
- Ogilvie St - Paxton St to Virginia St
- Palace St - Merino St to Macauley St
- Fleming St - Lorne St to Sydney St
- Anzac Pde - full length
- Wilkins Street - full length
- Jordan St - Sydney St to Lorne St

It is recommended that Brook St (Westbound from George to Carl St) be considered by Council as priority one (1). Following the upgrade of the watermain in Brook St in the 2019-20 year, restoration and renewal of the existing kerb and gutter, road pavement and footpaths was recently undertaken in adjacent areas. Thus, undertaking the rehabilitation of the adjacent block (George to Carl) would not only enhance but complete improvements to the road condition of this highly used route.

The above allocation can be funded partly from Roads 2 Recovery funding and the balance from Council revenue as outlined in the "Financial Implication" section.

Heavy Patching \$287,500

A list of heavy patching works is provided as Attachment G.

Traffic Safety/Control Devices Program \$ 135,000

A list of priorities for installation of guard rail is provided as Attachment H.

Car Parks \$100,000

A list of works required in Carpark work is provided as Attachment I. The amount of available funding and priorities, based on use and need, will influence the works to be carried out. On this basis, it is recommended that the Hill St Carpark be considered by Council as a priority due to its high use and need for improvements to facilitate access.

Rural Road Renewal Program - \$ 375,000

The amount of \$365,000 from the above allocation can be used as the co contribution for R4R R6 grant funded projects as described in the "Financial Implication" section.

The remaining \$10K will be allocated to Heavy Patching.

Resources for Regions Round 6

Council is in receipt of a grant of \$4,605,843 through the Resources for Regions Round 6 (R4R R6) Program for Upper Hunter Regional Mine Affected Roads Program. The 2019-20 budget allowed for a 50-50% split of funding between Restart and Council however the actual split is 75-25 respectively. Therefore, an additional amount of \$286,500 will need to be added to the 20-21 budget as Restart funding.

The grant funding includes an amount of \$679,593 to be claimed on behalf of Upper Hunter Shire Council (UHSC). The program is being delivered over two (2) financial years being 2019-20 and 2020-21. Council is required to provide the co contributions as shown in the table.

Upper Hunter Shire Council brought their project under this program forward to commence in the 2019-20 financial year, however this project was not included in the MSC 2019-20 Capital Works Program. As



some of the works were undertaken in the 2019-20 financial year, MSC has claimed from Restart NSW, on behalf of UHSC, for the work carried out in 2019-20 to the amount of \$318,645. The amount claimed was approved and paid by Restart NSW. Approval from Council is now sought to adjust the 2019-20 Capital Expenditure budget for Resources for Regions Round 6 to include the project – Nandowra Road UHSC with a budget of \$318,645 and further seek approval to adjust the 2020-21 Capital Expenditure Budget for Resources for Regions Round 6 to the amount \$360,948 to account for the remaining balance of work.

#### TfNSW Regional Roads Repair Program

Council will receive a total grant of \$400,000 from RMS under the Regional Roads REPAIR Program to rehabilitate a section of Bylong Valley Way in 2020-21 and 2020-22. The amount of \$347,500 will be granted in 2020-21 and the remaining amount of \$52,000 granted in 2021-22. The REPAIR grant is subject to Council providing matching amounts accordingly. Therefore, for the 2020-21 year an amount of \$347,000 towards the work is recommended to be matched by the R4R R6 funding.

As Council has to source the co-contribution for the above programs as indicated above, Council can consider reallocating the allocations of the various programs as recommended in the "Financial Implication" section.

#### Roads 2 Recovery

Council will receive funding of \$866,847 from Roads 2 Recovery. The amount of \$288,949 from this allocation must be spent by 31 December 2020 in accordance with the funding conditions.

At the 28 April 2020 meeting of Council – Tender for Construction of Williams Bridge, it was reported and endorsed by Council that funding to the amount of \$297,500 be sourced from the Roads 2 Recovery (2020-21 allocation) as a source of funding to construct Williams Bridge on Martindale Road. Unfortunately, upon application, Council has been advised by the Commonwealth Government that, as the proposed use of the Roads 2 Recovery funding in addition to the \$650K funding from the Bridges Renewal Program would mean that the total funding from Commonwealth sources exceeds 50% of the total project funding, it does not meet the guidelines and, therefore, use of Roads 2 Recovery money for the construction of Williams Bridge is not approved.

The Roads 2 Recovery funding is recommended to be allocated to fund the following programs /projects;

- Urban Road Renewal Brook St Muswellbrook - \$366,874
- Mangoola Road Culvert Replacement Ch 3.9Km - \$500,000 (if current funding application is unsuccessful)

### **SOCIAL IMPLICATIONS**

The implementation of the Capital Works Program will provide positive impacts to the Community through the provision of improved assets, amenity, access and road safety. It is acknowledged that there would be a short term inconvenience during the construction phase of the program, but this would be managed and minimised as much as possible, supported by the provision of effective, timely community information and advice.

### **FINANCIAL IMPLICATIONS**

No additional funding is required. Council has allocated funds in the 2020-21 Budget for various Capital Work Programs. This report however, requests reallocating some funds to appropriate projects.

It is proposed that funds be applied to the attached priorities to the extent of funding under the following programs:

- Footpath Renewal Program
- Kerb & Gutter Renewal Program
- Resealing Program
- New Footpath Program

- Heavy Patching Program
- Safety Devices Program
- Carparks Program

It is recommended that Council's co contributions for grant funded projects come from the funding sources as summarised in the following table.

Program	Project	Required Co Contribution/ budget allocation	Proposed Co-funding source			
			Rural road Renewal Allocation	R 4 R R6 Bylong Valley Way	Mine Affected Road Renewal	Ex 3 x3 Grant
R4R R6	Various	708,750	365,000		343,750	
2020-21 REPAIR TfNSW	Bylong Valley Way 1.3km	347,500		277,500		70,000

### POLICY IMPLICATIONS

Nil known.

### STATUTORY IMPLICATIONS

Nil known.

### LEGAL IMPLICATIONS

Nil known.

### OPERATIONAL PLAN IMPLICATIONS

The submission of the report complies with item 19.1.1 of the Operational Plan: 'Deliver the capital program substantially on time, on budget and in accordance with relevant design and construction standards'.

### RISK MANAGEMENT IMPLICATIONS

Endorsing the programs mitigates risk from non- delivery of works.

	Attachment A						
	FOOTPATH / CYCLE WAY RENEWAL PROGRAM						
Road Name	Number/Locator	Town	Description	Width	Length	m2	Priority
Maitland St	Servo Cnr Sydney St	Muswellbrook	2 x areas of heavy cracking and failure	5	30	150	1
Hunter Park	Jersey to Dangar	Muswellbrook	Join	1.2	15	18	1
Brook Park	Behind 17 Dalwood Pl	Muswellbrook	Severe step next to storm water drain	1.2	20	24	1
King St	7	Muswellbrook	Join - has been filled	1.2	18	21.6	1
Bridge St	path from View Place	Muswellbrook	Cracked and lifting path	1.2	14	16.8	1
Casuarina Cl	12 Casuarina	Muswellbrook	Cracked	2	10	20	1
Northerly Cl	6	Muswellbrook	Crack - footpath raised	1.2	15	18	1
Skellatar Stock Route	Various locations	Muswellbrook	Cracking various locations	1.2	60	72	1
Brentwood St	9	Muswellbrook	Join - has been filled	3	5	15	1
Highbrook Park	Various locations	Muswellbrook	Cracked	1.2	20	24	1
Denman Wetland	1951 (Jock Brown)	Denman	Cracking	2	6	12	1
Denman Wetland	1963 (Richard Le brocq)	Denman	Cracking	2	6	12	1
Denman Wetland	1971 (Gillian Garland)	Denman	Cracking	2	10	20	1
Skellatar Stock Route	Rocky Road	Muswellbrook	AC footpath cracks are widening. The AC is lifting at the join of the concrete creating a trip hazard	2	5	10	1
Hunter Park	Dangar to Wollombi cross path	Muswellbrook	Various locations joins	2	20	40	1/2
Lorne St	Bowman Park path	Muswellbrook	Pram Ramp	2	12	24	1/2
McGregor St	3	Muswellbrook		1.2	5	6	1/2
McGregor St	1	Muswellbrook		1.2	5	6	1/2
Belmore St	6	Muswellbrook	Cracking	1.2	4	4.8	1/2
Belmore St	8	Muswellbrook	Cracking	1.2	6	7.2	1/2
Sydney St	Matildas - subject to roadhouse	Muswellbrook	A/C footpath is cracked, uneven and potholled (water main?)	5.5	15	82.5	1/2
Black St	2	Muswellbrook	Cracking	1.2	3	3.6	1/2
Bloodwood Rd	17	Muswellbrook		1.2	6	7.2	1/2
Bridge St	Beta	Muswellbrook		1.2	6	7.2	1/2
Brook St	99	Muswellbrook	Crack	1.2	2	2.4	1/2
Brook St	98	Muswellbrook	Trench	1.2	2	2.4	1/2
Brook St	87	Muswellbrook		1.2	20	24	2
Brook St	82	Muswellbrook	Crack	1.2	4	4.8	2
Casuarina Cl	Path to Mbk Fair	Muswellbrook	Cracked	2	8	16	2
Crinoline St	3	Denman	2 X 10m sites	1.2	20	24	2
Day St	Opp 5	Muswellbrook	Cracking	1.2	3	3.6	2
Ogilvie St	NRMA	Denman	Extensively cracked path	2	15	30	1
Ogilvie St	Corner Virginia (56 Ogilvie)	Denman		2	2	4	1
Fitzgerald Park	Various locations	Muswellbrook	Cracked edges	10	2	20	2
Hill St	Marketplace	Muswellbrook		1.2	3	3.6	2
Hill St	Bridge to Sowerby St	Muswellbrook	Various locations - cracks and delamination	1.2	180	216	2
Hill St	124	Muswellbrook	Street tree	1.2	5	6	2
Hunter Park	Jersey to Dangar	Muswellbrook	Joins	1.2	3	3.6	2
Ogilvie St	Indoor Sports Centre	Denman	Slab needs replacement - shrub has lifted	3	5	15	1/2
Ogilvie St	Indoor Sports Centre	Denman	Crack	1.2	7	8.4	1/2
Ogilvie St	Dapkos	Denman	Cracking	2.5	3	7.5	1/2
Wanaruah Cct	23	Muswellbrook		1.2	3	3.6	2
Maitland St	between Weemala and Bimbadeen	Muswellbrook	2 x filled trenches	2	10	20	2
Maitland St	Lorne Street - Remington	Muswellbrook	Some cracks and trenches - match new path	1.2	75	90	2
Maitland St	Opp Reece/Supercheap	Muswellbrook	Grinded edges (Street tree)	2	10	20	2
Maitland St	Opp Reece	Muswellbrook	Grinded edges (Street tree)	2	6	12	2
Maitland St	Opp Reece	Muswellbrook	Grinded edges (Street tree)	2	6	12	2
Maitland St	Opp Pirtek	Muswellbrook	New Slab edged grinded (Street tree)	2	3	6	2
Maitland St	Opp Pirtek	Muswellbrook	New Slab edged grinded (Street tree)	2	6	12	2
Maitland St	Opp Pirtek	Muswellbrook	Grinded edges (Street tree)	2	7	14	2
Maitland St	Opp Caltex	Muswellbrook	Grinded edges (Street tree)	2	10	20	2
Maitland St	Opp Caltex	Muswellbrook	Grinded edges (Street tree)	2	3	6	2
Maitland St	Opp Caltex	Muswellbrook	Grinded edges (Street tree)	2	6	12	2
Maitland St	Opp Caltex	Muswellbrook	Grinded edges (Street tree)	2	3	6	2
Maitland St	Opp St Andrews	Muswellbrook	New Slab edged grinded (Street tree)	2	5	10	2
Maitland St	Opp St Andrews	Muswellbrook	Grinded edges (Street tree)	2	10	20	2
Maitland St	Opp 78	Muswellbrook	Grinded edges (Street tree)	2	7	14	2
Maitland St	Opp 76	Muswellbrook	Grinded edges (Street tree)	2	7	14	2
Maitland St	Crossfit	Muswellbrook	Grinded edges (Street tree)	2	6	12	2
Maitland St	Crossfit	Muswellbrook	Grinded edges (Street tree)	2	6	12	2
Maitland St	Crossfit	Muswellbrook	Grinded edges (Street tree)	2	3	6	2
Maitland St	Crossfit	Muswellbrook	Grinded edges (Street tree)	2	16	32	2
Maitland St	Crossfit	Muswellbrook	Grinded edges (Street tree)	2	8	16	2
Mill St	Length	Muswellbrook	Footpath is cracked and in poor condition, replacement for the length is recommended	1.2	80	96	2
Mill St	Cnr Sydney St	Muswellbrook	Cracking various locations	1.2	36	43.2	2
Ogilvie St	Cnr Virginia St	Denman	Sinking temp repairs done	2	2	4	2
Ogilvie St	Dapkos	Denman	Trench	2.8	0.5	1.4	2
Ogilvie St	Dapkos	Denman	Trench	2.8	0.5	1.4	2
Ogilvie St	Shell Garage	Denman	Small Pit	0.5	1	0.5	2
Palace St	36	Denman	street tree	1.2	3	3.6	2
Paxton St	Cnr Ogilvie (Fernandes)	Denman	Joins and cracks	2	12	24	2
Paxton St	48	Denman	Trench	2	2	4	2
Paxton St	40	Denman		2	5	10	2
Paxton St	42	Denman	Around street tree	0.5	4	2	2
Paxton St	60	Denman	Crack	1.2	1	1.2	2
Paxton St	62	Denman	two locations	1.2	10	12	2
Paxton St	22	Denman	Trench	1.2	2	2.4	2
Paxton St	34 Ogilvie St (Fernandez)	Denman		2	6	12	2
Paxton St	34 Ogilvie St (Fernandez)	Denman		2	4	8	2
Paxton St	34 Ogilvie St (Fernandez)	Denman		2	10	20	2
Paxton St	Carpark	Denman		2	2	4	2
Queen St	133	Muswellbrook	Footpath rising	1.2	3	3.6	2
Roger St	Primary School (main gates)	Muswellbrook	Cracking	1.3	2	2.6	2
Roger St	Primary School (near 11)	Muswellbrook	Cracking	1.3	4	5.2	2
Roger St	Koombahla to Primary School	Muswellbrook	Poor quality concrete at school	1.3	137	178.1	2
Rutherford Rd	Toyota Dealership	Muswellbrook	Replace uneven and cracked AC footpath	4	16	64	2
Sydney St	Cnr Bridge to 29	Muswellbrook	From corner Bridge St across full length of Workers Club is cracked and has many uneven joins	5	100	500	2
Sydney St	1	Muswellbrook	Cracking	1	4	4	2
Sydney St	7	Muswellbrook	Cracking	1.5	10	15	2
Sydney St	122	Muswellbrook	Cracking	1.2	7	8.4	2
Sydney St	Workers Club	Muswellbrook	Cracking	1.5	2	3	2
Sydney St	Workers Club	Muswellbrook	Cracking	1.2	4	4.8	2
Sydney St	Workers Club	Muswellbrook		1.2	10	12	2
Sydney St	Upper Hunter Fire Protection	Muswellbrook		1.2	3	3.6	2
Turtle St	12	Denman		1.2	2	2.4	2
Turtle St	12	Denman		1.2	4	4.8	2
Turtle St	20	Denman	Cracking	1.2	3	3.6	2
Turtle St	18	Denman	Trench	1.2	2	2.4	2
Virginia St	45	Denman	Crack	1.2	5	6	2
Virginia St	43/45	Denman	Premix join	1.2	2	2.4	2
Virginia St	55	Denman	Crack	1.2	8	9.6	2
Wanaruah Cct	9	Muswellbrook	Next to Telstra pit - sunk	1.2	3	3.6	2

	Attachment A						
	FOOTPATH / CYCLE WAY RENEWAL PROGRAM						
William St	Police Station	Muswellbrook	Cracking various locations	1.2	15	18	2
William St	Shamrock Hotel	Muswellbrook	AC footpath is cracked, uneven and potholled	3	19	57	2
Sydney St	Muswellbrook Nissan	Muswellbrook	Includes pram ramp - various locations	2	15	30	2
Sydney St	122	Muswellbrook		1.2	6	7.2	2\3
Sydney St	114	Muswellbrook	Cracking	1.2	6	7.2	3
Sydney St	114	Muswellbrook	Trench	1.2	3	3.6	3
Verdelho Reserve	Various locations	Muswellbrook	Cracking - minimum of 10 locations along length	2	20	40	3
Anzac Pde	Path to Tindale	Muswellbrook	Cracking	1.2	2	2.4	3
Arbor Park	Path from Virginia St (near bridge)	Denman	Cracks and joins	1.2	6	7.2	3
Bell Street	Olympic Park driveway	Muswellbrook	join	2	5	10	3
Bell Street	Olympic Park driveway	Muswellbrook	join	2	5	10	3
Bell Street	cnr Victoria	Muswellbrook	join	2	5	10	3
Belmore St	4	Muswellbrook	Cracking	1.2	3	3.6	3
Belmore St	10	Muswellbrook	Cracking	1.2	6	7.2	3
Belmore St	12	Muswellbrook	cracking	1.2	3	3.6	3
Belmore St	Opp 21	Muswellbrook	Cracking	1.2	6	7.2	3
Belmore St	Opp 23	Muswellbrook	Cracking	1.2	2	2.4	3
Bimbadeen Dr	1c	Muswellbrook	cracking	1.2	3	3.6	3
Bimbadeen Dr	26	Muswellbrook		1.2	3	3.6	3
Bimbadeen Dr	32	Muswellbrook	cracking	1.2	3	3.6	3
Bimbadeen Dr	39	Muswellbrook	Join	1.2	3	3.6	3
Bimbadeen Dr	45	Muswellbrook	Street tree (temp fill needed)	1.2	5	6	3
Bimbadeen Dr	45	Muswellbrook	Pram ramp	1.2	5	6	3
Bimbadeen Dr	45	Muswellbrook	Join	1.2	3	3.6	3
Bimbadeen Dr	47	Muswellbrook	Street tree (temp fill needed)	1.2	6	7.2	3
Bimbadeen Dr	49	Muswellbrook	Join	1.2	3	3.6	3
Bimbadeen Dr	51	Muswellbrook	Join	1.2	3	3.6	3
Bimbadeen Dr	53	Muswellbrook	Join x 2	1.2	5	6	3
Bimbadeen Dr	59	Muswellbrook	Crack	1.2	6	7.2	3
Bimbadeen Dr	61	Muswellbrook	join	1.2	5	6	3
Bimbadeen Dr	26	Muswellbrook		1.2	5	6	3
Bimbadeen Dr	26	Muswellbrook	Crack	1.2	2	2.4	3
Black St	10	Muswellbrook	Join	1.2	2	2.4	3
Bloodwood Rd	31	Muswellbrook	Grinding done	1.2	2	2.4	3
Bloodwood Rd	29	Muswellbrook	Grinding done	1.2	5	6	3
Bloodwood Rd	29	Muswellbrook	Crack	1.2	5	6	3
Bloodwood Rd	25	Muswellbrook	Crack	1.2	3	3.6	3
Bloodwood Rd	11	Muswellbrook	Trench	1.2	3	3.6	3
Brentwood St	15	Muswellbrook		2	3	6	3
Brook Park	Various locatons	Muswellbrook	Joins	1.2	4	4.8	3
Brook St	95	Muswellbrook	Cracks	1.2	2	2.4	3
Brook St	98	Muswellbrook	Cracked path + pram ramp	1.2	20	24	3
Brook St	121	Muswellbrook	Trench	1.2	3	3.6	3
Brook St	114	Muswellbrook	Crack	1.2	3	3.6	3
Brook St	108	Muswellbrook		1.2	2	2.4	3
Brook St	94	Muswellbrook	Crack	1.2	3	3.6	3
Brook St	86	Muswellbrook		1.2	3	3.6	3
Brook St	79/81	Muswellbrook		1.2	30	36	3
Brook St	95	Muswellbrook	Crack	1.2	22	26.4	3
Casuarina Cl	12 Casuarina	Muswellbrook	Cracked	2	15	30	3
Crinoline St	23\25	Denman		1.2	2	2.4	3
Crinoline St	25\27	Denman	Trench	1.2	3	3.6	3
Crinoline St	19\17	Denman	Trench	1.5	3	4.5	3
Crinoline St	13\15	Denman	Trench	1.2	2	2.4	3
Crinoline St	RSL	Denman	Trench	1.2	4	4.8	3
Crinoline St	7	Denman		1.2	4	4.8	3
Crinoline St	9	Denman		1.2	2	2.4	3
Crinoline St	9	Denman		1.2	2	2.4	3
Dangar Pl	Path to Wollombi Road	Muswellbrook	Cracking	2	2	4	3
Day St	opp 1/3	Muswellbrook	Cracking	1.2	6	7.2	3
Day St	6	Muswellbrook	Cracking	1.2	6	7.2	3
Denman Wetland	1953 (Gary Day)	Denman	Cracking	2	2	4	3
Denman Wetland	1973 (Shaun Martin)	Denman	Cracking	2	2	4	3
Denman Wetland	At creek crossing (Path to Virginia)	Denman	Poor Repair	2	6	12	3
Denman Wetland	1995 (Mitchell Collins)	Denman	Cracking	2	2	4	3
Denman Wetland	1998 (Josh Smith)	Denman	Cracking	2	2	4	3
Finnigan Cres	5	Muswellbrook	Cracking	1.2	5	6	3
Finnigan Cres	13	Muswellbrook	Cracking	1.2	10	12	3
Finnigan Cres	27	Muswellbrook	Cracking	1.2	2	2.4	3
Finnigan Cres	39	Muswellbrook		1.2	2	2.4	3
Fitzgerald Park	Various locations	Muswellbrook	Cracked edges	16	2	32	3
Glenbawn Place	path to Tobruk Ave	Muswellbrook	Join - has been filled with premix x 3	2	9	18	3
Henry Dangar Dr	Roundabout (Bimbadeen/Day)	Muswellbrook	Cracking	1.2	6	7.2	3
Henry Dangar Dr	21	Muswellbrook		1.2	0.5	0.6	3
Hunter Park	Side no 5 Jersey	Muswellbrook	Join - premix	1.2	2	2.4	3
Hunter Park	Jersey to Dangar	Muswellbrook	Joins various locations	1.2	5	6	3
Hunter Park	Jersey to Wollombi	Muswellbrook	Various locations	1.2	15	18	3
Ironbark Road	1 Plashett (house boundary)	Muswellbrook	Has been filled with premix	2	5	10	3
Ironbark Road	64 (w of driveway)	Muswellbrook	Chip - not yet filled with premix	2	3	6	3
Ironbark Road	64 (e of driveway)	Muswellbrook	Chip - filled with premix	2	3	6	3
Ironbark Road	114	Muswellbrook	2 x badly restored trenches	2	3	6	3
Ironbark Road	68/70	Muswellbrook	Badly restored trench - grinding has been done	2	4	8	3
John Howe Cct	1	Muswellbrook		1.2	3	3.6	3
John Howe Cct	13	Muswellbrook	Cracking	1.2	2	2.4	3
John Howe Cct	30 (vacant block)	Muswellbrook	Cracking	1.2	4	4.8	3
John Howe Cct	2	Muswellbrook	Join x 2	1.2	4	4.8	3
John Howe Cct	5	Muswellbrook		1.2	2	2.4	3
John Howe Cct	11	Muswellbrook		1.2	2	2.4	3
John Howe Cct	17	Muswellbrook	Join	1.2	5	6	3
John Howe Cct	30	Muswellbrook	Join	1.2	2	2.4	3
John Howe Cct	34	Muswellbrook	Cracks	1.2	5	6	3
John Howe Cct	36	Muswellbrook	Cracks	1.2	2	2.4	3
John Howe Cct	61 Bimbadeen	Muswellbrook	Street tree	1.2	3	3.6	3
John Howe Cct	28	Muswellbrook	Street tree	1.2	2	2.4	3
King St	3	Muswellbrook	Cracking	1.2	2	2.4	3
King St	Muswellbrook High School	Muswellbrook	Cracking - various locations	1.2	11	13.2	3
Kinji Reserve	Calgaroo entrance	Muswellbrook	Join	2	10	20	3
Kinji Reserve	Rear 33 Calgaroo	Muswellbrook	Join	2	5	10	3
Loftberg Court	2	Muswellbrook		1.2	2	2.4	3
Lou Fisher Pl	4	Muswellbrook	2 x trenches (Have been ground) and 1 join	1.2	5	6	3
Maitland St	75	Muswellbrook	Cracking	1.2	1.2	1.44	3
Maitland St	75	Muswellbrook	Cracking	1.2	1.2	1.44	3
Maitland St	75	Muswellbrook	Cracking	1.2	1.2	1.44	3
Maitland St	Cnr Francis Street	Muswellbrook	Poorly restored slab	1.3	3	3.9	3
Maitland St	Thompson St to TAFE	Muswellbrook	Cracked and worn	1.2	130	156	3
McGregor St	17	Muswellbrook		1.2	3	3.6	3
McGregor St	15	Muswellbrook		1.2	2	2.4	3

Attachment A							
FOOTPATH / CYCLE WAY RENEWAL PROGRAM							
Road Name	Number/Locator	Town	Description	Width	Length	m2	Priority
McGregor St	11	Muswellbrook		1.2	5	6	3
McGregor St	5	Muswellbrook		1.2	3	3.6	3
Ogilvie St	43	Denman	Asphalt around Telstra Pit	1	2	2	3
Ogilvie St	Dapkoss	Denman	Cracks	3	15	45	3
Ogilvie St	IGA Carpark	Denman	Cracking	1.2	9	10.8	3
Osborn Ave	38	Muswellbrook	Cracking	2	2	4	3
Osborn Ave	38	Muswellbrook	Cracking	2	6	12	3
Osborn Ave	38	Muswellbrook	Cracking	2	10	20	3
Osborn Ave	32	Muswellbrook	Cracking	2	8	16	3
Osborn Ave	36	Muswellbrook		2	6	12	3
Osborn Ave	4	Muswellbrook		2	3	6	3
Osborn Ave	73	Muswellbrook	Trench	2	2	4	3
Osborn Ave	65	Muswellbrook	Crack	2	7	14	3
Osborn Ave	53	Muswellbrook		2	2	4	3
Palace St	47	Denman	Cracks	1.2	10	12	3
Palace St	Bowling Club	Denman	Trench	1.2	2	2.4	3
Palace St	Denman Hotel	Denman	Cracks	1.2	2	2.4	3
Palace St	Denman Hotel	Denman	Cracks	1.2	2	2.4	3
Palace St	44	Denman		1.2	2	2.4	3
Palace St	42	Denman		1.2	6	7.2	3
Paterson Place	Various locations	Muswellbrook	Joins and cracks	1.2	17	20.4	3
Paxton St	22	Denman		1.2	2	2.4	3
Paxton St	24	Denman	Cracks	1.2	4	4.8	3
Paxton St	24	Denman	Cracks	1.2	2	2.4	3
Paxton St	25	Denman		2	2	4	3
Paxton St	30	Denman	Street tree - cracks	1.2	2	2.4	3
Paxton St	42	Denman	Poorly restored trench	2	0.5	1	3
Paxton St	42	Denman	Street tree	1.2	8	9.6	3
Paxton St	42/44	Denman	Trench	4	0.5	2	3
Paxton St	46	Denman		2	10	20	3
Paxton St	50	Denman	Street tree	2	2	4	3
Paxton St	56	Denman	Cracked	1.2	12	14.4	3
Paxton St	Crinoline St to Denman Public School	Denman	widened path has join/trip edge, tree issues, etc along length	2	220	440	3
Paxton St	Denman Public School	Denman		1.2	5	6	3
Paxton St	29	Denman		1.5	2	3	3
Paxton St	Memorial Park	Denman		2	6	12	3
Plashett Cl	2 Angus Peebles	Muswellbrook	Trench filled with A/C	1.2	3	3.6	3
Skellatar Stock Route	3	Muswellbrook		3	3	9	3
Skellatar Stock Route	29	Muswellbrook		2	4	8	3
Skellatar Stock Route	St James School entrance driveway	Muswellbrook	Hydrant surround was cut out and filled - is now cracking	3	2	6	3
Sowerby St	Cnr Manning	Muswellbrook	Undulating A/C footpath	4.4	20	88	3
Sowerby St	Cnr Manning	Muswellbrook	Crack	2	5	10	3
Sowerby St	Marketplace loading dock	Muswellbrook		3	5	15	3
Sowerby St	8	Muswellbrook	Crack	3	2	6	3
St Heliers St	11	Muswellbrook	Damaged trench - possible to premix	1.2	2	2.4	3
Sydney St	80	Muswellbrook	Filled	1.2	4	4.8	3
Sydney St	82	Muswellbrook	Crack	1.5	3	4.5	3
Sydney St	124	Muswellbrook	Street tree	1.2	3	3.6	3
Sydney St	132	Muswellbrook		1.2	3	3.6	3
Sydney St	98	Muswellbrook		2	3	6	3
Sydney St	90	Muswellbrook		1.2	3	3.6	3
Turtle St	22 Paxton St	Denman	Tree	1.2	6	7.2	3
Turtle St	24	Denman		1.2	2	2.4	3
Turtle St	26	Denman	tree	1.2	5	6	3
Turtle St	26	Denman	driveway	1.2	3	3.6	3
Verdelho Reserve	Just down from Chablis	Muswellbrook	Cracks	2	4	8	3
Verdelho Reserve	Cabernet to Shiraz	Muswellbrook	Cracks	2	6	12	3
Verdelho Reserve	Shiraz St	Muswellbrook	Cracks	2	5	10	3
Verdelho Reserve	near seats (Cabernet end)	Muswellbrook	Cracks	2	6	12	3
Virginia St	53	Denman	Crack	1.2	2	2.4	3
Virginia St	57	Denman		1.2	3	3.6	3
Virginia St	73	Denman	Crack	1.2	2	2.4	3
Virginia St	73	Denman	Crack	1.2	2	2.4	3
Virginia St	75	Denman	Crack	1.2	0.5	0.6	3
Virginia St	83	Denman	Trench - has been filled with premix	1.2	0.5	0.6	3
Virginia St	85 & 87	Denman	Crack	1.2	5	6	3
Virginia St	93	Denman	Crack	2	5	10	3
Virginia St	41	Denman		1.2	2	2.4	3
Virginia St	27	Denman		1.2	3	3.6	3
Virginia St	23	Denman		1.2	3	3.6	3
Wanaruah Cct	3	Muswellbrook		1.2	2	2.4	3
Wanaruah Cct	3	Muswellbrook		1.2	2	2.4	3
Wanaruah Cct	25	Muswellbrook		1.2	3	3.6	3
Wanaruah Cct	opp 45	Muswellbrook	cracking	1.2	5	6	3
Wattle St	16/18	Muswellbrook	Failed trench	1.2	2	2.4	3
Wattle St	16	Muswellbrook	Failed trench	1.2	2	2.4	3
Wattle St	14	Muswellbrook	Failed trench	1.2	2	2.4	3
Wattle St	8	Muswellbrook	Failed trench	1.2	2	2.4	3
Henry Dangar Dr	Path across Ridge	Muswellbrook	Cracking	2	2	4	3
Belmore St	12	Muswellbrook	Cracking	1.2	6	7.2	4
Black St	10	Muswellbrook	Cracking	1.2	3	3.6	4
Brook St	104	Muswellbrook	Cracking - various locations	1.2	9	10.8	4
Brook St	100	Muswellbrook	Crack	1.2	3	3.6	4
Brook St	94	Muswellbrook	Crack	1.2	2	2.4	4
Brook St	92	Muswellbrook	Crack	1.2	2	2.4	4
Brook St	90	Muswellbrook	Crack (at driveway)	1.2	2	2.4	4
Brook St	88	Muswellbrook	Crack	1.2	2	2.4	4
King St	1	Muswellbrook	Cracking	1.2	3	3.6	4
King St	3	Muswellbrook	Cracking	1.2	3	3.6	4
King St	Muswellbrook High School	Muswellbrook	Cracking - various locations	1.2	8	9.6	4
Osborn Ave	34	Muswellbrook	Cracking	2	2	4	4
Osborn Ave	40	Muswellbrook	Cracking	2	2	4	4
Sydney St	68	Muswellbrook	Cracking	1.2	3	3.6	4
Sydney St	84	Muswellbrook	Cracking	1.5	3	4.5	4
Sydney St	86	Muswellbrook	Cracking	1.5	3	4.5	4
Sydney St	92	Muswellbrook	Cracking	1.5	3	4.5	4
Sydney St	100	Muswellbrook	Cracking	1.5	5	7.5	4
Sydney St	Brookside	Muswellbrook	Cracking	1.2	6	7.2	4
Wanaruah Cct	opp 47	Muswellbrook	Cracking	1.2	5	6	4
Wanaruah Cct	opp 37	Muswellbrook	Cracking	1.2	15	18	4
Wanaruah Cct	opp 33	Muswellbrook	Cracking	1.2	10	12	4
Wanaruah Cct	opp 1/31	Muswellbrook	Cracking	1.2	15	18	4
Wattle St	6	Muswellbrook	Trench	1.2	2	2.4	4
Wattle St	10	Muswellbrook	Trench	1.2	2	2.4	4
Wattle St	22	Muswellbrook	Trench	1.2	2	2.4	4
Wattle St	3 Chivers	Muswellbrook	Trench	1.2	2	2.4	4
Olympic Park	Various locations	Muswellbrook	Grinding done	208	2	416	4
						7305	

Attachment B									
Sub Location	Priority	Standard Y/N	Ramp %	Road %	Lip mm	Join?	Wings Y/N	Comment	Extra work needed
Art Gallery to QEII	1	N	6	4	20	Join	N	Observed electric wheel chair had difficulty over lip. Not a proper pram ramp	
17 Skellatar Stock Route	1	N	9	3	0		Y	Damaged pram ramp (has premix) needs to be replaced	
108 Wollombi Rd	1	N	10	5	20		Y	Cracked and damaged	
Mbk High School	1	N	12	5	50		Y	Path coming from School Hall - mobility issues on school grounds	
QEII to Art Gallery	1	N	11	5	20	Join	N	Not a proper pram ramp	
Prince of Wales Hotel	1	N	10	14	0		Y		
C&F Electronics	1	N	10	5	0		Y		
124 Sydney Street	1	N	19	10	0		N	Not a proper pram ramp	
126 Sydney Street	1	N	17	12	5		N	Not a proper pram ramp	
140 Sydney Street	1	N	13	7	10		N		Pavement issues
FM Mechanical	1	N	11	10	0		Y		Pavement issues
160 Sydney Street	1	N	11	11	0		Y		
100 Sydney Street	1	N	11	8	30		Y	Not a proper pram ramp	
4 Lorne	1	N	13	6	20		N		
51 Ruth White Avenue	1	N	12	9	0	Join	Y		
40 Ruth White Avenue	1	N	13	7	0	Join	Y		
106 Wollombi Rd	1	N	12	6	20		Y	Cracked	
Denman Hospital	1	N	11	8	0		Y	Doing with road work	Footpath work
Denman Hospital	1	N	9	12	5-10		Y	Doing with road work	Footpath work
37 Virginia	1	N	10	8	0		Y	Comply with standard	
22 Palace St	1	N	10	5	0		Y	Footpath not continue	
RSL Club	2	N	10	6	0		Y	Filled with Coldmix	Need road work
Memorial Park	2	N	9	4	0		Y	Filled with Coldmix	Need road work
11 Crinoline St	2	N	9	6	0		Y	End of path - no corresponding footpath on the other side	
95 Virginia	2	N	7	8	0	Join	Y	End of path - no corresponding pram ramp	
27 Crinoline	2	N						Path stops at property boundary there is no pram ramp or connecting path	
Denman Hotel	2	N	8	6	0	Join	Y	Pram ramp is constructed of pavers	
Denman Hotel	2	N	8	5	0	Join	Y	Pram ramp is constructed of pavers	
NRMA	2	N	8	5	0	Join	Y	Pram ramp is constructed of pavers	
Fernandes	2	N	10	6	5-10	Join	Y	Pram ramp is constructed of pavers	
Edward Higgins Parkinson	2	N	16	5	5-10		Y	Pram ramp is constructed of pavers	
Edward Higgins Parkinson	2	N	9	6	0		Y	Pram ramp is constructed of pavers	
Denman Dapkoss	2	N	15	17	0		Y		Need road work
56 Ogilvie	2	N	10	6	5	Join	Y		Need road work
37 Ogilvie	2	N	14	11	0		Y	Road's angle actually 24%	Need road work
37 Ogilvie	2	N	11	6	30-40		Y	Filled with Coldmix previously due to service request	Need road work
Opposite 91 Virginia St	2	N	18	5	0	Join	Y		10 m <sup>2</sup> Footpath work and more earth work
Corner Ogilvie St	2	N	14	7	5	Join	Y		5 m <sup>2</sup> Footpath work and more earth work
62 Paxton	2	N						Path stops at property boundary there is no pram ramp or connecting path	
122 Brook		N	9	8	5		N	Not a proper pram ramp	
next to 121 Brook		N	10	7	0		Y	Path extends from George St Culdesac to Brook St	
99 Brook		N	7	6	40-50		N	Not a proper pram ramp	
79 Brook		N	9	4	0		Y	Path crosses Brook St near Sowerby St	
79 Brook		N	10	8	0		Y		
2 King		N	9	9	10		Y	Long slope to path - no other platform	
13 Brentwood		N	8	5	5		Y		
Hermitage Motel		N	7	13	0		N		
2 Bell		N	8	11	5-10		N	Small gap at join	Weeds need to be removed
Pedestrian crossing		N	8	6	0	Join	Y	No platform	
Centre island		N						The only way to get to the centre island is via the speed hump	Install ramps
Brookside		N	11	6	10		Y	Pram ramp to nowhere - end of constructed path	
Accidental		N	3	13	20		N	Not a proper pram ramp	
Senior Citizens to Simpson Park		N	8	6	10	Join	Y		
King Cycle Works		N	8	2	0	Join	Y		
Muscle Creek		N	5	10	0		N	Path leads to the road, there is no corresponding ramp opposite	Road edge to pavers
Prince of Wales Hotel		N	7	8	0		Y		
Matilda Service Station		N	11	7	0		Y		
Muswellbrook Motors		N	9	7	20		N		
112 Sydney Street		N	5	10	0		N		
114 Sydney Street		N	8	7	0		Y		
88 Sydney Street		N	7	13	10-15		N	Not a proper pram ramp	
Fitzgerald Park		N	6	9	0	Join	Y		
7 Haydon Street		N	9	7	0	Join	Y		
7 Jeune Street		N	8	8	0	Join	Y		
8 Sepoy Crescent		N	9	7	0	Join	Y		
6 Sepoy Crescent		N	11	6	0	Join	Y		
131 Queen Street		N	8	6	0	Join	Y	Path leads to the road, there is a pram ramp but no path opposite	
139 Queen Street		N	9	9	0	Join	Y	Path leads to the road, there is a pram ramp but no path opposite	
139 Queen Street		N	4	7	0	Join	Y		



Attachment B									
Sub Location	Priority	Standard Y/N	Ramp %	Road %	Lip mm	Join?	Wings Y/N	Comment	Extra work needed
141 Queen Street		N	7	9	0	Join	Y		
ANZ		N	9	2	0		Y		
ANZ		N	9	3	0		Y		
ANZ		N	9	3	0		Y		
RMS		N	3	10	30		N	Not a proper pram ramp	Cracked gutters
Commonwealth Bank		N	10	4	0	Join	Y		
Commonwealth Bank		N	9	4	0	Join	Y		
Caltex		N	8	7	0		Y		
Convent		N	8	7	0		Y		
Westpac		N	8	4	0	Join	Y		
Westpac		N	9	4	0		Y		
28 Sowerby		N	1	12	0		Y		
28 Sowerby		N	7	11	0		Y		
Marketplace		N	8	7	0		Y		
Convent		N	9	9	0		Y	Pedestrian Crossing	
Karoola Park East		N	7	8	5	Join	Y	No platform	
154/156 Queen		N	8	7	0		Y		
19/21 Burgundy		N	9	4	40-50		Y		
Hastings St Carpark		N	8	7	0	Join	Y		
near 100 Queen		N	7	4	0	Cracked	Y	On fill list	
near 15 Cabernet		N	8	7	0		Y	Does not line up to path across road	
near 12 Cabernet		N						No pram ramp - does not connect to kerb - foot worn path lines up with other path	
near 79 Shiraz		N	9	7	30		Y	Connects to driveway opposite to Karoola Park - no constructed path	
33/36 Chardonnay		N						No pram ramp - connects directly to mountable kerb	
opposite 62 Queen		N	9	unable	0		Y	Gutter full of dirt	
36/33 Chardonnay		N						No pram ramp - connects directly to mountable kerb	
17/19 Chablis		N						No pram ramp - path has steps - does not connect to kerb - does not line up with path opposite	
14/16 Chablis		N						No pram ramp - path has steps - does not connect to kerb - does not line up with path opposite	
9/11 Gamay		N						No pram ramp - does not connect to kerb	
25/27 Humphries		N						No pram ramp - path has steps - does not connect to kerb	
5/7 Humphries		N						No pram ramp - path has steps - does not connect to kerb	
18/83 Sowerby Ave/St		N						No pram ramp - path stops at property boundary	
Thiess Crescent		N						No pram ramp - connects directly to barrier kerb - 2 flat entrances to Weeraman	
St Aubins		N	13	9	0	Join	N	Ramp connects to A/C path - in poor condition	
St Aubins		N	10	10	0	Join	N	Ramp connects to A/C path - in poor condition	
Hermitage Motel (ped xing)		N	8	5	0		Y		
52 Maitland Street (ped xing)		N						Unconstructed driveway, no pram ramp as such, does not really line up with line marking, some barrier kerb	
44 Maitland		N	11	6	40	Join	Y		
42 Maitland		N	8	6	40	Join	N		
36 Maitland		N	8	4	0		N	Pits in footpath restrict wings	
Remington Hotel		N	10	7	0		Y	Does not line up to path across road	
25 Maitland		N	10	7	0		Y		
ex NPWS office		N	10	5	0		Y		
Mbk South Primary School		N	9	6	50		Y	Pram ramp is not the width of the linemarking on the road, some barrier kerb	
Tafe Western side (ped xing)		N	8	8	0		Y		
1 Thompson		N	11	7	30		Y		
147 Bridge St		N	7	9	40	Join	Y	Not a proper pram ramp	
Kurrajong Reserve East		N	7	3	0		Y		
Culdesac		N	12	6	0		Y		
Culdesac		N	9	5	0		Y		
Culdesac		N	8	7	0		Y		
Culdesac		N	11	6	0		Y		
76/76 Calgaroo Avenue		N						No pram ramp - connects directly to barrier kerb	
4 Lou Fisher		N	11	10	0	Join	Y	Mountable Kerb	
opposite 4 Lou Fisher		N	9	9	10	Join	Y	Mountable Kerb	
9/11 Cypress Place		N						No Pram Ramp - path connects to kerb over KIP	
Woollybutt Way		N	8	6	0		Y		
7/9 Casuarina		N						No pram ramp - connects directly to mountable kerb	
10/12 Casuarina		N						No pram ramp - connects directly to mountable kerb	
Muswellbrook Fair North		N	7	8	0	Join	N	No platform. Has tactiles	
22 Ironbark Road		N	8	7	0		Y	Future path	
2 Calgaroo Ave		N	8	8	0		N		
Wideland Toyota		N	8	5	0		N		
45 Rutherford Road		N	8	7	0		Y		
3 Plashett Close		N	8	7	5	Join	Y		
2 Angus Peebles Close		N	10	7	0	Join	Y	Weeds in join	
33/35 Calgaroo		N	8	5	0		Y		
Culdesac		N						No pram ramp - connects directly to mountable kerb	
75 Osborn		N	8	6	0	Join	Y		
75 Osborn		N	8	6	0	Join	Y		
1 Finnegan		N	8	6	0	Join	Y		

Attachment B									
Sub Location	Priority	Standard Y/N	Ramp %	Road %	Lip mm	Join?	Wings Y/N	Comment	Extra work needed
1 Finnegan		N	10	6	0	Join	Y		
52/58 Mitchell St		N	10	7	0	Join	Y		
2 Osborn Avenue		N	10	5	0	Join	Y		
5/7 Edinglassie		N						No pram ramp - path connects to kerb over KIP	
		N						No pram ramp	
		N						No pram ramp	
49 Osborn Ave		N	11	6	10	Join	Y		
5 Finnegan Cr		N	8	8	5	Join	Y		
3 Finnegan Cr		N	9	8	5	Join	Y		
69 Osborn Ave		N	8	6	0		Y		
67 Osborn Ave		N	10	6	0		Y		
53 Osborn Ave		N	10	6	5	Join	Y		
55 Osborn Ave		N	8	6	0	Join	Y		
55 Osborn Ave		N	8	6	10	Join	Y		
52-56 Mitchell St		N	10	7	0	Join	Y		
21 Barrett St		N	8	4	0		Y		
16 Fleming St		N	6	9	0		Y		
Mitsubishi		N	9	7	20		Y		
Mitsubishi		N	8	6	0	Join	Y		
McDonalds		N	9	6	0		Y		
Toyota		N	5	9	0		Y		
1C Bimbadeen		N	9	6	4	Join	Y		
Newcastle side of intersection		N	9	5	5	Join	Y		
34/35 Weemala		N						No pram ramp - path connects to kerb over KIP	
45 Bimbadeen		N	7	8	0	Join	Y	Path leads to the road, there is no pram ramp or path opposite	
1 John Howe		N	6	8	0	Join	Y		
31 Bimbadeen		N	10	9	0	Join	Y	Path leads to the road, there is no pram ramp or path opposite	
36 John Howe		N	5	8	5	Join	Y		
34 John Howe		N	8	6	0	Join	Y		
Opposite 1 Day		N	8	1	20	Join	Y	Not a proper pram ramp	
Opposite 17 Day		N	10	1	0	Join	Y	Cracked - not a proper pram ramp	
1 Hunt Pl		N	10	4	0	Join	Y		
2 Hunt Pl		N	12	1	0	Join	Y		
2 Hunt Pl		N	11	2	0	Join	Y		
19 Day		N	8	7	0	Join	Y		
16 Belmore		N	11	5	5	Join	Y		
2 Black		N	8	7	0	Join	Y		
12 Belmore		N	8	7	0	Join	Y		
2 Belmore		N	9	9	0	Join	Y		
5 Cunningham		N	8	7	0	Join	Y		
31 Wanaruah		N	6	9	0	Join	Y		
18 McGregor		N	8	8	0	Join	Y		
23 Wanaruah		N	6	8	0	Join	Y		
21 Wanaruah		N	11	7	0	Join	Y		
43 Henry Dangar		N	7	5	0	Join	Y		
6 Wattle		N	7	8	5	Join	Y		
2 Wattle		N	7	9	10	Join	Y		
5 Wattle		N	10	9	0	Join	Y		
19 Chivers		N	6	8	8	Join	Y		
Public Reserve at top		N	8	6	0	Join	Y	Path leads to the road, there is no pram ramp or path opposite	
20 Dixon		N						Car parked over pram ramp at time of inspection (28/10/2015)	
2 Loftberg (Opp 22 Dixon)		N	9	8	0	Join	Y		
22 Dixon		N	11	6	0	Join	Y		
22 Dixon		N	10	4	0	Join	Y		
24 Grant Miller		N	8	8	5	Join	Y		
30 Henry Danger		N	8	6	0	Join	Y		
40 Henry Dangar		N	7	8	0	Join	Y		
49/51 Tobruk		N	12	6	40	Join	N		
6/7 Glenbawn		N						Path stops at property boundary there is no pram ramp or connecting path	
7 Jersey Pl		N						Path stops at property boundary there is no pram ramp or connecting path	
5 Jersey Pl		N						Path stops at property boundary there is no pram ramp or connecting path	
10 Dangar Pl		N						Path stops at property boundary there is no pram ramp or connecting path	
17 Dangar Pl		N						Path stops at property boundary there is no pram ramp or connecting path	
7/9 Dangar Pl		N						Path stops at property boundary there is no pram ramp or connecting path	
96/94 Wollombi Rd		N	9	7	20		Y	Pram ramp does not connect to path	
5/6 Paterson St		N						Path stops at property boundary there is no pram ramp or connecting path	
57/Mt Providence		N	12	unable			Y	Pram ramp full of dirt from scour next to path - could not measure (5/2/16)	
End of road		N						No pram ramp from connector path	
Common Rd		N	16	4	0		N	Kerb only	
53a Queen St		N	7	8	0		Y	Debris in gutter	
Common Rd		N						Path eroded, pram ramp full of debris - could not measure (5/2/16)	
opposite 59 Queen St		N						Path stops short of Queen St	

Attachment B									
Sub Location	Priority	Standard Y/N	Ramp %	Road %	Lip mm	Join?	Wings Y/N	Comment	Extra work needed
King St		N						Path stops short of Queen St	
Visitor Information Centre		Y	3	5	0	Join	Y	Pram ramp is constructed of pavers	
Visitor Information Centre		Y	6	6	0	Join	Y	Pram ramp is constructed of pavers	
NRMA		Y	7	4	0	Join	Y	Pram ramp is constructed of pavers	
Royal Hotel		Y	5	6	10	Join	Y	Pram ramp is constructed of pavers	
Royal Hotel		Y	5	6	0	Join	Y	Pram ramp is constructed of pavers	
Fernandes		Y	6	5	5-10	Join	Y	Pram ramp is constructed of pavers	
Denman Dapkos		Y	6	6	0		Y		
21 Virginia		Y	4	7	0		Y	End of path - no corresponding pram ramp	
39 Virginia		Y	7	7	0		Y		
17 Turtle		Y	7	6	0		Y		
next to 10 Macauley St		Y	7	7	10	Join	Y		
36 Palace St		Y	4	6	5	Join	Y		
Parish Hall		Y	4	4	20	Join	Y		
22 Paxton St		Y	7	6	0		Y		
Corner Virginia St		Y	7	5	5	Join	Y		
51 Almond St (East)		Y	6	5	0	Join	Y		
51 Almond St (West)		Y	1	4	0	Join	Y		
RSL Club		Y	5	6	0	Join	Y		
22 Crinoline St		Y	6	6	0	Join	Y		
Denman Public School		Y	6	8	40	Join	Y	Layback design	
2 King	1	Y	4	8	10		Y	No platform - 8% over peak	
next to 20 Lorne St		Y	6	5	0		Y	Pram ramp cracked and separating from path - needs replacement	
98 Brook		Y	5	12	40		N	Not a proper pram ramp	
100 Brook		Y	4	5	30		N	Not a proper pram ramp	
97 Brook		Y	7	7	0		N		
80 Brook		Y	7	4	0		Y		
80 Brook		Y	7	4	0		Y	Path crosses Brook St near Sowerby St	
Culdesac		Y	7	4	20		N	Path extends from George St Culdesac to Brook St - 2 x bollards	
Near Roundabout		Y	7	7	30		Y	Not joined to path, extends across King Street towards Brentwood	Connect to path
7 King		Y	7	6	0		Y		
Mbk High School		Y	5	9	0		Y	Ramp replaced 2016 with Dolahenty footpath	
Mbk High School		Y	6	6	0		Y	Footpath joins to A/C path from school	Replace A/C with Concrete
11 Brentwood		Y	6	9	5		Y		Road pavement needs repair
2 Bell (across Bell)		Y	6	7	5-10		N	Small gap at join	
Pedestrian crossing		Y	3	3	0		N		
Path near park gates		Y	6	6	5		Y	Path leads to the road, there is no corresponding ramp opposite	
Rodeo Ranch		Y	6	6	0	Join	Y	Pavement needs repair	
Valley Hotel		Y	7	7	0		Y		
John Flood		Y	4	5	0	Join	N		
Royal Hotel		Y	5	6	0	Join	N		
Millers		Y	5	2	0	Join	Y		
Court House		Y	4	2	0	Join	Y		
Wellness Chiropractic		Y	4	6	0	Join	Y		
TLE		Y	7	2	0	Join	Y		
Muswellbrook Hotel		Y	4	5	0		Y		
Simpson Park		Y	4	5	0		Y	Path leads to the road, there is no corresponding ramp opposite	There are no pram ramps on the interchange
Simpson Park to Senior Citizens		Y	7	1	0	Join	Y		
Sydney Street Tyres		Y	4	3	10		Y		
68 Sydney Street		Y	6	6	10-20		Y		
90 Sydney Street		Y	5	5	20		N	Not a proper pram ramp	
78 Sydney Street		Y	7	5	0	Join	Y		
76 Sydney Street		Y	7	6	0	Join	Y		
Opposite Pool		Y	6	4	0	Join	Y	Path to nowhere (on new footpath list?)	
151 Queen Street		Y	5	3	0	Join	N		
153 Queen Street		Y	7	5	0	Join	Y		
163 Queen Street		Y	5	5	0	Join	Y		
165 Queen Street		Y	6	6	0	Join	Y		
10 Lonhro Place		Y	7	5	0	Join	Y		
129 Queen Street		Y	6	6	5-10	Join	Y	Path leads to the road, there is a pram ramp but no path opposite	Some kerb damaged
141 Queen Street		Y	7	7	0	Join	Y		
4 Northview Circuit		Y	7	6	0	Join	Y	Path leads to the road, there is a pram ramp but no path opposite	
6 Northview Circuit		Y	6	6	0	Join	Y		
5 Northview Circuit		Y	7	7	0	Join	Y		
5 Northview Circuit		Y	7	6	0	Join	Y	Path leads to the road, there is a pram ramp but no path opposite	
Muswellbrook Holden		Y	6	1	0	Join	Y		
Loxton House		Y	5	7	0		Y		
Shell		Y	9	7	10	Join	Y		
Hungry Jacks		Y	9	7	5	Join	Y		
Centabrook		Y	7	7	0		Y		
Happy Tooth		Y	7	2	0	Join	Y		
1 Parkinson		Y	7	4	0		Y		
62 Queen		Y	7	5	0	Join	Y		
Karoola Park West		Y	6	6	5	Join	Y	Not connected to a constructed path	
68 Carl		Y	4	1	0	Join	Y	No Platform - crosses blister to no constructed path	

Attachment B									
Sub Location	Priority	Standard Y/N	Ramp %	Road %	Lip mm	Join?	Wings Y/N	Comment	Extra work needed
1 Parkinson		Y	7	4	0		Y		
62 Queen		Y	7	5	0	Join	Y		
Karoola Park West		Y	6	6	5	Join	Y	Not connected to a constructed path	
68 Carl		Y	4	1	0	Join	Y	No Platform - crosses blister to no constructed path	
23 Cook		Y	8	6	0	Cracked	Y	Cracked	
41 Cook		Y	6	5	0		Y		
2 Koombahla		Y	6	5	0		N		
10/12 Merlot		Y	6	6	0	Join	Y		
3 Lorne		Y	7	6	60		N	Not a proper pram ramp	
Pizza Bella		Y	5	6	0		Y		
43 Maitland		Y	6	5	0		Y		
Tafe (ped xing)		Y	7	7	0		Y		
Tafe Eastern side (ped xing)		Y	6	6	0		Y		
208 Bridge		Y	7	5	10	Join	Y	Leads to 3 steps. Also has a paved path.	
11 St Heliers St		Y	7	6	0		N	No platform	
Kurrajong Reserve West		Y	6	4	0		Y		
Culdesac		Y	6	6	0		Y		
Culdesac		Y	7	4	0		Y		
Harvey Norman		Y	7	2	0		Y		
Aldi		Y	7	6	10-15		Y	Small pram ramp pointing diagonally across Rutherford Road	
Aldi		Y	7	3	0		Y		
37 Rutherford Road		Y	7	4	0		Y		
Muswellbrook Fair South		Y	5	4	0		N	Has tactiles	
1 Rutherford Road		Y	7	7	0		Y		
Wideland Mitsubishi		Y	5	3	0		N		
Wideland Toyota		Y	5	6	0		Y		
77 Osborn Avenue		Y	7	6	0		Y		
51 Osborn Ave		Y	6	7	5	Join	Y		
53 Osborn Ave		Y	7	6	15	Join	Y		
16-18 Jordan St (Flats)		Y	6	1	0		Y		
19 Jordan St		Y	7	5	0		Y		
18 Mitchell St		Y	7	5	0	Join	Y		
25 Mitchell St		Y	7	6	0	Join	Y		
20 Barrett St		Y	7	2	4		Y		
22 Skellatar St		Y	6	4	0		Y		
15 Skellatar St		Y	6	7	0		Y		
43 Bimbadeen		Y	6	6	0	Join	Y	Weeds in join	
2 John Howe		Y	6	7	0	Join	Y		
31 Bimbadeen		Y	7	4	0	Join	Y		
24 Bimbadeen		Y	6	5	0	Join	Y	Path leads to the road, there is no pram ramp or path opposite	
28 Bimbadeen		Y	5	4	10	Join	Y		
Corner Henry Dangar Drive		Y	6	5	5	Join	Y		
Corner Day St (Reserve)		Y	7	5	0	Join	Y		
Opposite 5 Day		Y	5	1	0	Join	Y	Weeds in join - not a proper pram ramp	
Opposite 19 Day		Y	5	1	0	Join	Y		
2 Day St		Y	7	1	0	Join	Y		
17 Day		Y	2	3	0	Join	Y		
17 Day		Y	5	7	5	Join	Y	Path leads to the road, there is no pram ramp or path opposite	
11 Black		Y	1	6	0	Join	Y		
6 Dixon		Y	5	3	0	Join	Y		
4 Dixon		Y	6	4	0	Join	Y		
15 Cunningham		Y	5	7	0	Join	Y		
5 Day St		Y	7	7	0	Join	Y	Path leads to the road, there is no pram ramp or path opposite	
5 Day St		Y	2	1	0	Join	Y		
1 Day St		Y	4	4	5	Join	Y		
1 Day St		Y	6	6	0	Join	Y	Path leads to driveway (with lip)	
24 Henry Dangar		Y	7	6	0	Join	Y		
1 Wanaruah Cct		Y	7	5	0	Join	Y		
2 Mussell St		Y	6	5	0	Join	Y		
11 Henry Dangar		Y	5	0	10	Join	Y		
2 McGregor St		Y	6	1	0	Join	Y		
5 Henry Dangar Dr		Y	5	0	10	Join	Y		
Corner Wanaruah (Reserve)		Y	7	7	0	Join	Y		
Opposite 31 Wanaruah		Y	4	3	10	Join	Y		
19 McGregor		Y	6	7	0	Join	Y		
5 Wanaruah		Y	7	4	20	Join	Y		
1 Wanaruah Cct		Y	7	1	5	Join	Y		
24 Wattle		Y	7	7	10	Join	Y		
40 Chivers		Y	7	7	0	Join	Y		
45 Henry Dangar		Y	7	7	0	Join	Y		
2 Wanaruah		Y	7	2	0	Join	Y		
24 Henry Dangar		Y	6	1	0	Join	Y		
23 Dixon		Y	4	3	0	Join	Y		

Attachment B									
Sub Location	Priority	Standard Y/N	Ramp %	Road %	Lip mm	Join?	Wings Y/N	Comment	Extra work needed
17 Grant Miller		Y	3	4	0	Join	Y		
1 Dixon		Y	6	2	0	Join	Y		
		Y						Path goes from driveway to driveway in front of public housing	
Newcastle Permanent	R						Y	Has been replaced - to be rechecked 31/5/16	
Newcastle Permanent	R						Y	Has been replaced - to be rechecked 31/5/16	
Crossover			5	5	0		Y	Gutter full of dirt	
Crossover			10	7	0		Y	Gutter full of dirt	
Crossover			12	7	10	Join	Y		
Crossover			11	5	10	Join	Y		
ISC		Y	8		0		Y	New with Rutherford/Cassidy path 2016	
Crossover		Y	8		0		Y	New with Rutherford/Cassidy path 2016	
Crossover		Y	7		0		Y	New with Rutherford/Cassidy path 2016	
opp 3		N	15		0		N	New path from existing to King St 2016	

Attachment C KERB & GUTTER REPLACEMENT PROGRAM										
LOCATION	Town	LENGTH	Priority	Pvmt Work	Tree Work	TYPE	LAYBACK	WIDTH INC PAV	ADDITIONAL WORK	Comment
Cassidy Avenue	Muswellbrook	30	1		Y					
Lower William St	Muswellbrook	20	1							
Queen St	Muswellbrook	22	1				2 x	2		2 x driveways
McClintock Dr	Muswellbrook	10	1		Y				Need road work, need tree removal	st & KIP (being lifted)
Mitchell St	Muswellbrook	130	1				Y	2		
Towarri St	Muswellbrook	10	1					2		
McClintock Dr	Muswellbrook	12	1		Y		Y			dw st
Chardonay St	Muswellbrook	20	1							
Cabernet St	Muswellbrook	15	1		Y			2	Street Tree - old layback can be removed	
Cabernet St	Muswellbrook	20	1				Y			
Chablis Cl	Muswellbrook	10	1		Y	SA Kerb		2.5	Street tree	
Cousins St	Muswellbrook	10	1							
Cousins St	Muswellbrook	5	1		Y			2		
Queen St	Muswellbrook	2	1							
Foley St	Muswellbrook	10	1		Y					St
Foley St	Muswellbrook	10	1		Y					st
Parkinson Ave	Muswellbrook	35	1				Y	2		
Towarri St	Muswellbrook	40	1	2			Y			
Traminer St	Muswellbrook	10	1							
Lofberg Ct	Muswellbrook	15	1							Kerb and gutter has sunk and is retaining water, also a cracked portion
Hill St	Muswellbrook	5	1		Y			2		st
Carl St	Muswellbrook	10	1		Y					
Clifford St	Muswellbrook	2	1							Cracking
McClintock Dr	Muswellbrook	20	1							
McClintock Dr	Muswellbrook	5	1							
Mitchell St	Muswellbrook	10	1					2		
Boronia Dr	Muswellbrook	15	1						Tree removed	
Brecht St	Muswellbrook	10	1							
Acacia Dr	Muswellbrook	10	1		Y					St
Mitchell St	Muswellbrook	10	1					2		
Brook St	Muswellbrook	10	1					2		
Kamilaroi St	Muswellbrook	25	1	Y			Y	2		
Anzac Pde	Muswellbrook	520	1	Y				10	Pavement work also needed/Rehab	Anzac Parade is on the Rehab program
Palace St	Denman	130	1	Y	Y	SA Kerb	Y	4	Pavement work also needed/Rehab	With pavement rehab
Merton St	Denman	100	1						Rehab project? - total length to be checked	
Merton St	Denman	30	1				Y		Rehab project? - total length to be checked	Across driveway
Wilder St	Muswellbrook	10	1							st
Wilder St	Muswellbrook	10	1		Y					st - holding water
Wilder St	Muswellbrook	10	1		Y					st - holding water
Wilder St	Muswellbrook	10	1		Y					st - holding water
Ruth White Ave	Muswellbrook	15	1					2		
Rutherford Rd	Muswellbrook	10	1							
Hunter St	Muswellbrook	10	1	Y				2		Water holding from top of median
Hunter St	Muswellbrook	10	1							st - At median
Hunter St	Muswellbrook	10	1		Y					st - At median
Brook St	Muswellbrook	10	1							
Brook St	Muswellbrook	10	1							
Paxton St	Denman	80	1		Y				Possibly needs a stormwater pit and pipes rather than replacing the kerb and gutter	Water holding in gutter - also some problems with trees - Service request raised by DDDA 19/7/2014
Paxton St	Denman	20	1						Possibly needs a stormwater pit and pipes rather than replacing the kerb and gutter	Water holding at gutter - school bus zone - service request raised by DDDA 19/7/2014, service request from Errol George 8/12/14
Maitland St	Muswellbrook	5	1						Missing section of sandstone	
McClintock Dr	Muswellbrook	30	1				Y			
Queen St	Muswellbrook	120	1				Y	2		with pavement
Queen St	Muswellbrook	2	1							Chip
Chardonay St	Muswellbrook	10	1							
Chardonay St	Muswellbrook	5	1							
Carl St	Muswellbrook	5	1					2		
Roger St	Muswellbrook	10	1				Y			
Sowerby Ave	Muswellbrook	5	1							
Palace St	Denman	20	1							
Merriwa Road	Sandy Hollow	2	1							Broken K&G completely out of the ground
Foley St	Muswellbrook	10	1		Y					st
Towarri St	Muswellbrook	30	1							
Cassidy Avenue	Muswellbrook	10	1							
Ruth White Ave	Muswellbrook	10	1							



Attachment C										
KERB & GUTTER REPLACEMENT PROGRAM										
LOCATION	Town	LENGTH	Priority	Pvmt Work	Tree Work	TYPE	LAYBACK	WIDTH INC PAV	ADDITIONAL WORK	Comment
Fleming St	Muswellbrook	20	1							
Fleming St	Muswellbrook	10	1							
Anzac Pde	Muswellbrook	10	1/2							Anzac Parade is on the Rehab program
Humphries St	Muswellbrook	10	1/2							
Lexia St	Muswellbrook	10	1/2				Y	2		
Lexia St	Muswellbrook	50	1/2				Y	2		
Maitland St	Muswellbrook	10	1/2							Cracked and chipped
Maitland St	Muswellbrook	5	1/2		Y					st
Osborn Ave	Muswellbrook	10	1/2				Y			Cracked and damaged
Rutherford Rd	Muswellbrook	10	1/2	Y				2		New kerb?? Turning where pedestrians start the path from carpark
Lome St	Muswellbrook	5	1/2				Y			
Acacia Dr	Muswellbrook	10	2		Y		Y			ST
Acacia Dr	Muswellbrook	10	2		Y	SA Kerb		2		
Almera Glen	Muswellbrook	14	2					2	Mountable K&G near driveway	
Anzac Pde	Muswellbrook	5	2							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	10	2							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	5	2							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	10	2							Anzac Parade is on the Rehab program
Birralee St	Muswellbrook	5	2		Y				St	
Boronia Dr	Muswellbrook	20	2		Y					Street tree x 2
Boronia Dr	Muswellbrook	5	2							
Bligh St	Muswellbrook	5	2							
Brentwood St	Muswellbrook	10	2							
Burgundy St	Muswellbrook	10	2		Y					
Cabernet St	Muswellbrook	24	2					2		
Cedar Place	Muswellbrook	10	2		Y					st
Chablis Cl	Muswellbrook	20	2				Y			
Chardonnay St	Muswellbrook	50	2							
Chardonnay St	Muswellbrook	20	2							
Claret Ave	Muswellbrook	10	2				Y			
Claret Ave	Muswellbrook	20	2					2		
Clendinnig St	Muswellbrook	15	2				Y	2		
Cousins St	Muswellbrook	20	2				Y	2	Monitor	
Ogilvie Street	Denman	50	2							Left side of entrance
Dumaresq St	Muswellbrook	16	2					2		
Dumaresq St	Muswellbrook	5	2					2		
Fleming St	Muswellbrook	5	2					2		
Hastings St	Muswellbrook	10	2				Y	2		
Hastings St	Muswellbrook	30	2				Y			
Hill St	Muswellbrook	5	2							ex street tree
Hunter St	Muswellbrook	5	2							
Hunter St	Muswellbrook	10	2		Y					
Hunter St	Muswellbrook	30	3							
Hunter Tce	Muswellbrook	10	2							
Hyde St	Denman	10	2							
Isabel St	Denman	5	2		Y					
Isabel St	Denman	5	2		Y					
Isobella St	Muswellbrook	20	2						Stormwater pit	
Isobella St	Muswellbrook	40	2							
Kamilaroi St	Muswellbrook	10	2		Y					st
Karoola Ave	Muswellbrook	5	2		Y					
Karoola Ave	Muswellbrook	10	2							
Koombahla St	Muswellbrook	20	2							
Lexia St	Muswellbrook	40	2				Y			
Lexia St	Muswellbrook	15	2				Y			
Lexia St	Muswellbrook	20	2				Y	2		
Lexia St	Muswellbrook	40	2							
McClintock Dr	Muswellbrook	10	2		Y					st
Maitland St	Muswellbrook	5	2							
Maitland St	Muswellbrook	25	2		Y				Holding water	5 x street trees - various locations
Marlock Pl	Muswellbrook	10	2		Y					
Mitchell St	Muswellbrook	20	2	y			y	2		Holding water
Palace St	Denman	20	2				Y			
Palace St	Denman	10	2							
Parkinson Ave	Muswellbrook	5	2							st
Queen St	Muswellbrook	10	2		Y					st
Queen St	Muswellbrook	20	2					2		with pavement
Rutherford Rd	Muswellbrook	10	2		Y					Large tree in showground

Attachment C										
KERB & GUTTER REPLACEMNT PROGRAM										
LOCATION	Town	LENGTH	Priority	Pvmt Work	Tree Work	TYPE	LAYBACK	WIDTH INC PAV	ADDITIONAL WORK	Comment
Sydney Street	Muswellbrook	20	2	Y				2		
Sydney Street	Muswellbrook	10	2							
Sylvana St	Muswellbrook	10	2							
Ted Clay St	Muswellbrook	5	2						Cracked	
Towarri St	Muswellbrook	15	2		Y					
Turanville Ave	Muswellbrook	20	2							
Virginia St	Denman	10	2				Y			
Virginia St	Denman	10	2		Y					Cracked layback
Virginia St	Denman	30	2	Y				2	Rutting next to Kerb and gutter along length	
Wilder St	Muswellbrook	5	2		Y					st - holding water
St James Cr	Muswellbrook	5	2/3							
St James Cr	Muswellbrook	30	2/3							
Acacia Dr	Muswellbrook	5	3		Y					ST
Acacia Dr	Muswellbrook	10	3						Tree removed	ST
Acacia Dr	Muswellbrook	5	3							
Acacia Dr	Muswellbrook	5	3							
Acacia Dr	Muswellbrook	5	3							
Acacia Dr	Muswellbrook	10	3				Y	2		
Acacia Dr	Muswellbrook	5	3							
Acacia Dr	Muswellbrook	5	3		Y					
Adams St	Muswellbrook	5	3							
Adams St	Muswellbrook	5	3							
Adams St	Muswellbrook	20	3							
Adams St	Muswellbrook	30	3							
Adams St	Muswellbrook	5	3		Y					
Adams St	Muswellbrook	5	3							
Adams St	Muswellbrook	5	3							
Adams St	Muswellbrook	5	3		Y					
Adams St	Muswellbrook	10	3							
Almond St	Denman	1	3							Cracked and broken
Anzac Pde	Muswellbrook	4	3							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	10	3							
Anzac Pde	Muswellbrook	10	3							
Beech St	Muswellbrook	5	3		Y					
Bimbadeen Dr	Muswellbrook	4	3							Chips
Birralee St	Muswellbrook	5	3							
Bligh St	Muswellbrook	25	3		Y					
Bligh St	Muswellbrook	50	3							
Bligh St	Muswellbrook	5	3		Y					
Bligh St	Muswellbrook	7	3		Y			2	Street Tree	
Bligh St	Muswellbrook	5	3						Street Tree	
Bligh St	Muswellbrook	10	3							
Bligh St	Muswellbrook	27	3					2	Monitor	
Bloodwood Road	Muswellbrook	2	3		Y					
Boronia Dr	Muswellbrook	5	3		Y					
Boronia Dr	Muswellbrook	5	3		Y					
Boronia Dr	Muswellbrook	20	3							
Bowman St	Muswellbrook	6	3					2		
Brecht St	Muswellbrook	10	3							
Brecht St	Muswellbrook	40	3							
Brecht St	Muswellbrook	10	3							
Brentwood St	Muswellbrook	5	3							
Brook St	Muswellbrook	10	3		Y					ST
Brook St	Muswellbrook	5	3							Cracked and damaged by vehicle movement
Brook St	Muswellbrook	5	3							
Burgundy St	Muswellbrook	5	3							
Burgundy St	Muswellbrook	30	3							
Cabernet St	Muswellbrook	5	3		Y			2		
Cabernet St	Muswellbrook	30	3	Y				2		
Cabernet St	Muswellbrook	10	3					2		
Cabernet St	Muswellbrook	20	3					2	Monitor	
Cabernet St	Muswellbrook	5	3							
Carl St	Muswellbrook	3	3							
Cassidy Avenue	Muswellbrook	10	3		Y					
Cassidy Avenue	Muswellbrook	5	3							
Cassidy Avenue	Muswellbrook	5	3							Cracking
Cassidy Avenue	Muswellbrook	5	3							Cracking
Chablis Cl	Muswellbrook	10	3							
Chardonnay St	Muswellbrook	10	3							

Attachment C										
KERB & GUTTER REPLACEMENT PROGRAM										
LOCATION	Town	LENGTH	Priority	Pvmt Work	Tree Work	TYPE	LAYBACK	WIDTH INC PAV	ADDITIONAL WORK	Comment
Claret Ave	Muswellbrook	4	3							
Claret Ave	Muswellbrook	10	3							
Claret Ave	Muswellbrook	5	3							Crack
Claret Ave	Muswellbrook	5	3		Y					
Cousins St	Muswellbrook	5	3							
Cousins St	Muswellbrook	5	3		Y					st - holding water
Cousins St	Muswellbrook	2	3		Y					
Cousins St	Muswellbrook	10	3							
Cousins St	Muswellbrook	15	3				Y			
Crete St	Muswellbrook	10	3							
Crinoline St	Denman	10	3							Cracked and damaged at corner
Donaldson St	Muswellbrook	5	3							
Donaldson St	Muswellbrook	10	3							
Dumaresq St	Muswellbrook	5	3					2		st
Flanders Ave	Muswellbrook	10	3	Y				2		
Flanders Ave	Muswellbrook	20	3							
Fleming St	Muswellbrook	10	3	Y				2		
Fleming St	Muswellbrook	5	3	Y				2		
Fleming St	Muswellbrook	20	3							
Fleming St	Muswellbrook	5	3							
Foley St	Muswellbrook	5	3		Y					st
Foley St	Muswellbrook	5	3							
Forbes St	Muswellbrook	5	3							
Ford Street	Muswellbrook	10	3		Y					st
Ford Street	Muswellbrook	5	3		Y					st
Ford Street	Muswellbrook	5	3		Y					st
Ford Street	Muswellbrook	5	3		Y					
Glenbawn Pl	Muswellbrook	10	3					2		
Grevillea St	Muswellbrook	10	3	Y						
Haydon St	Muswellbrook	40	3							
Hastings St	Muswellbrook	30	3							
Hastings St	Muswellbrook	10	3		Y					st
Hermitage Place	Muswellbrook	5	3		Y					
Hill St	Muswellbrook	93	3							
Hill St	Muswellbrook	5	3		Y					
Hill St	Muswellbrook	5	3		Y					
Hill St	Muswellbrook	10	3							
Hill St	Muswellbrook	10	3		Y					
Hill St	Muswellbrook	5	3		Y					
Humphries St	Muswellbrook	1	3						Damaged at pram ramp	
Humphries St	Muswellbrook	10	3							
Hyde St	Denman	5	3							
Ironbark Rd	Muswellbrook	5	3		Y					
Isabel St	Denman	5	3		Y					
Isobella St	Muswellbrook	10	3							
Isobella St	Muswellbrook	10	3				Y			
Isobella St	Muswellbrook	10	3							
Isobella St	Muswellbrook	40	3							
Isobella St	Muswellbrook	10	3							
Isobella St	Muswellbrook	10	3							
Isobella St	Muswellbrook	10	3							
Isobella St	Muswellbrook	5	3							
Isobella St	Muswellbrook	30	3							
Isobella St	Muswellbrook	15	3							
Kamilaroi St	Muswellbrook	5	3					2		
Kamilaroi St	Muswellbrook	5	3		Y					st
Kamilaroi St	Muswellbrook	30	3							
Kamilaroi St	Muswellbrook	1	3							
Kamilaroi St	Muswellbrook	5	3		Y					
Kamilaroi St	Muswellbrook	5	3							
Karoola Ave	Muswellbrook	10	3							
Karoola Ave	Muswellbrook	5	3							
Karri Pl	Muswellbrook	5	3							
Kenilworth St	Denman	1	3							Cracked and broken
Kenilworth St	Denman	5	3							
Kenilworth St	Denman	10	3							
King St	Muswellbrook	15	3							
King St	Muswellbrook	10	3							st
King St	Muswellbrook	5	3		Y					
King St	Muswellbrook	5	3							

Attachment C										
KERB & GUTTER REPLACEMENT PROGRAM										
LOCATION	Town	LENGTH	Priority	Pvmt Work	Tree Work	TYPE	LAYBACK	WIDTH INC PAV	ADDITIONAL WORK	Comment
King St	Muswellbrook	5	3							
Kokoda St	Muswellbrook	20	3							
Koombahla St	Muswellbrook	3	3							
Lae St	Muswellbrook	5	3		Y					
Lexia St	Muswellbrook	10	3						Monitor	
Lome St	Muswellbrook	5	3							At spring outlet
Lome St	Muswellbrook	5	3		Y					
Lower Hill St	Muswellbrook	5	3							
Mahogany Ave	Muswellbrook	10	3		Y					
Mahogany Ave	Muswellbrook	5	3							
Maitland St	Muswellbrook	5	3							
Maitland St	Muswellbrook	10	3		ex					Street tree removed
Maitland St	Muswellbrook	20	3							st
Maitland St	Muswellbrook	10	3		Y	Sandstone				st
Maitland St	Muswellbrook	20	3		Y	Sandstone			Holding water	
Maitland St	Muswellbrook	5	3							
Maitland St	Muswellbrook	5	3							
Maitland St	Muswellbrook	10	3							
Maitland St	Muswellbrook	5	3							
Maitland St	Muswellbrook	5	3							
Maitland St	Muswellbrook	5	3		Y					st
Marlock Pl	Muswellbrook	5	3							Dip in kerb
Martindale Street	Denman	5	3							
Martindale Street	Denman	10	3							
Mataro Ave	Muswellbrook	80	3				Y			
McClintock Dr	Muswellbrook	20	3					2		
McClintock Dr	Muswellbrook	20	3							
McClintock Dr	Muswellbrook	10	3							
McClintock Dr	Muswellbrook	30	3	Y				2		
McClintock Dr	Muswellbrook	10	3							
Merriwa Road	Sandy Hollow	40	3	y				2		
Midanga Ave	Muswellbrook	5	3							
Millard Cl	Muswellbrook	15	3							
Mitchell St	Muswellbrook	5	3							
Mitchell St	Muswellbrook	10	3							
Mitchell St	Muswellbrook	5	3							
Mitchell St	Muswellbrook	10	3							
Mitchell St	Muswellbrook	20	3	y				2		
Ogilvie Street	Denman	5	3							st
Ogilvie Street	Denman	20	3							Right side of entrance
Oorange St	Muswellbrook	5	3		Y					
Osborn Ave	Muswellbrook	2	3							Chip
Palace St	Denman	20	3							
Palace St	Denman	5	3							
Palace St	Denman	20	3							
Palace St	Denman	10	3							
Parkinson Ave	Muswellbrook	10	3							
Parkinson Ave	Muswellbrook	5	3							
Paxton St	Denman	10	3							
Paxton St	Denman	5	3							
Paxton St	Denman	30	3				Y	2		
Paxton St	Denman	60	3							
Paxton St	Denman	10	3							
Paxton St	Denman	20	3							
Paxton St	Denman	20	3							
Paxton St	Denman	5	3							
Paxton St	Denman	5	3							
Peppermint St	Muswellbrook	5	3							
Queen St	Muswellbrook	10	3							
Queen St	Muswellbrook	5	3		Y					
Queen St	Muswellbrook	2	3							
Queen St	Muswellbrook	10	3							dw
Queen St	Muswellbrook	20	3							redirecting water from pit
Queen St	Muswellbrook	20	3							redirecting water from pit
Queen St	Muswellbrook	5	3		Y					
Queen St	Muswellbrook	10	3		x 2					two sections of kerbing damaged
Queen St	Muswellbrook	5	3		Y					
Richmond St	Muswellbrook	15	3					2	Monitor	
Riesling St	Muswellbrook	10	3	Y				2		

Attachment C										
KERB & GUTTER REPLACEMNT PROGRAM										
LOCATION	Town	LENGTH	Priority	Pvmt Work	Tree Work	TYPE	LAYBACK	WIDTH INC PAV	ADDITIONAL WORK	Comment
Riesling St	Muswellbrook	10	3					2		
Roger St	Muswellbrook	5	3				Y	2		
Roger St	Muswellbrook	2	3					2		
Roger St	Muswellbrook	20	3		Y					ST
Rosemount Road	Denman	10	3					2		
Rutherford Rd	Muswellbrook	5	3							
Rutherford Rd	Muswellbrook	1	3							Chip
Ruth White Ave	Muswellbrook	5	3							Crack
Ruth White Ave	Muswellbrook	10	3							
Ruth White Ave	Muswellbrook	5	3		Y				New Pit needed	
Ruth White Ave	Muswellbrook	5	3							
Sauterne Cl	Muswellbrook	5	3		Y					
Sauterne Cl	Muswellbrook	10	3							
Scott St	Muswellbrook	5	3		Y					st
Scott St	Muswellbrook	10	3							
Scott St	Muswellbrook	10	3							
Scott St	Muswellbrook	5	3							
Scott St	Muswellbrook	5	3		Y					
Shaw Cr	Muswellbrook	10	3		Y			2		
Sheppard Ave	Muswellbrook	15	3			SA Kerb		2		
Sheppard Ave	Muswellbrook	15	3			SA Kerb		2		
Shiraz St	Muswellbrook	2	3		Y			2		
Shiraz St	Muswellbrook	5	3		Y					
Shiraz St	Muswellbrook	5	3				Y	2	Monitor	
Shiraz St	Muswellbrook	10	3		Y					
Sowerby Ave	Muswellbrook	10	3	Y	Y		Y	2		st
Sowerby Ave	Muswellbrook	5	3	Y				2		st
Sowerby St	Muswellbrook	5	3							
Sowerby St	Muswellbrook	25	3					2		
Sowerby St	Muswellbrook	5	3							
St James Cr	Muswellbrook	30	1				Y			
St James Cr	Muswellbrook	10	3					2		
St James Cr	Muswellbrook	5	3							
St James Cr	Muswellbrook	10	3				Y			
St James Cr	Muswellbrook	5	3		Y					
St James Cr	Muswellbrook	5	3							
St James Cr	Muswellbrook	10	3				Y			
Sydney Street	Muswellbrook	3	3							
Sydney Street	Muswellbrook	10	3							
Sydney Street	Muswellbrook	5	3		Y					
Sydney Street	Muswellbrook	5	3		Y					
Sylvana St	Muswellbrook	10	3							
Sylvana St	Muswellbrook	10	3							
Sylvana St	Muswellbrook	10	3							
Sylvana St	Muswellbrook	10	3							
Thompson St	Muswellbrook	15	3							
Thompson St	Muswellbrook	5	3							
Thompson St	Muswellbrook	10	3							
Thompson St	Muswellbrook	5	3							
Tindale St	Muswellbrook	10	3		Y			2		
Tindale St	Muswellbrook	20	3		Y			2	Including KIP	st
Tindale St	Muswellbrook	6	3				Y	2		
Tindale St	Muswellbrook	10	3							
Tindale St	Muswellbrook	10	3							
Tindale St	Muswellbrook	30	3							
Tindale St	Muswellbrook	10	3							
Tindale St	Muswellbrook	5	3				y			
Tobruk Ave	Muswellbrook	5	3		Y					st
Tobruk Ave	Muswellbrook	5	3		Y					st
Tobruk Ave	Muswellbrook	10	3		Y					
Tobruk Ave	Muswellbrook	5	3		Y					
Tobruk Ave	Muswellbrook	10	3							
Towarri St	Muswellbrook	5	3							
Towarri St	Muswellbrook	10	3							
Towarri St	Muswellbrook	10	3							
Traminer St	Muswellbrook	20	3							
Tuart Cl	Muswellbrook	5	3							Water holding in kerbing
Tumer St	Denman	5	3							
Tumer St	Denman	10	3		Y					

Attachment C										
KERB & GUTTER REPLACEMENT PROGRAM										
LOCATION	Town	LENGTH	Priority	Pvmt Work	Tree Work	TYPE	LAYBACK	WIDTH INC PAV	ADDITIONAL WORK	Comment
Turtle St	Denman	5	3							
Victoria St	Muswellbrook	10	3		Y					Holding water
Virginia St	Denman	10	3							
Virginia St	Denman	10	3							
Virginia St	Denman	5	3		Y					
Virginia St	Denman	5	3				Y			
Virginia St	Denman	10	3							
Virginia St	Denman	10	3					2		
Virginia St	Denman	20	3					2		
Virginia St	Denman	10	3		Y					
Virginia St	Denman	1	3						chip	
Virginia St	Denman	5	3							
Virginia St	Denman	10	3							
Virginia St	Denman	5	3							
Virginia St	Denman	20	3							
Wilder St	Muswellbrook	5	3							
Wollombi Road	Muswellbrook	10	3							
Wollombi Road	Muswellbrook	5	3		Y					
Wandoo Cl	Muswellbrook	10	3							
Woollybutt Way	Muswellbrook	10	3		Y					
Woollybutt Way	Muswellbrook	5	3		Y					
Kokoda St	Muswellbrook	5	3/4							
Acacia Dr	Muswellbrook	1.5	4							Cracked and broken
Bligh St	Muswellbrook	5	4							
Boronia Dr	Muswellbrook	5	4							
Brecht St	Muswellbrook	20	4				Y			
Brentwood St	Muswellbrook	1	4							Chip
Carl St	Muswellbrook	5	4							
Coral St	Muswellbrook	20	4							Chip
Cunningham St	Muswellbrook	1	4							Chip
Dumaresq St	Muswellbrook	1	4							Chipped
Foley St	Muswellbrook	1	4							Chipped
Hermitage Place	Muswellbrook	1	4							Chip
Isobella St	Muswellbrook	15	4							
Isobella St	Muswellbrook	5	4							
Lexia St	Muswellbrook	1	4							Chip
Lexia St	Muswellbrook	20	4							
Lexia St	Muswellbrook	20	4							
Malbec St	Muswellbrook	5	4							
Newman St	Muswellbrook	1	4							Gap
Paxton St	Denman	10	4					2	Monitor	
Queen St	Muswellbrook	5	4							Cracked and broken
Queen St	Muswellbrook	5	4		Y					
Queen St	Muswellbrook	5	4							Cracked and broken
Queen St	Muswellbrook	5	4							Cracked and broken
Queen St	Muswellbrook	5	4							Cracked and broken
Queen St	Muswellbrook	5	4							
Queen St	Muswellbrook	2	4							Chipped
Queen St	Muswellbrook	2	4							Chipped
Queen St	Muswellbrook	2	4							Chipped
Richmond St	Muswellbrook	1	4							Chipped
Riesling St	Muswellbrook	1	4							Chipped
Roger St	Muswellbrook	1	4							Chip
Rutherford Rd	Muswellbrook	1	4							Chip
Scott St	Muswellbrook	5	4							
Semilon St	Muswellbrook	2	4							2 x 1m chip
Shaw Cr	Muswellbrook	5	4					2		
Shaw Cr	Muswellbrook	5	4		Y					
Shiraz St	Muswellbrook	2	4							Chip
Shiraz St	Muswellbrook	1	4							chip
Shiraz St	Muswellbrook	2	4							chip
Shiraz St	Muswellbrook	1	4							chip
Shiraz St	Muswellbrook	2	4							chip
Shiraz St	Muswellbrook	1	4						Chip	
Shiraz St	Muswellbrook	2	4						Chip	
Sowerby St	Muswellbrook	5	4							
St Andrews Pl	Muswellbrook	2	4							Chip



Attachment C										
KERB & GUTTER REPLACEMNT PROGRAM										
LOCATION	Town	LENGTH	Priority	Pvmt Work	Tree Work	TYPE	LAYBACK	WIDTH INC PAV	ADDITIONAL WORK	Comment
Sydney St	Muswellbrook	20	4							
Thompson St	Muswellbrook	10	4							
Thompson St	Muswellbrook	2	4						Chip	
Tobruk Ave	Muswellbrook	5	4							
Tobruk Ave	Muswellbrook	5	4		Y					
Tobruk Ave	Muswellbrook	10	4							
Virginia St	Denman	10	4							
Weemala Pl	Muswellbrook	1	4							small chunk missing from layback
Palace St	Denman	130	1	Y	Y	SA Kerb	Y	4	Pavement work also needed	With pavement rehab
Anzac Pde	Muswellbrook	260	1	Y	Y		Y	10		Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	260	1	Y	Y		Y			Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	10	1							
Anzac Pde	Muswellbrook	10	1							
Anzac Pde	Muswellbrook	10	1							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	5	1							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	5	1							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	5	1							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	10	3							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	4	3							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	10	4							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	5	1							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	5	2/3							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	5	2/3							Anzac Parade is on the Rehab program
Anzac Pde	Muswellbrook	10	2/3							Anzac Parade is on the Rehab program

## Attachment D

Segment No	Street	From	To	Treatment Type	STR_LEN GTH	STR_WIDTH	CL_NAAS RA	Priority	Urban Class
<b>Rural - Reseal</b>									
RLMANGR130	MANGOOLA RD	12.00 KM	12.41 KM	Rehab	406	5.8	8	Priority 1	Rural
RLMANGR135	MANGOOLA RD	12.43 KM	13.00 KM	Rehab	568	5.8	8	Priority 1	Rural
RLWIDVR120	WIDDEN VALLEY RD	11.00 KM	12.00 KM	Reseal	1000	5.8	9	Priority 1	Rural
RLWIDVR130	WIDDEN VALLEY RD	12.00 KM	13.00 KM	Reseal	1000	5.8	9	Priority 1	Rural
RLWIDVR140	WIDDEN VALLEY RD	13.00 KM	13.58 KM	Reseal	582	5.8	9	Priority 1	Rural
RLROSER010	ROSEMOUNT RD	0.00 KM	1.30 KM	Reseal	1000	8	8	Priority 1	Rural
RLROSER040	ROSEMOUNT RD	3.00 KM	4.00 KM	Reseal	1000	9.2	8	Priority 1	Rural
RLROSER050	ROSEMOUNT RD	4.00 KM	4.95 KM	Reseal	1000	8.2	8	Priority 1	Rural
RLGIACR130	GIANTS CREEK RD	11.40 KM	12.60 KM	Reseal	1200	5.6	9	Priority 1	Rural
RLHORSR030	HORSESHOE RD	2.00 KM	3.20 KM	Reseal	1200	6	9	Priority 1	Rural
RLMTDAR050/060	MOUNT DANGAR RD	4.40 KM	5.635KM	Reseal	1235	5.4	9	Priority 1	Rural
RLWYBOR220	WYBONG RD	19.60 KM	20.60 KM	Reseal	920	6.4	7	Priority 1	Rural
RLWYBOR310/320	WYBONG RD	31.00 KM	32.30 KM	Reseal	1300	7	7	Priority 2	Rural
RLBYLOW170	BYLONG VALLEY WY	17.82 KM	18.49 KM	Reseal	470	3.8	9	Priority 2	Rural
<b>Urban - Reseal</b>									
RMROGES010	ROGER ST	DUMARESQ ST	KOOMBAHLA ST	Reseal	285	11.5	7	Priority 1	Urban
RMROGES020	ROGER ST	KOOMBAHLA ST	COOK ST	Reseal	62	11.5	7	Priority 1	Urban
RMCARLS010	CARL ST	COOK ST	ROAD WIDENING	Reseal	24	11	7	Priority 1	Urban
RMCARLS020	CARL ST	ROAD WIDENING	BLIGH LN	Reseal	76	18.2	7	Priority 1	Urban
RMCARLS030	CARL ST	BLIGH LN	HILL ST	Reseal	107	18.2	7	Priority 1	Urban
RMCARLS040	CARL ST	HILL ST	HILLVIEW AVE	Reseal	88	15.8	7	Priority 1	Urban
RMCOALR010	COAL RD	VICTORIA ST	Reservoir entrance	Reseal	1000	7.7	8	Priority 1	Urban
RMCOOKS050	COOK ST	GEORGE ST	ROGER ST	Reseal	210	6	6	Priority 1	Urban
RMGEORS030	GEORGE ST	HILL ST	CARL LN	Reseal	106	7.2	9	Priority 1	Urban

RMGEORS040	GEORGE ST	CARL LN	BROOK ST	Reseal	111	7.1	9	Priority 1	Urban
RMQUEES070	QUEEN ST	WILSON ST	BRENTWOOD ST	Reseal	110	7	8	Priority 1	Urban
RMQUEES080	QUEEN ST	BRENTWOOD ST	KING ST	Reseal	293	7	8	Priority 1	Urban
RMHENRD010	HENRY DANGAR DR	BIMBADEEN DR	DAY ST	Reseal	48	10	7	Priority 1	Urban
RMHENRD100	HENRY DANGAR DR	WRIGHT CCT	GORE ST	Reseal	364	11	9	Priority 1	Urban
RSGOULD030	GOULBURN DR	420m S	RAILWAY LINE W	Reseal	257	5.4	9	Priority 1	Urban
RSGOULD040	GOULBURN DR	RAILWAY LINE W	RAILWAY LINE E	Reseal	859	3.6	9	Priority 1	Urban
RMJORDS010	JORDAN ST	SYDNEY ST	LORNE ST	Reseal	212	6.2	9	Priority 1	Urban
RMCOMMR020	COMMON RD	ROAD WIDENING	INDUSTRIAL CL	Reseal	262	11	7	Priority 1	Urban
RMCOMMR030	COMMON RD	INDUSTRIAL CL	QUEEN ST	Reseal	100	12.8	7	Priority 1	Urban
RDMACAS010	MACAULEY ST	VIRGINIA ST	HUNTER ST	Reseal	156	7.3	9	Priority 1	Urban
RDMACAS020	MACAULEY ST	HUNTER ST	HYDE ST	Reseal	160	7	9	Priority 1	Urban
RDMACAS030	MACAULEY ST	HYDE ST	PALACE ST	Reseal	113	7	9	Priority 1	Urban
RMCOOKS040	COOK ST	33m west of RBT - Brech St	GEORGE ST	Reseal	418	12.8	6	Priority 2	Urban
RMRUTHR030	RUTHERFORD RD	WOOLLYBUTT WY	RBT	Reseal	147	7.8	6	Priority 2	Urban
RMRUTHR031	RUTHERFORD RD	RBT	WOOLLYBUTT WY	Reseal	147	7.8	6	Priority 2	Urban
RMBLIGL010	BLIGH LN	ROAD NARROWING	CARL ST	Reseal	108	5.8	9	Priority 2	Urban
RMBLIGL020	BLIGH LN	GEORGE ST	ROAD NARROWING	Reseal	106	8	9	Priority 2	Urban
RMDOYLL010	DOYLE LN	DOYLE ST	ARMITAGE AVE	Reseal	127	4	9	Priority 2	Urban
RMDOYLL020	DOYLE LN	ARMITAGE AVE	GEORGE ST	Reseal	93	4	9	Priority 2	Urban
RMKINGS010	KING ST	QUEEN ST	MIDANGA AVE	Reseal	199	18.4	9	Priority 2	Urban
RMKINGS020	KING ST	MIDANGA ST	BRECHT ST	Reseal	60	18.4	9	Priority 2	Urban
RMKINGS030	KING ST	BRECHT ST	CLENDINNING ST	Reseal	220	19.2	7	Priority 2	Urban
RMQUEES030	QUEEN ST	LEXIA ST	HOLDSWORTH CR	Reseal	295	13	8	Priority 2	Urban
RMSANRC010	SANRAY CR	RUTH WHITE AVE	CULDESAC	Reseal	29	9	9	Priority 2	Urban
RMSHAWC010	SHAW CR	THOMPSON ST S	THOMPSON ST	Reseal	305	9	9	Priority 2	Urban
RMTHOMS030	THOMPSON ST	DONALDSON ST	RUTH WHITE AVE	Reseal	115	9	8	Priority 2	Urban
RMSTOCR012	SKELLATAR STOCK ROUTE	ST JAMES SCHOOL	OSBORN AVE	Reseal	250	6.5	6	Priority 2	Urban
RMSTOCR020	SKELLATAR STOCK ROUTE	OSBORN AVE	150m towards Adam st	Reseal	150	8.2	6	Priority 2	Urban
RDVIRGS030	VIRGINIA ST	MERTON ST	CRINOLINE ST	Reseal	331	12	8	Priority 3	Urban

Attachment E - NEW PROPOSED FOOTPATH LIST

ROUTE	ROUTE NAME	ROUTE SECTION		APPROX	WIDTH (m)	AREA (m)	FACILITY	PRIORITY	RESPONSIBILITY
	Rutherford Rd (Northern side)	Acacia Dr	Cassidy Ave	210	2.5	525.0	NP	High	MSC
M29	Dolahenty St	Victoria St	King St	375	2.5	937.5	NP	High	MSC
M27-C	Carl St	Market Lane	Victoria St	80	2.5	200.0	NP	Medium	MSC
M58-A	New England Hwy / Muswellbrook Golf Course	Thompson St	Bell St	280	2.0	560.0	NP	High	MSC
D1-B	Turtle Street	Creek Crossing	Paxton Street	112	1.5	168.0	NP	High	MSC
M16-A	Thompson St	Kamilaroi St	New England Highway	290	2.0	580.0	NP	High	MSC
M4-D	Ironbark Rd	Adams St	Rutherford Rd	335	2.5	837.5	NP	Medium	MSC
M5-C	Ironbark Rd	Calgaroo Ave	Plashett	172	2.5	430.0	NP	Medium	MSC
M15	Kamilaroi St	Cassidy Ave	Thompson St	130	2.0	260.0	NP	High	MSC
M9-A	Bloodwood Rd	Acacia Dr	9 Bloodwood	190	1.5	285.0	NP	Medium	MSC
	Brook St-Doyle St- Brentwood St	George St	Brentwood St	160	2.0	320.0	NP		MSC
	Karoola Park (FMKAROP913P-02)*	Shiraz St (int)	Newman St (flats)	395	1.5	592.5	NP		MSC
M16-B	Thompson St	Kamilaroi St	Adams St	370	1.5	555.0	NP	Medium	MSC
M33	Wilkinson Ave	Haydon St	Olympic Park	380	2.0	760.0	NP	Medium	MSC
D9-A	Virginia Street	Isobel Street	Merton Street	330	1.5	495.0	NP	High	MSC
M18	Nowland St	Tindale St	Skellatar St	160	1.5	240.0	NP	Medium	MSC
D12-B	Palace Street	St Josephs School	Merton Street	195	2.0	390.0	NP	High	MSC
	Karoola Park (FMKAROP913P-01)*	Shiraz St	Newman St (school)	250	1.5	375.0	NP		MSC
M16-C	Thompson St	Adams St	Fitzgerald Ave	240	1.5	360.0	NP	Medium	MSC
M8	Beech St	Calgaroo Ave	Acacia Dr	320	1.5	480.0	NP	Medium	MSC
M7-A	Calgaroo Ave	Ironbark Rd	Beech St	495	1.5	742.5	NP	Medium	MSC
M42	Semillon St	Shiraz St	Cook St	180	1.5	270.0	NP	Medium	MSC
	Karoola Park (FMKAROP913P-03)*	Newman Street	Semilon Street	390	1.5	585.0	NP		MSC
	Karoola Park (FMKAROP913P-00)*	Humphries St steps	Shiraz St (Int)	250	1.5	375.0	NP		MSC
M6-B	Rutherford Rd	Cassidy Ave	Ironbark Rd	238	2.0	476.0	NP	Medium	MSC
M10-B	Acacia Dr	Rutherford Rd	Bloodwood Rd	780	2.0	1,560.0	NP	Low Medium Timed with future residential development	MSC MSC Developer
D7	Arbor Park	57 Virginia Street	Olgivie Street	420	1.5	630.0	NP	Medium	MSC
M53	Bowman St	Victoria Park	Queen St	585	1.5	877.5	NP	Low	MSC
	Bridge St	Wilkins St	Aberdeen St	600	1.5	900.0	NP		MSC
M21-A	Ruth White Ave	Osborn Ave	Thompson St	590	1.5	885.0	NP	Medium	MSC
M23-B	Mitchell St	Property 48	Francis St	80	1.5	120.0	NP	Medium	MSC
M51	George St	Karoola Park	Cook St	155	1.5	232.5	NP	Low	MSC
M24	Francis St	Mitchell St	Maitland St	200	1.5	300.0	NP	Medium	MSC
M20 - A	Skellatar St	Sydney St	Moosh	235	1.5	352.5	NP	Low	MSC
M47-B	Roger St	Muswellbrook Primary School	Dumaresq St	180	2.0	360.0	NP	High	MSC
M12-A	Woollybutt Way	Kurrajong Reserve	Casuarina Cl	155	2.0	310.0	NP	Low / High	MSC / Centre Management
M44-A2	Cook St	Sowerby St	Carl St	198	2.0	396.0	NP	Medium	MSC
M44-A1	Cook St	Queen St	George St	722	2.0	1,444.0	NP	Medium	MSC
M22	Adams St	Skellatar Stock Route	Ruth White Ave	740	1.5	1,110.0	NP	Low	MSC
D16	Almond Street	16 Almond St	Bray Street	140	1.5	210.0	NP	Timed with future residential development	MSC
M12-B	Woollybutt Way	Casuarina Cl	Calgaroo Ave	120	1.5	180.0	NP	High	MSC
D8-A	Turner Street	Ogilvie Street/Rail Crossing	Bell St	350	1.5	525.0	NP	Timed with future residential development	MSC
M27-A	Carl St	Cook St	Brook St	360	1.5	540.0	NP	Medium	MSC
M46-A	Sowerby St	Hastings St (Karoola Park access)	Hill St	700	1.5	1,050.0	NP	Very Low	MSC
M26	Osborn Ave	Thompson St	Ruth White Ave	190	2.0	380.0	NP	High	MSC
D3-C	Macauley Street	Hyde Park	Virginia Street	220	1.5	330.0	NP	Low	MSC
M23-A	Mitchell St	Francis St	Sydney St	370	1.5	555.0	NP	Low	MSC
M54-A	Brecht St	Cook St	King St	670	1.5	1,005.0	NP	Medium	MSC
M43	Lexia St	Queen St	Verdelho Reserve	715	1.5	1,072.5	NP	Low	MSC
M19	Tindale St	Fitzgerald Ave	Nowland St	295	1.5	442.5	NP	Medium	MSC / School
D13	Merton Street	Virginia Street	Palace Street	395	1.5	592.5	NP	Low	MSC
M48	Dumaresq St	Sowerby St	Roger St	215	1.5	322.5	NP	High	MSC
M30-A	King St	Clendinning St	Queen St	390	1.5	585.0	NP	Low	MSC
M35	The Common (Queen St)	King St	Verdelho Reserve	1690	1.5	2,535.0	UP	Medium	MSC
M3	Wollombi Rd	Sydney St	Patterson Pl (easement path)	1210	1.5	1,815.0	NP	Medium	MSC
	Ironbark Rd#	Plasheet Cl	Ironbark Ridge Estate	25	1.5	37.5	NP	Medium	
M17	Fitzgerald Ave	St James Primary School	Tindale St	135	1.5	202.5	NP	Medium	MSC
D18	Turner Street	Bray Street	Kenilworth Street	120	2.0	240.0	NP	Timed with future residential development	MSC

ROUTE	ROUTE NAME	ROUTE SECTION		APPROX	WIDTH (m)	AREA (m)	FACILITY	PRIORITY	RESPONSIBILITY
M13	Casuarina Cl	Woollybutt Way	Easement	90	1.5	135.0	NP	Very Low Timed with future development	MSC/Developer
M40-B	Hunter St	Sowerby Ave	View Pl	90	1.5	135.0	NP	Low	MSC
D5	Virginia Street	Babbington Street	Bright Street	205	1.5	307.5	NP	Very Low	MSC
M25	Anzac Pde	Sydney St	Easement path (Nowland St)	305	1.5	457.5	NP	Low	MSC
M21-B	Ruth White Ave	Easement	-	40	1.5	60.0	UP	Low	MSC
D17	Bray Street	Almond Street	Turner Street	130	1.5	195.0	NP	Timed with future residential development	MSC/Developer
D9-B	Virginia Street	Merton Street	Kenilworth Street	285	1.5	427.5	NP	Low	MSC
D3-A	Hyde Park	Macauley Street	Babbington Street	260	2.0	520.0	NP	Medium	MSC
D4	Babbington Street	Virginia Street	Hyde Street	290	1.5	435.0	NP	Very Low	MSC
M10-A	Acacia Dr (easement)	Acacia Dr	Goruk Cl	175	1.5	262.5	NP	Very Low	MSC
M36-A	Queen St	Cook St	Verdelho Reserve	550	1.5	825.0	SZNP	Very Low Low Timed with future residnetial development	MSC / Developer
M1-D	Sydney Street	Tarakan Ave	Wollombi Road	290	1.5	435.0	NP	High	MSC/RTA
M4-A	Skellatar Stock Rt	Sydney St	Wollombi Park	1030	2.0	2,060.0	NP	Medium	MSC
D15-B	Bell Street	Turner Street	Almond Street	135	1.5	202.5	NP	Timed with future residential development	MSC/Developer
M40-A	Hunter St	Karoola Park (west)	Sowerby Ave	235	1.5	352.5	NP	Medium	MSC
M14	Cassidy Ave	Rutherford Rd	Kamilaroi St	215	1.5	322.5	T	High	MSC
10	Sydney Street	Brook Park	Skellatar Stock Rt	25	1.5	37.5	NP	High	MSC/RTA
D3-B	Palace Street	Hyde Park		105	1.5	157.5	NP	Low	MSC
D6	Rosemount Road	Virginia Street	Surveyor Generals Drive	295	1.5	442.5	NP	Timed with future residential development	MSC/Developer
M36-B	Queen St	Verdelho Reserve	Property 121	550	1.5	825.0	NP	Very Low Low Timed with future residnetial development	
M36-C	Queen St	Property 145	Property 147	117	1.5	175.5	NP	Very Low Low Timed with future residnetial development	
D10	Kenilworth Street	Virginia Street	Palace Street	400	1.5	600.0	NP	Very Low	MSC
NUMBER	From To		LENGTH (m)				TYPE		

1. Facility access assessment
- 10 Access to only one facility

20 2 facilities

Within access to Mbk south school and Muswellbrook High school (refer to Walk Cycle Plan 2009 survey).

30 3 facilities

40 >3 facilities

50 with in 0.5km radius of CBD or south Shopping centre
- \* Suggested by Resident Leslie Cutriss 20/2/17

# Suggested by Resident Brett Miller 23/3/19 - to connect Ironbark Ridge Estate to future (M4-D & M5-C) and existing footpath



## Attachment F

Segment No	Street	From	To	Treatment Type	STR_LENGTH	STR_WIDTH	CL_NAASRA	Priority	Urban Class
<b>Urban - Rehab</b>									
RDOGILS020	OGILVIE ST	VIRGINIA ST	PAXTON ST	Rehab	221	7.5	7	In progress	Urban
RMBROOS061	BROOK ST	RBT	CARL ST	Rehab	256	6.9	6	Priority 1	Urban
RMANZAP010	ANZAC PDE	SYDNEY ST	TARAKAN AVE	Rehab	111	10.3	8	Priority 1	Urban
RMANZAP020	ANZAC PDE	TARAKAN AVE	TOBRUK AVE	Rehab	91	7.1	8	Priority 1	Urban
RMANZAP030	ANZAC PDE	TOBRUK AVE	END OF ROAD	Reseal	364	7.1	9	Priority 2	Urban
RMBROOS030	BROOK ST	BRIDGE ST	RAILWAY LINE	Rehab	102	19.3	6	Priority 1	Urban
RMFLEMS010	FLEMING ST	SYDNEY ST	LORNE ST	Rehab	212	11	9	Priority 1	Urban
RMHILLS100	HILL ST	BRIDGE ST	Railway Fence	Rehab	96	10	7	Priority 1	Urban
RMLORNS010	LORNE ST	MAITLAND ST	JORDAN ST	Rehab	136	12.6	7	Priority 1	Urban
RMLORNS020	LORNE ST	JORDAN ST	MITCHELL ST	Rehab	91	12.6	7	Priority 1	Urban
RMRUTHR010	RUTHERFORD RD	NEW ENGLAND HWY	ACACIA DR	Rehab	90	7.8	6	Priority 1	Urban
RMRUTHR011	RUTHERFORD RD	ACACIA DR	NEW ENGLAND HWY	Rehab	90	7.8	6	Priority 1	Urban
RDOGILS010	OGILVIE ST	TURNER ST	VIRGINIA ST	Reseal/Rehab	75	7	8	Priority 1	Urban
RDMERTS020	MERTON ST	PAXTON ST	PALACE ST	Reseal/rehab	212	13.7	8	Priority 1	Urban
RMWILKS020	WILKINS ST	FORD ST	ABERDEEN ST	Reseal	159	11.5	7	Priority 1	Urban
RMHILLS041	HILL ST	RAILWAY	RMHILLS030	Rehab	35	3	9	Priority 2	Urban
RMHILLS080	HILL ST	BLIGH ST	SOWERBY ST	Rehab	114	10	7	Priority 2	Urban
RMROGES020	ROGER ST	KOOMBAHLA ST	COOK ST	Rehab	62	11.5	7	Priority 2	Urban
RMTHOMS010	THOMPSON ST	DRAINAGE RESERVE	ADAMS ST	Rehab	34	9	8	Priority 2	Urban
RMHUNT008	HUNTER TCE	HILL ST	ROAD WIDENING	Rehab	38	7.7	6	Priority 2	Urban
RMRUTHR036	RUTHERFORD RD	RBT	CASSIDY AVE	Rehab	42	10.8	6	Priority 3	Urban
RMRUTHR037	RUTHERFORD RD	CASSIDY AVE	RBT	Rehab	42	7.6	6	Priority 3	Urban
RMCAEMPL010	CAMPION LN	MARKET LN	MARKET ST	Rehab	88	6	9	Priority 3	Urban
RDPALAS040	PALACE ST	MERINO ST	MACAULEY ST	Rehab	212	11.1	7	Priority 1	Urban
RMTARAA010	TARAKAN AVE	ANZAC PDE	CRETE ST	Reseal	92	7.8	9	Priority 2	Urban



## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
12/12/2019	Market St	Muswellbrook	Urban		Railway Hotel	Failed pavement	N	5	5	25	1
12/12/2019	Market St	Muswellbrook	Urban		Electrical shop	Failed pavement	N	5	5	25	1
	Hill St	Muswellbrook	Urban		RMS	Failed pavement	N	5	5	25	1
12/02/2018	Scott St	Muswellbrook	Urban		12	Cut trench	N	3	5	15	1
2/02/2017	Bylong Valley Way		Rural	LHS	8560	Pavement Failure	N	2	10	20	1
6/06/2019	King Street	Muswellbrook	Urban		Brook St Roundabout	Pavement Failure	N	2	15	30	1
9/11/2016	Dalswinton Road		Rural		3905	Pavement Failure	N	2	15	30	1
9/11/2016	Dalswinton Road		Rural	F/W	2720	Pavement Failure	N	3	15	45	1
5/04/2017	Flanders Avenue	Muswellbrook	Urban		19	Failed patch	N	3	10	30	1
11/04/2017	Nowland St	Muswellbrook	Urban		58 Tindale	Pavement Failure	N	3	3	9	1
20/03/2017	Queen Street	Muswellbrook	Urban		81	Pavement Failure	N	2	5	10	1
23/04/2013	Sandy Creek Road		Rural	RHS	11625	Pavement Failure	N	15	2.5	37.5	1
11/04/2017	Sanray Cr	Muswellbrook	Urban		3	Pavement Failure	N	2	2	4	1
24/06/2015	Scott St	Muswellbrook	Urban		Rutherford Park	Trench	N	2	5	10	1
12/04/2017	Shaw Cr	Muswellbrook	Urban		37 (Int Thompson)	Trench (W&W)	N	5	20	100	1
5/04/2017	Sowerby Ave	Muswellbrook	Urban		11\13	Pavement Failure	Y	2	10	20	1
22/07/2016	Thomas Mitchell Drive		Rural	RHS	50	Pavement Failure	N	3	10	30	1
22/07/2016	Thomas Mitchell Drive		Rural	RHS	425	Pavement Failure	N	4	30	120	1
22/07/2016	Thomas Mitchell Drive		Rural	RHS	850	Pavement Failure	N	3	30	90	1
7/12/2017	Thomas Mitchell Drive		Rural	F/W	1515	Pavement Failure	N	10	20	200	1
16/06/2014	Thompson Street	Muswellbrook	Urban		Int Kamilaroi	Pavement Failure	N	9	30	270	1
5/04/2017	Turanville Ave	Muswellbrook	Urban		55 Sowerby	Pavement failure	N	2	2	4	1
15/03/2017	Turner Street	Denman	Urban		28	Pavement Failure	N	3	10	30	1
23/01/2017	Turnermans Road Nth		Rural		610	Pavement Failure	N	2	30	60	1
7/03/2017	Wilkins Street	Muswellbrook	Urban		7-9	Pavement failure	N	2	5	10	1
8/01/2015	William St	Muswellbrook	Urban		Sowerby to Culdesac	First 20 m		5	20	100	1
21/01/2016	Wybong Road		Rural	RHS	23640	Pavement Failure	N	2	2	4	1
11/01/2017	Wybong Road		Rural	RHS	33160	Pavement Failure	N	2	5	10	1
11/01/2017	Wybong Road		Rural	RHS	18050	Pavement Failure in new pavement	N	2	15	30	1
5/04/2017	Hill St	Muswellbrook	Urban		Bridge to Railway Line	Pavement Failure	Y	10	96	960	1
5/04/2017	Anzac Pde	Muswellbrook	Urban		17	Pavement Failure	N	5	10	50	1
5/04/2017	Anzac Pde	Muswellbrook	Urban		Sydney St to 20	Pavement Failure	Y	10	260	2600	1
20/06/2014	Merton Street	Denman	Urban		12	Trench	N	2	14	28	1
20/06/2014	Merton Street	Denman	Urban		64 Paxton	Trench	N	2	6	12	1
12/01/2015	Merton Street	Denman	Urban		2	Pavement Failure	Y	4	4	16	1
14/03/2017	Merton Street	Denman	Urban		4 to 8	Rut along kerb and gutter	Y	2	50	100	1
14/03/2017	Merton Street	Denman	Urban		10	Trench	N	2	14	28	1
6/12/2017	Heron Lane		Rural	Centre	5150	Pavement failure	N	5	5	25	1
7/12/2017	Thomas Mitchell Drive		Rural	RHS	7280	Pavement failure	N	4	10	40	1
20/12/2017	Mangoola Road		Rural		7250	Pavement failure	N	2	2	4	1
4/01/2018	Castlerock Road		Rural		17580	Pavement failure	N	2	20	40	1
12/02/2018	Hunter Terrace	Muswellbrook	Urban		27 (Centre)	Trench	N	3	10	30	1
19/02/2018	Palace St	Denman	Urban		11\33	Pavment Failure	Y	11	220	2420	1
19/02/2018	Lofberg Ct	Muswellbrook	Urban	F/W	14	Pavement Sink	Y	3	6.5	19.5	1
19/02/2018	Turner Street	Denman	Urban		24	Pavement Failure	N	2	10	20	1
22/03/2018	Baerami Creek Road		Rural	RHS	8700	Pavement Failure	N	2	10	20	1
15/03/2018	Bylong Valley Way		Rural	RHS	9575	Pavement Failure	N	2	3	6	1
27/03/2018	Cousins St	Muswellbrook	Urban		4	Trench across road	N	2	8	16	1
27/03/2018	Humphries St	Muswellbrook	Urban		27	Dip over stormwater drain	N	2	2	4	1
27/03/2018	Malbec St	Muswellbrook	Urban		12	Pavement failure	N	2	2	4	1
19/02/2018	Rosemount Road	Denman	Urban		23	Trench	N	8	2	16	1
5/04/2018	Brecht St	Muswellbrook	Urban		45	Pavement failure	N	4	20	80	1
5/04/2018	Chardonnay St	Muswellbrook	Urban		29	Pavement failure	N	2	2	4	1
5/04/2018	Chardonnay St	Muswellbrook	Urban		25	Pavement failure	N	2	5	10	1
7/04/2017	Foley Street	Muswellbrook	Urban		20	Pavement Failure	N	4	4	16	1

## Attachment G

HEAVY PATCHING PROGRAM										
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area Priority
17/04/2018	Turanville Ave	Muswellbrook	Urban		11	Pavement Failure	N	3	3	9 1
18/04/2018	Clifford Street	Muswellbrook	Urban		7	Trench across the road is failing		2	8	16 1
18/04/2018	Bligh Street	Muswellbrook	Urban		17	Pavement Failure	N	4	4	16 1
18/04/2018	Birrallee Street	Muswellbrook	Urban		Int Brentwood Street	Pavement Failure	N	3	10	30 1
18/04/2018	Brecht St	Muswellbrook	Urban		From Birrallee to Brentwood	5 x trenches	N			1
18/04/2018	Bligh Lane	Muswellbrook	Urban		6 to Carl St	Rough surface	N	3	85	255 1
18/04/2018	Lower Hill St	Muswellbrook	Urban		64	Trench	N	3	15	45 1
18/04/2018	Brentwood Street	Muswellbrook	Urban		Length of road	13 x trenches across road	N	10	26	260 1
18/04/2018	Brook Street	Muswellbrook	Urban		105-Carl St		N	3	78	234 1
18/04/2018	Campion Ln	Muswellbrook	Urban		Cycle shop	Pavement failure	N	4	10	40 1
18/04/2018	Glenbawn Place	Muswellbrook	Urban		Int Wollombi Road	Pavement Failure (Full width)	N	3	12	36 1
18/04/2018	Wilkins Street	Muswellbrook	Urban		113	Pavement Failure	N	2	40	80 1
12/12/2019	Roger St	Muswellbrook	Urban		26	Pavement Failure	N	5	10	50 1
9/04/2018	Brook St	Muswellbrook	Urban		St Albans	Pavement Failure	N	3	3	9 1
9/04/2018	Market St	Muswellbrook	Urban		Interchange	Cracking	N	2	10	20 1
9/04/2018	Market St	Muswellbrook	Urban		Interchange	Failed Patch/cracking pavement	N	5	10	50 1
12/12/2019	Market St	Muswellbrook	Urban		Interchange	Cracking	N	2	3	6 1
16/04/2018	Carramere Road	Muswellbrook	Urban		Hanson	Pavement Failure	N	2	2	4 1
6/06/2019	Rutherford Road	Muswellbrook	Urban		Toyota	Cracking	N	2	85	170 1
6/06/2019	Rutherford Road	Muswellbrook	Urban		Opp Toyota	Cracking	N	3	10	30 1
1/06/2019	St James Cr	Muswellbrook	Urban		Thompson to Kamilaroi	Pavement Failure	N	9	45	405 1
29/10/2019	Burgundy St	Muswellbrook	Urban		8	Pavement failure	N	2	2	4 1
29/10/2019	Cabemet St	Muswellbrook	Urban		11	Trench	N	2	2	9 1
29/10/2019	Queen Street	Muswellbrook	Urban		154	Pavement failure	N	3	10	30 1
29/10/2019	Queen Street	Muswellbrook	Urban		147	Trench	N	2	12	24 1
18/11/2019	Aberdeen Street	Muswellbrook	Urban		43?	Cracking	N	5	5	25 1
18/11/2019	Chardonnay St	Muswellbrook	Urban		11	Pavement Failure	N	3	3	9 1
18/11/2019	Flanders Avenue	Muswellbrook	Urban		1a-3	Pavement Failure	N	2	30	60 1
19/11/2019	Lower William St	Muswellbrook	Urban		6	Pavement Failure	N	2	10	20 1
19/11/2019	Lower William St	Muswellbrook	Urban		8	Rut at gutter	Y	2	15	30 1
19/11/2019	Lower William St	Muswellbrook	Urban		6	Rut and shove at gutter/driveway	Y	2	10	20 1
19/11/2019	Lower William St	Muswellbrook	Urban		4	Shove at driveway		2	4	8 1
12/12/2019	Bell St	Muswellbrook	Urban		Opposite Golf Shop	Pavement failure	N	3	3	9 1
4/11/2019	Carl St	Muswellbrook	Urban		Lot 6	Pavement failure	N	4	50	200 1
12/12/2019	Carl St	Muswellbrook	Urban		Cnr Bligh Ln	Pavement failure	N	2	2	4 1
12/12/2019	Dumaresq St	Muswellbrook	Urban		416	Pavement Failure	Y	2	40	80 1
12/12/2019	Hill St	Muswellbrook	Urban		113	Pavement Failure	N	3	3	9 1
7/04/2017	Market Lane	Muswellbrook	Urban		Int Campton Ln	Pavement failure	N	3	10	30 1
12/12/2019	Market Lane	Muswellbrook	Urban		Senior Citizens	Pavement failure	N	3	10	30 1
12/12/2019	Market Lane	Muswellbrook	Urban		Cnr Williams	Pavement failure	N	3	10	30 1
12/12/2019	Newman St	Muswellbrook	Urban		cnr Koombahla	Pavement failure	N	2	2	4 1
12/12/2019	Parkinson Ave	Muswellbrook	Urban		1	Pavement failure	N	2	10	20 1
12/12/2019	Parkinson Ave	Muswellbrook	Urban		19-21	Pavement failure	N	2	5	10 1
12/12/2019	Parkinson Ave	Muswellbrook	Urban		23	Pavement failure	N	2	5	10 1
12/12/2019	Scott St	Muswellbrook	Urban	F/W	45	Trench	N	2	10	20 1
12/12/2019	Sowerby St	Muswellbrook	Urban		69	Pavement failure	N	2	2	4 1
12/12/2019	Sowerby St	Muswellbrook	Urban		66	Pavement Failure		2.5	20	50 1
12/12/2019	Wilkinson Ave	Muswellbrook	Urban		Guides Hut	Pavement failure	N	2	3	6 1
10/01/2020	Martindale Street	Denman	Urban		24	Pavement failure	N	2	5	10 1
16/01/2020	Bloodwood Road	Muswellbrook	Urban		80 Acacia Drive	Pavement failure	N	5	3	15 1
9/01/2020	Industrial Close	Muswellbrook	Urban		SES	Trench across road		2	11	22 1
9/01/2020	Upper William St	Muswellbrook	Urban		Int King St	Pavement failure	N	2	5	10 1
9/01/2020	Upper William St	Muswellbrook	Urban		93	Pavement failure	N	3	2	6 1
29/01/2020	Brook St	Muswellbrook	Urban		121	Pavement failure	N	2	5	10 1
21/01/2020	Gyarran Street	Muswellbrook	Urban		4	Pavement Failure	N	3	5	15 1

## Attachment G

HEAVY PATCHING PROGRAM										
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area Priority
20/01/2020	Gyarran Street	Muswellbrook	Urban		Int Maitland Street	Trench 1/2 way across	N	4	2	8 1
20/01/2020	Gyarran Street	Muswellbrook	Urban		Int Maitland Street	Pavement Failure	N	3	5	15 1
28/01/2020	Cassidy Ave	Muswellbrook	Urban		36	Trench	N	2	8	16 1
11/02/2020	Wollombi Road	Muswellbrook	Urban		25	Pavement failure	N	3	3	9 1
11/02/2020	Fleming St	Muswellbrook	Urban		3\5	Pavement Failure	Y	3	20	60 1
24/02/2016	Bylong Valley Way		Rural	RHS	32800	Pavement Failure	N	2	5	10 1\2
2/02/2017	Bylong Valley Way		Rural	RHS	8180	Pavement Failure	N	2	100	200 1\2
9/11/2016	Edderton Road		Rural	RHS	2650	Pavement Failure	N	2	60	120 1\2
9/11/2016	Edderton Road		Rural	RHS	2850	Pavement Failure	N	2	30	60 1\2
9/11/2016	Edderton Road		Rural	F/W	4560	Dip over culvert	N	6	3	18 1\2
16/10/2014	Martindale Road		Rural	RHS	8060	Pavement Failure	N	2.5	30	75 1\2
23/01/2017	Martindale Road		Rural		23265	Pavement Failure	N	3	4	12 1\2
23/01/2017	Martindale Road		Rural		23276	Pavement Failure	N	3	4	12 1\2
10/01/2020	Palace St	Denman	Urban		Macaulay to Merino	Trench	N	2	220	440 1\2
21/03/2017	Queen Street	Muswellbrook	Urban		144-156	Pavement along k&g line damaged	Y	2	140	280 1\2
13/01/2017	Ridgeland's Road		Rural	RHS	10978	Pavement Failure	N	2.5	10	25 1\2
22/07/2016	Thomas Mitchell Drive		Rural	RHS	1530	Pavement Failure	N	3	20	60 1\2
22/07/2016	Thomas Mitchell Drive		Rural	RHS	1750	Pavement Failure	N	4	100	400 1\2
7/03/2017	Aberdeen Street	Muswellbrook	Urban		26-28	Cracking	N	2	30	60 2
7/03/2017	Aberdeen Street	Muswellbrook	Urban		45	Pavement Failure	N	3	10	30 2
18/11/209	Aberdeen Street	Muswellbrook	Urban		21	Pavement Failure	N	3	2	6 2
5/04/2017	Acacia Drive	Muswellbrook	Urban		60	Pavement Failure	Y	2	5	10 2
5/04/2017	Anzac Pde	Muswellbrook	Urban		27\29	Cracking	N	2	10	20 2
25/02/2016	Baerami Creek Road		Rural	F/W	10834	Pavement Failure	N	6	5	30 2
19/10/2019	Barrington St	Muswellbrook	Urban		11	Trench	N	2	9	18 2
9/04/2018	Birralee Street	Muswellbrook	Urban		3	Pavement Failure	N	2	5	10 2
5/04/2017	Bligh Street	Muswellbrook	Urban		5	Trench	N	2	15	30 2
18/04/2018	Bligh Street	Muswellbrook	Urban		53	Pavement Failure	N	3	3	9 2
12/12/2019	Bowman St	Muswellbrook	Urban		17	Pavement failure	N	2	3	6 2
4/04/2016	Bray St	Denman	Urban		8	Shove	N	3	3	9 2
8/01/2015	Brecht St	Muswellbrook	Urban		37-39	Failed Patch	N	2	10	20 2
6/04/2017	Brook Street	Muswellbrook	Urban		89a	Pavement Failure	N	2	15	30 2
6/04/2017	Brook Street	Muswellbrook	Urban		121-Carl Street (WB lane)	Rough Surface	N	7	256	1792 2
6/04/2017	Brook Street	Muswellbrook	Urban		Carl-Sowerby	Delamination	N	6	215	1290 2
16/10/2014	Bureen Road		Rural	RHS	5950	Pavement Failure	N	2.5	6	15 2
29/10/2019	Burgundy St	Muswellbrook	Urban		8	Pavement Failure	Y	2	3	6 2
15/03/2018	Bylong Valley Way		Rural	LHS	2670	Patch failure	N	2	20	40 2
15/03/2018	Bylong Valley Way		Rural	RHS	10940	Pavement Failure	N	2	20	40 2
21/11/2013	Bylong Valley Way		Rural	RHS	35890	Pavement Failure	N	5	2	10 2
15/03/2018	Bylong Valley Way		Rural	RHS	14395	Pavement Failure	N	4	2	8 2
15/03/2018	Bylong Valley Way		Rural	RHS	26290	Pavement Failure	N	4	4	16 2
4/01/2018	Castlerock Road		Rural	LHS	4400	Pavement Failure	N	2	5	10 2
4/01/2018	Castlerock Road		Rural	LHS	1410	Pavement Failure	N	2	2	4 2
9/11/2016	Dalswinton Road		Rural	LHS	120	Pavement Failure	N	5	3	15 2
21/10/2013	Dalswinton Road		Rural	LHS	2090	Pavement Failure	N	10	2	20 2
21/10/2013	Dalswinton Road		Rural	RHS	2120	Pavement Failure		2.5	3	7.5 2
21/10/2013	Dalswinton Road		Rural	LHS	2121	Pavement Failure		2.5	3	7.5 2
15/01/2018	Dalswinton Road		Rural	F/W	3690	Pavement Failure	N	4	30	120 2
27/05/2016	Dumaresq St	Muswellbrook	Urban		17	Pavement Failure	N	3	2	6 2
12/12/2019	Dumaresq St	Muswellbrook	Urban		14	Trench x 2	N	4	10	40 2
9/11/2016	Edderton Road		Rural	LHS	480	Pavement Failure	N	2	40	80 2
9/11/2016	Edderton Road		Rural	LHS	2780	Pavement Failure	N	2	10	20 2
9/11/2016	Edderton Road		Rural	RHS	3080	Pavement Failure	N	2	30	60 2
9/11/2016	Edderton Road		Rural	RHS	3785	Pavement Failure	N	2	10	20 2
9/11/2016	Edderton Road		Rural	RHS	3830	Pavement Failure	N	3	3	9 2

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
9/11/2016	Edderton Road		Rural	RHS	13390	Pavement Failure	N	2	15	30	2
9/11/2016	Edderton Road		Rural	RHS	14160	Pavement Failure	N	2	60	120	2
25/01/2016	Femdale Road		Rural	RHS	1915	Pavement Failure	N	2	5	10	2
25/01/2016	Femdale Road		Rural	LHS	1920	Pavement Failure	N	2	10	20	2
14/01/2014	Femdale Road		Rural	RHS	2790	Pavement Failure	N	10	2.5	25	2
17/01/2017	Femdale Road		Rural	RHS	2855	Pavement Failure	N	2	5	10	2
14/01/2014	Femdale Road		Rural	F/W	3170	Pavement Failure	N	4	20	80	2
18/11/2019	Flanders Avenue	Muswellbrook	Urban		16	Pavement Failure	N	2	10	20	2
11/04/2017	Fleming St	Muswellbrook	Urban		4	Pavement Failure	N	5	5	25	2
7/04/2017	Foley Lane	Muswellbrook	Urban		Int Carl St	Pavement Failure	N	4	4	16	2
18/11/2019	Ford St	Muswellbrook	Urban		44-66	Failed Patch	N	2	200	400	2
5/04/2016	Glen Munro Road	Muswellbrook	Urban		Erwin's (number 5)	Cracking	N	2	3	6	2
9/12/2014	Hastings Street	Muswellbrook	Urban		7	Pavement Failure	Y	3	3	9	2
	2012 Humphries St	Muswellbrook	Urban		1	Pavement Failure	Y	2	8	16	2
26/05/2016	Hunter Street	Muswellbrook	Urban		Median near sw drain	Pavement Failure	Y	2	10	20	2
20/01/2015	Ironbark Rd	Muswellbrook	Urban		End road	Pavement Failure	N	3	10	30	2
8/10/2015	Jordan St	Muswellbrook	Urban		Int Lorne St	Trench	N	6.5	2	13	2
20/01/2015	Kamilaroi Street	Muswellbrook	Urban		5	Pavement Failure	N	2	3	6	2
12/04/2017	Kamilaroi Street	Muswellbrook	Urban		Cnr Cassidy	Trench along gutter	Y	2	25	50	2
12/11/2016	Kayuga Road		Rural	RHS	4080	Pavement Failure	N	3	15	45	2
	2012 Lexia St	Muswellbrook	Urban		48/46	Failure near K&G	Y	40	2	80	2
18/01/2016	Logues Lane		Rural	LHS	380	Pavement Failure		2	2	4	2
23/12/2014	Lower Hill St	Muswellbrook	Urban		80	Pavement Failure	N	3	15	45	2
21/01/2016	Mangoola Road		Rural	LHS	14470	Pavement Failure	N	4	4	16	2
	2012 Martindale Road		Rural	LHS	8325	Pavement Failure	N	2.5	5	12.5	2
23/01/2017	Martindale Road		Rural		16360	Pavement Failure - at floodway	N	5	5	25	2
7/04/2017	Midanga Ave	Muswellbrook	Urban		Intersection King Street	Trench	N	3	12	36	2
12/01/2015	Mill Street	Muswellbrook	Urban		Cnr Sydney St	Pavement Failure	N	2	13	26	2
4/02/2020	Mitchell Street	Muswellbrook	Urban		50	Pavement failure	N	2	5	10	2
7/04/2017	Mitchell Street	Muswellbrook	Urban		Int Francis St	Pavement Failure	N	3	5	15	2
23/08/2016	Muscle Creek Road		Rural	LHS	6610	Pavement Failure	N	3	10	30	2
7/12/2017	Muscle Creek Road		Rural	LHS	3510	Pavement Failure	N	2	20	40	2
17/04/2018	Parkinson Ave	Muswellbrook	Urban		Cnr Cook St	Trench	N	2	8	16	2
17/04/2018	Paterson Pl	Muswellbrook	Urban		4	Pavement Failure	N	2	2	4	2
30/03/2016	Queen Street	Muswellbrook	Urban		48	Pavement Failure	Y	3	10	30	2
20/03/2017	Queen Street	Muswellbrook	Urban		16\18	Pavement Failure	Y	3	20	60	2
17/11/2016	Sandy Creek Road		Rural	RHS	11440	Pavement Failure	N	2	2	4	2
6/02/2019	Sandy Creek Road		Rural	CENTRE	3765	Pavement Failure	N	2	20	40	2
16/10/2013	Scrumlo Road		Rural		2550	Pavement Failure	N	5	2	10	2
27/03/2018	Shiraz St	Muswellbrook	Urban		87	Trench	N	2	10	20	2
7/04/2017	Skellatar St	Muswellbrook	Urban		Blooms	Cracking	N	2	9	18	2
5/04/2016	Skellatar Stock Route	Muswellbrook	Urban		184	Failed Patch	N	2	20	40	2
5/04/2018	Sowerby St	Muswellbrook	Urban		72	Pavement failure	N	3	3	9	2
	2012 St Andrews Place	Muswellbrook	Urban		3	Pavement Failure		5	5	25	2
7/04/2016	St James Cr	Muswellbrook	Urban		11	Trench/Subsidence	N	9	2	18	2
4/02/2020	Sydney Street	Muswellbrook	Urban		Brookside/Garlics	Pavement Failure	N	2	20	40	2
22/07/2016	Thomas Mitchell Drive		Rural	RHS	85	Pavement Failure	N	3	3	9	2
22/07/2016	Thomas Mitchell Drive		Rural	RHS	480	Pavement Failure	N	3	15	45	2
22/07/2016	Thomas Mitchell Drive		Rural	LHS	630	Pavement Failure	N	3	40	120	2
22/07/2016	Thomas Mitchell Drive		Rural	RHS	1910	Pavement Failure	N	3	10	30	2
7/12/2017	Thomas Mitchell Drive		Rural	F/W	2020	Pavement Failure	N	10	5	50	2
7/12/2017	Thomas Mitchell Drive		Rural	LHS	2365	Pavement failure	N	4	10	40	2
15/03/2018	Turanville Ave	Muswellbrook	Urban		12\16	Pavement Failure	Y	4	35	140	2
23/01/2017	Turnermans Road Nth		Rural	RHS	510	Pavement Failure	N	2	2	4	2
23/01/2017	Turnermans Road Nth		Rural	RHS	557	Pavement Failure	N	2	2	4	2

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
23/01/2017	Turnermans Road Nth		Rural		585	Pavement Failure	N	2	20	40	2
23/01/2017	Turnermans Road Nth		Rural	LHS	740	Pavement Failure	N	2	3	6	2
10/01/2020	Virginia St	Denman	Urban		41-43	Pavement failure	N	2	30	60	2
23/12/2014	Wilkins Street	Muswellbrook	Urban		3	Pavement Failure	N	2	4	8	2
25/05/2016	Wilkins Street	Muswellbrook	Urban		At Median	Pavement Failure		5	3	15	2
14/01/2015	Wollombi Road	Muswellbrook	Urban		Int Sydney Street	Shove at intersection	N	2	3	6	2
11/01/2017	Wybong Road		Rural	LHS	Int Kayuga Road	Pavement Failure	N	2	10	20	2
21/01/2016	Wybong Road		Rural	RHS	670	Pavement Failure	N	2	10	20	2
12/09/2014	Wybong Road		Rural		2945	Pavement Failure	N	2	70	140	2
	2012 Wybong Road		Rural	RHS	7080	Pavement Failure	N	2.5	5	12.5	2
8/01/2014	Wybong Road		Rural	LHS	21550	Pavement Failure	N	2	5	10	2
25/01/2016	Yarrawa Road		Rural	CENTRE	6365	Pavement Failure	N	2	2	4	2
19/01/2018	Yarrawa Road		Rural	RHS	9590	Pavement Failure	N	2	10	20	2
25/01/2016	Yarrawa Road		Rural	RHS	9640	Pavement Failure	N	5	2.5	12.5	2
25/01/2016	Yarrawa Road		Rural	LHS	9660	Pavement Failure	N	20	2.5	50	2
19/01/2018	Yarrawa Road		Rural	RHS	9670	Pavement Failure	N	2	20	40	2
16/10/2014	Yarrawa Road		Rural	RHS	9970	Pavement Failure	N	2.5	25	62.5	2
19/01/2018	Yarrawa Road		Rural	LHS	10150	Pavement Failure	N	2	30	60	2
18/11/2019	Aberdeen Street		Urban		24 to 28	Pavement Failure	N	4	100	400	2\3
16/05/2013	Bylong Valley Way		Rural	LHS	24130	Pavement Failure	N	10	2.5	25	2\3
16/05/2013	Bylong Valley Way		Rural	RHS	31750	Pavement Failure	N	25	2.5	62.5	2\3
14/01/2015	Crete St	Muswellbrook	Urban		Cnr Tarakan	Pavement Failure	N	10	10	100	2\3
15/02/2016	Duggans Road		Rural	RHS	120	Pavement Failure	N	2	130	260	2\3
9/11/2016	Edderton Road		Rural	LHS	2260	Pavement Failure	N	2	70	140	2\3
9/11/2016	Edderton Road		Rural	F/W	9260	Pavement Failure	N	6	30	180	2\3
	2012 Femdale Road		Rural	F/W	3170	Pavement Failure	N	5	15	75	2\3
	2012 Hyde St	Denman	Urban		21	Rut along kerb and gutter		2	10	20	2\3
12/11/2016	Kayuga Road		Rural	RHS	4200	Pavement Failure	N	2	30	60	2\3
16/10/2014	Martindale Road		Rural	LHS	1140	Pavement Failure	N	4	10	40	2\3
16/10/2014	Martindale Road		Rural	RHS	1315	Pavement Failure	N	2	3	6	2\3
26/09/2014	Ridgeland Rd		Rural	RHS	9127	Pavement Failure	N	3	3	9	2\3
17/04/2018	Turanville Ave	Muswellbrook	Urban		12	Pavement Failure	N	2	20	40	2\3
20/01/2015	Adams St	Muswellbrook	Urban		Cnr Thompson	Pavement Failure	N	3	3	9	3
19/05/2016	Adams St	Muswellbrook	Urban		Various	15 x trenches	N	30	8	240	3
5/04/2017	Adams St	Muswellbrook	Urban		2 Kamilaroi	Pavement failure	N	5	5	25	3
11/01/2016	Antienne Road		Rural	RHS	380	Pavement Failure	N	2	2	4	3
6/12/2017	Antienne Road		Rural	RHS	290	Pavement Failure	N	2	2	4	3
8/01/2019	Antienne Road		Rural	RHS	200	Pavement Failure	N	2	2	4	3
5/04/2017	Anzac Pde	Muswellbrook	Urban		43	Trench	N	2	7	14	3
12/01/2015	Babbington St	Denman	Urban		Int Hunter St	Trench	N	2	8	16	3
4/04/2016	Babbington St	Denman	Urban		Int Palace St	Trench	N	8	3	24	3
25/02/2016	Baerami Creek Road		Rural	F/W	480	Pavement Failure	N	8	3	24	3
7/02/2017	Baerami Creek Road		Rural	RHS	780	Pavement Failure	N	2	2	4	3
7/02/2017	Baerami Creek Road		Rural	RHS	1160	Pavement Failure	N	2	2	4	3
22/03/2018	Baerami Creek Road		Rural	F/W	1900	Pavement Failure	N	8	30	240	3
7/02/2017	Baerami Creek Road		Rural	RHS	2240	Pavement Failure	N	4	4	16	3
7/02/2017	Baerami Creek Road		Rural	F/W	4095	Pavement Failure	N	7	3	21	3
25/02/2016	Baerami Creek Road		Rural	F/W	4780	Pavement Failure - over culvert	N	8	2	16	3
7/02/2017	Baerami Creek Road		Rural	RHS	7380	Pavement failure	N	2	2	4	3
25/02/2016	Baerami Creek Road		Rural	CENTRE	8266	Pavement Failure	N	2	2	4	3
10/12/2013	Baerami Creek Road		Rural	F/W	8810	Pavement Failure	N	10	6	60	3
7/02/2017	Baerami Creek Road		Rural	LHS	8832	Pavement Failure	N	2	40	80	3
7/02/2017	Baerami Creek Road		Rural	RHS	8920	Pavement Failure	N	2	10	20	3
7/02/2017	Baerami Creek Road		Rural	RHS	8960	Pavement Failure	N	2	10	20	3
7/02/2017	Baerami Creek Road		Rural	RHS	9020	Pavement Failure	N	2	4	8	3

## Attachment G

HEAVY PATCHING PROGRAM										
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area Priority
22/03/2018	Baerami Creek Road		Rural	RHS	9175	Pavement Failure	N	3	3	9 3
10/12/2013	Baerami Creek Road		Rural	RHS	9345	Pavement Failure	N	2	20	40 3
25/02/2016	Baerami Creek Road		Rural	RHS	9370	Pavement Failure	N	3	2	6 3
25/02/2016	Baerami Creek Road		Rural	RHS	9380	Pavement Failure	N	2	5	10 3
22/03/2018	Baerami Creek Road		Rural	LHS	9650	Pavement Failure	N	2	2	4 3
2012	Baerami Creek Road		Rural	LHS	9730	Pavement Failure	N	2.5	2.5	6.25 3
22/03/2018	Baerami Creek Road		Rural	LHS	9785	Pavement Failure	N	2	2	4 3
22/03/2018	Baerami Creek Road		Rural	LHS	9835	Pavement Failure	N	2	2	4 3
22/03/2018	Baerami Creek Road		Rural	RHS	10685	Pavement Failure	N	2	2	4 3
10/12/2013	Baerami Creek Road		Rural	LHS	10792	Pavement Failure	N	2	5	10 3
25/02/2016	Baerami Creek Road		Rural	LHS	10808	Pavement Failure	N	2	3	6 3
10/12/2013	Baerami Creek Road		Rural	LHS	11240	Pavement Failure	N	2	3	6 3
6/11/2014	Baerami Creek Road		Rural	RHS	11360	Pavement Failure	N	2	2	4 3
7/02/2017	Baerami Creek Road		Rural	LHS	11900	Pavement Failure	N	3	3	9 3
25/02/2016	Baerami Creek Road		Rural	F/W	12909	Over culvert	N	3	6	18 3
22/03/2008	Baerami Creek Road		Rural	RHS	12965	Pavement failure	N	2	2	4 3
12/01/2016	Balmoral Road		Rural	LHS	435	Pavement Failure	N	2.5	2	5 3
16/10/2013	Balmoral Road		Rural	F/W	890	Dip near culvert full width	N	6	2.5	15 3
27/05/2016	Barrington St	Muswellbrook	Urban		29	Pavement Failure	N	2	3	6 3
21/03/2017	Barrington St	Muswellbrook	Urban		23	Trench	N	2	9	18 3
5/04/2001	Beech St	Muswellbrook	Urban		Int Acacia	Trench	N	2	8	16 3
25/11/2016	Bengalla Link Road		Rural	LHS	7295	Pavement Failure	N	2	10	20 3
11/01/2018	Bengalla Link Road		Rural	LHS	7515	Wheel Ruts	N	4	315	1260 3
25/11/2016	Bengalla Link Road		Rural	RHS	7625	Pavement Failure	N	2	20	40 3
21/01/2016	Bengalla Link Road		Rural	RHS	7785	Pavement Failure	N	2	10	20 3
11/01/2018	Bengalla Link Road		Rural	RHS	9100	Pavement Failure	N	2	200	400 3
21/01/2016	Bengalla Link Road		Rural	LHS	9195	Pavement Failure	N	2	10	20 3
6/04/2016	Bimbadeen Drive	Muswellbrook	Urban		5	Pavement Failure	N	2	3	6 3
2012	Birralee Street	Muswellbrook	Urban		Various	14 x trenches across road	N	8	28	224 3
9/04/2018	Birralee Street	Muswellbrook	Urban		4	Pavement Failure	N	2	5	10 3
5/04/2017	Bligh Lane	Muswellbrook	Urban		22	Trench	N	2	8	16 3
10/12/2014	Bligh Street	Muswellbrook	Urban		7	Trench	N	4	15	60 3
18/04/2018	Bligh Street	Muswellbrook	Urban		41	Pavement Failure	N	3	10	30 3
26/05/2016	Bligh Street	Muswellbrook	Urban		9	Trench	N	2	14	28 3
5/04/2017	Bligh Street	Muswellbrook	Urban		26 Turanville	Pavement Failure	N	3	3	9 3
5/04/2017	Bligh Street	Muswellbrook	Urban		35	Trench 1/2 way	N	2	9	18 3
5/04/2018	Bligh Street	Muswellbrook	Urban		5	Patch repair	N	4	5	20 3
5/04/2018	Bligh Street	Muswellbrook	Urban		6	Pavement Failure	N	3	20	60 3
5/04/2018	Bligh Street	Muswellbrook	Urban		24	Pavement Failure	N	2	2	4 3
1/03/2017	Bloodwood Road	Muswellbrook	Urban		29	Trench	N	3	9	27 3
28/01/2015	Boronia Dr	Muswellbrook	Urban		10	Trench	N	2	8	16 3
6/01/2015	Bowman St	Muswellbrook	Urban	CENTRE	5\7	Pavement Failure	N	2	25	50 3
12/02/2018	Bowman St	Muswellbrook	Urban		26	Failed patch	N	3	10	30 3
12/01/2015	Bray St	Denman	Urban		Int Tumer St	Pavement Failure	N	4	4	16 3
4/04/2016	Bray St	Denman	Urban		Int Almond St	Pavement Failure	N	3	5	15 3
4/04/2016	Bray St	Denman	Urban		34 Tumer St	Pavement Failure	N	2	3	6 3
18/11/2019	Brecht St	Muswellbrook	Urban		67	Pavement Failure	N	3	3	9 3
6/01/2015	Brentwood Street	Muswellbrook	Urban		57	Pavement Failure	N	3	2	6 3
29/03/2017	Brentwood Street	Muswellbrook	Urban		47	Failed Patch	N	2	2	4 3
12/12/2019	Brentwood Street	Muswellbrook	Urban		49	Pavement Failure	N	3	2	6 3
29/03/2017	Brentwood Street	Muswellbrook	Urban		Int Nardoo	Pavement failure	N	3	3	9 3
29/03/2017	Brentwood Street	Muswellbrook	Urban		Int Queen	Trench	N	2	9	18 3
9/04/2018	Brentwood Street	Muswellbrook	Urban		37	Pavement failure	N	2	3	6 3
9/04/2018	Brentwood Street	Muswellbrook	Urban		33	Pavement failure	N	3	5	15 3



## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
12/01/2015	Bright St	Denman	Urban		1 to 3	Trench along gutter - recently resealed 19/2/18	N	2	70	140	3
19/05/2016	Bronte St	Muswellbrook	Urban		6\8	Pavement Failure	N	4	4	16	3
15/01/2014	Bureen Road		Rural	RHS	6040	Pavement Failure	N	2	5	10	3
15/01/2014	Bureen Road		Rural	LHS	8530	Pavement failure	N	2	3	6	3
22/02/2016	Bureen Road		Rural	F/W	2420	Pavement Failure	N	8	5	40	3
23/01/2017	Bureen Road		Rural	LHS	3050	Pavement failure	N	2	3	6	3
22/01/2018	Bureen Road		Rural	LHS	6680	Pavement failure	N	2	10	20	3
15/03/2018	Bylong Valley Way		Rural	F/W	440	Pavement Failure - either side of railway lines	N	2	20	40	3
24/02/2016	Bylong Valley Way		Rural	RHS	2500	Patch failure	N	2	15	30	3
24/02/2016	Bylong Valley Way		Rural	LHS	2790	Pavement Failure	N	2	40	80	3
16/05/2013	Bylong Valley Way		Rural	LHS	2905	Pavement Failure	N	2	2	4	3
24/02/2016	Bylong Valley Way		Rural	RHS	3050	Pavement Failure	N	2	100	200	3
24/02/2016	Bylong Valley Way		Rural	CENTRE	3230	Pavement Failure	N	3	3	9	3
15/03/2018	Bylong Valley Way		Rural	CENTRE	4020	Pavement Failure	N	2	3	6	3
16/05/2013	Bylong Valley Way		Rural	RHS	4140	Pavement Failure	N	5	2.5	12.5	3
15/03/2018	Bylong Valley Way		Rural	RHS	4450	Pavement Failure	N	2	5	10	3
31/01/2014	Bylong Valley Way		Rural	LHS	4506	Pavement Failure	N	4	4	16	3
15/03/2018	Bylong Valley Way		Rural	RHS	6040	Pavement Failure	N	2	2	4	3
15/03/2018	Bylong Valley Way		Rural	CENTRE	6105	Pavement Failure	N	3	10	30	3
24/02/2016	Bylong Valley Way		Rural	F/W	6778	Trench across road	N	2	8	16	3
24/02/2016	Bylong Valley Way		Rural	LHS	6940	Pavement Failure	N	2	5	10	3
24/02/2016	Bylong Valley Way		Rural	RHS	6940	Failed pavement	N	2	80	160	3
15/03/2018	Bylong Valley Way		Rural	RHS	7055	Pavement failure	N	2	3	6	3
24/02/2016	Bylong Valley Way		Rural	LHS	7220	Pavement Failure	N	2	3	6	3
24/02/2016	Bylong Valley Way		Rural	LHS	7330	Pavement Failure	N	2	40	80	3
24/02/2016	Bylong Valley Way		Rural	LHS	7410	Failed patch	N	3	10	30	3
6/11/2014	Bylong Valley Way		Rural	LHS	7510	Pavement Failure	N	2	2	4	3
31/01/2014	Bylong Valley Way		Rural	LHS	7550	Pavement Failure	N	2	2	4	3
2/02/2017	Bylong Valley Way		Rural	LHS	7880	Pavement Failure	N	3	3	9	3
24/02/2016	Bylong Valley Way		Rural	LHS	8300	Pavement Failure	N	2	3	6	3
24/02/2016	Bylong Valley Way		Rural	RHS	8510	Pavement Failure	N	2	10	20	3
2/02/2017	Bylong Valley Way		Rural	LHS	8510	Pavement Failure	N	2	2	4	3
15/03/2018	Bylong Valley Way		Rural	RHS	8545	Pavement Failure	N	2	5	10	3
15/03/2018	Bylong Valley Way		Rural	RHS	8850	Pavement Failure	N	2	2	4	3
24/02/2016	Bylong Valley Way		Rural	RHS	10071	Pavement Failure	N	2	2	4	3
24/02/2016	Bylong Valley Way		Rural	LHS	10560	Pavement Failure	N	2	5	10	3
15/03/2018	Bylong Valley Way		Rural	RHS	10990	Pavement Failure	N	2	10	20	3
6/11/2014	Bylong Valley Way		Rural	RHS	11030	Pavement Failure	N	2	5	10	3
15/03/2018	Bylong Valley Way		Rural	LHS	11410	Pavement Failure	N	4	20	80	3
6/11/2014	Bylong Valley Way		Rural	RHS	14015	Pavement Failure	N	2	5	10	3
6/11/2014	Bylong Valley Way		Rural	RHS	14095	Pavement Failure	N	2	15	30	3
24/02/2016	Bylong Valley Way		Rural	RHS	14246	Pavement Failure	N	2	2	4	3
2/02/2017	Bylong Valley Way		Rural	RHS	14275	Pavement Failure	N	2	2	4	3
15/03/2018	Bylong Valley Way		Rural	LHS	14475	Pavement Failure	N	2	20	40	3
15/03/2018	Bylong Valley Way		Rural	LHS	14700	Pavement Failure	N	3	40	120	3
2/02/2017	Bylong Valley Way		Rural	LHS	16780	Pavement Failure	N	2	2	4	3
2/02/2017	Bylong Valley Way		Rural	F/W	18470	Pavement Failure	N	8	2	16	3
15/03/2018	Bylong Valley Way		Rural	LHS	18980	Pavement Failure	N	4	10	40	3
15/03/2018	Bylong Valley Way		Rural	RHS	19500	Pavement Failure	N	2	2	4	3
2/02/2017	Bylong Valley Way		Rural	LHS	19910	Pavement Failure	N	2	5	10	3
31/01/2014	Bylong Valley Way		Rural	LHS	21183	Pavement Failure	N	2	2	4	3
2/02/2017	Bylong Valley Way		Rural	F/W	21450	Pavement Failure	N	8	20	160	3
15/03/2018	Bylong Valley Way		Rural	LHS	22200	Pavement Failure	N	3	15	45	3

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
15/03/2018	Bylong Valley Way		Rural	F/W	22840	Trench	N	2	8	16	3
15/03/2018	Bylong Valley Way		Rural	LHS	23300	pavement Failure	N	2	2	4	3
15/03/2018	Bylong Valley Way		Rural	LHS	23340	pavement Failure	N	2	5	10	3
2/02/2017	Bylong Valley Way		Rural	LHS	24120	Pavement Failure	N	2	30	60	3
16/05/2013	Bylong Valley Way		Rural	LHS	30000	Pavement Failure	N	5	2.5	12.5	3
15/03/2018	Bylong Valley Way		Rural	LHS	30215	Pavement Failure	N	2	3	6	3
15/03/2018	Bylong Valley Way		Rural	RHS	31920	Pavement Failure	N	3	5	15	3
2/02/2017	Bylong Valley Way		Rural	RHS	31570	Pavement Failure	N	30	2	60	3
6/11/2014	Bylong Valley Way		Rural	RHS	31720	Pavement Failure	N	2	5	10	3
2/02/2017	Bylong Valley Way		Rural	RHS	31860	Pavement Failure	N	2	3	6	3
24/02/2016	Bylong Valley Way		Rural	CENTRE	34850	Pavement Failure	N	2	5	10	3
27/08/2015	Cabemet St	Muswellbrook	Urban		Cnr Shiraz	Rippling effect in pavement	N	2.5	20	50	3
20/03/2017	Cabemet St	Muswellbrook	Urban		26-28	Pavement failure	N	2	20	40	3
20/03/2017	Cabemet St	Muswellbrook	Urban		Cnr Reisling St	Pavement failure	Y	2	10	20	3
20/03/2017	Cabemet St	Muswellbrook	Urban		Opp 31\33	Pavement failure	Y	2	30	60	3
5/04/2016	Calgaroo Ave	Muswellbrook	Urban		34-38	Trench next to gutter	Y	2	20	40	3
5/04/2016	Calgaroo Ave	Muswellbrook	Urban		13 Grevillea	Trench next to gutter	Y	2	10	20	3
8/01/2015	Campion Ln	Muswellbrook	Urban		near Int Market Ln	Pavement Failure	N	3	2	6	3
29/03/2017	Carl Ln	Muswellbrook	Urban		4	Pavement Failure	N	2	2	4	3
18/04/2018	Carl St	Muswellbrook	Urban		66	Pavement Failure	N	2	2	4	3
12/12/2019	Carl St	Muswellbrook	Urban		Cnr Brook St	Pavement Failure	N	5	5	25	3
12/12/2019	Carl St	Muswellbrook	Urban		43	Pavement Failure	N	5	2	10	3
7/04/2016	Cassidy Ave	Muswellbrook	Urban		12	Depression	N	2	2	4	3
9/09/2014	Castlerock Road		Rural		1010	Pavement Failure	N	2	60	120	3
15/11/2016	Castlerock Road		Rural	LHS	2290	Pavement Failure	N	2	20	40	3
15/11/2016	Castlerock Road		Rural	RHS	2540	Pavement Failure	N	2	50	100	3
5/10/2012	Castlerock Road		Rural	LHS	3470	Pavement Failure	N	2	10	20	3
4/01/2018	Castlerock Road		Rural	LHS	5375	Pavement Failure	N	2	10		3
15/11/2016	Castlerock Road		Rural	LHS	5400	Pavement Failure	N	2	2	4	3
27/01/2016	Castlerock Road		Rural	RHS	8170	Pavement Failure	N	2	30	60	3
4/01/2018	Castlerock Road		Rural	LHS	8700	Pavement Failure	N	3	5	15	3
15/11/2016	Castlerock Road		Rural	LHS	9135	Pavement Failure	N	2	5	10	3
15/11/2016	Castlerock Road		Rural	LHS	9160	Pavement Failure	N	2	3	6	3
4/01/2018	Castlerock Road		Rural	RHS	9770	Pavement Failure	N	2	10	20	3
15/11/2016	Castlerock Road		Rural	RHS	9925	Pavement Failure	N	2	10	20	3
4/01/2018	Castlerock Road		Rural	LHS	10090	Pavement Failure	N	2	20		3
15/11/2016	Castlerock Road		Rural	LHS	13850	Pavement Failure	N	2	2	4	3
15/11/2016	Castlerock Road		Rural	RHS	14270	Pavement Failure	N	2	10	20	3
4/01/2018	Castlerock Road		Rural	RHS	14830	Pavement Failure	N	2	2	4	3
4/12/2013	Castlerock Road		Rural	RHS	17520	Pavement Failure	N	2	50	100	3
4/01/2018	Castlerock Road		Rural		17600	Pavement Failure	N	2	40	80	3
27/01/2016	Castlerock Road		Rural	F/W	18020	Trench	N	2	6	12	3
27/01/2016	Castlerock Road		Rural		18395	Pavement Failure	N	2	10	20	3
15/11/2016	Castlerock Road		Rural	RHS	18890	Pavement Failure	N	2	5	10	3
29/10/2019	Chablis Cl	Muswellbrook	Urban		14	Pavement Failure	N	2	20	40	3
4/07/1905	Chardonnay St	Muswellbrook	Urban		30	Pavement Failure	N	2	2	4	3
8/12/2014	Claret Avenue	Muswellbrook	Urban		Various	7 x trenches across road	N	14	8	112	3
9/04/2018	Coal Road	Muswellbrook	Urban	LHS	Ch 130	Pavement failure	N	2	3	6	3
8/01/2015	Coal Road	Muswellbrook	Urban	LHS	Ch 450	Pavement Failure	N	2	60	120	3
8/01/2015	Coal Road	Muswellbrook	Urban	CENTRE	Ch 735	Pavement Failure	N	3	10	30	3
8/01/2015	Coal Road	Muswellbrook	Urban	LHS	Ch 745	Pavement Failure	N	2	10	20	3
8/01/2015	Coal Road	Muswellbrook	Urban	RHS	Ch 1185	Pavement Failure	N	5	5	25	3
8/01/2015	Coal Road	Muswellbrook	Urban	LHS	Ch 1260	Pavement Failure	N	2	4	8	3
8/01/2015	Coal Road	Muswellbrook	Urban	RHS	Ch 1420	Pavement Failure	N	2	20	40	3
	2012 Cousins St	Muswellbrook	Urban		Int Humphries (west)	Pavement Failure	N	5	4	20	3

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
8/12/2014	Cousins St	Muswellbrook	Urban		Various	Trench across road	N				3
21/03/2017	Cousins St	Muswellbrook	Urban		30	Trench	N	2	8	16	3
21/03/2017	Cousins St	Muswellbrook	Urban		40	Trench	N	2	8	16	3
29/10/2019	Cousins St	Muswellbrook	Urban		1	Pavement failure	N	2	2	4	3
29/10/2019	Cousins St	Muswellbrook	Urban		50	Pavement failure	N	2	2	4	3
19/02/2018	Crinoline St	Denman	Urban		12	W&W pit?	N	2	3	6	3
19/02/2018	Crinoline St	Denman	Urban		27	W&W pit?	N	3	4	12	3
	2012 Dalswinton Road		Rural	LHS	3387	Pavement Failure	N	2	2	4	3
9/11/2016	Dalswinton Road		Rural	LHS	90	Pavement Failure	N	2	5	10	3
15/02/2016	Dalswinton Road		Rural	RHS	360	Pavement Failure	N	2	30	60	3
15/02/2016	Dalswinton Road		Rural	RHS	700	Pavement Failure	N	2	2	4	3
9/11/2016	Dalswinton Road		Rural	RHS	920	Pavement Failure	N	2	5	10	3
9/11/2016	Dalswinton Road		Rural	RHS	980	Pavement Failure	N	2	5	10	3
15/02/2016	Dalswinton Road		Rural	RHS	1060	Pavement Failure	N	2	20	40	3
15/02/2016	Dalswinton Road		Rural	LHS	1500	Pavement Failure	N	2	25	50	3
9/11/2016	Dalswinton Road		Rural	RHS	1980	Pavement Failure	N	2	20	40	3
15/01/2018	Dalswinton Road		Rural	RHS	870	Pavement Failure	N	3	40	120	3
15/01/2018	Dalswinton Road		Rural	LHS	900	Pavement Failure	N	2	10	20	3
15/01/2018	Dalswinton Road		Rural	RHS	1010	Pavement Failure	N	2	3	6	3
15/01/2018	Dalswinton Road		Rural	RHS	1960	Pavement Failure	N	2	30	60	3
20/05/2016	Dalwood Place	Muswellbrook	Urban		7	Trench x 2	N	2	14	28	3
18/12/2017	Dartbrook Road		Rural	F/W	78	Trench	N	4	6	24	3
8/09/2014	Dartbrook Road		Rural	F/W	300	Trench	N	2	6	12	3
19/01/2016	Dartbrook Road		Rural	F/W	476	Trench	N	2	6	12	3
12/11/2016	Dartbrook Road		Rural	F/W	750	Trench	N	2	6	12	3
12/11/2016	Dartbrook Road		Rural	F/W	880	Trench	N	2	6	12	3
12/11/2016	Dartbrook Road		Rural	F/W	1070	Trench	N	2	6	12	3
12/11/2016	Dartbrook Road		Rural	RHS	1375	pavement Failure	N	2	5	10	3
12/11/2016	Dartbrook Road		Rural	RHS	1520	Pavement Failure	N	2	10	20	3
4/10/2012	Dartbrook Road		Rural	LHS	1560	Pavement Failure	N	2	2	4	3
8/09/2014	Dartbrook Road		Rural	F/W	1580	Trench	N	6	2	12	3
	2012 Donaldson St	Muswellbrook	Urban		Various	6 trenches across road	N	12	9	108	3
	2012 Dorset Road		Rural	LHS	160	Pavement Failure	N	2	2	4	3
9/09/2014	Dorset Road		Rural		109	Pavement Failure	N	2	2	4	3
9/09/2014	Dorset Road		Rural		260	Pavement Failure	N	2	2	4	3
19/03/2019	Dorset Road		Rural	RHS	325	Pavement Failure	N	2	2	4	3
9/09/2014	Dorset Road		Rural		470	Pavement Failure	N	2	20	40	3
19/01/2016	Dorset Road		Rural	LHS	635	Pavement Failure	N	2	2	4	3
15/11/2016	Dorset Road		Rural	F/W	1370	Pavement Failure	N	3	6	18	3
15/11/2016	Dorset Road		Rural	F/W	2880	Pavement Failure	N	3	6	18	3
15/11/2016	Dorset Road		Rural	LHS	3575	Pavement Failure	N	2	35	70	3
15/11/2016	Dorset Road		Rural	RHS	3600	Pavement Failure	N	2	3	6	3
15/11/2016	Dorset Road		Rural	RHS	3680	Pavement Failure	N	2	30	60	3
12/02/2018	Doyle Lane	Muswellbrook	Urban		11	Pavement Failure	N	2	5	10	3
29/03/2017	Doyle Lane	Muswellbrook	Urban		1	Trench	N	2	4	8	3
12/02/2018	Doyle Lane	Muswellbrook	Urban		5	Pavement Failure	N	2	3	6	3
12/02/2018	Doyle Lane	Muswellbrook	Urban		cnr George	Pavement Failure	N	2	3	6	3
10/02/2016	Dry Creek Road		Rural		1478	Pavement Failure	N	2	4	8	3
15/02/2016	Duggans Road		Rural	RHS	Int Dalswinton	Pavement Failure	N	2	5	10	3
15/02/2016	Duggans Road		Rural	LHS	120	Pavement Failure	N	2	30	60	3
15/02/2016	Duggans Road		Rural	LHS	210	Pavement Failure	N	2	15	30	3
15/02/2016	Duggans Road		Rural	RHS	304	Pavement Failure	N	2	60	120	3
15/02/2016	Duggans Road		Rural	LHS	435	Pavement Failure	N	2	40	80	3
15/02/2016	Duggans Road		Rural	F/W	595	Pavement Failure	N	5	20	100	3
9/12/2014	Dumaresq St	Muswellbrook	Urban		2	Trench	N	2	9	18	3

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
5/04/2017	Dumaresq St	Muswellbrook	Urban		Int Sowerby	Trench	N	2	10	20	3
9/11/2016	Edderton Road		Rural	RHS	50	Pavement Failure	N	2	20	40	3
9/11/2016	Edderton Road		Rural	LHS	670	Pavement Failure	N	2	10	20	3
9/11/2016	Edderton Road		Rural	RHS	730	Pavement Failure	N	2	40	80	3
9/11/2016	Edderton Road		Rural	RHS	890	Pavement Failure	N	3	5	15	3
9/11/2016	Edderton Road		Rural	F/W	950	Pavement Failure - over culvert	N	5.6	10	56	3
9/11/2016	Edderton Road		Rural	LHS	1230	Failed Patch	N	3	15	45	3
9/11/2016	Edderton Road		Rural	RHS	1230	Pavement Failure	N	2	3	6	3
9/11/2016	Edderton Road		Rural	RHS	1380	Pavement Failure	N	2	50	100	3
9/11/2016	Edderton Road		Rural	LHS	1445	Pavement Failure	N	5	5	25	3
9/11/2016	Edderton Road		Rural	RHS	1470	Pavement Failure	N	5.6	10	56	3
9/11/2016	Edderton Road		Rural	LHS	1500	Pavement Failure	N	2	50	100	3
9/11/2016	Edderton Road		Rural	RHS	1500	Pavement Failure	N	2	20	40	3
9/11/2016	Edderton Road		Rural	RHS	1650	Pavement Failure	N	2	140	280	3
9/11/2016	Edderton Road		Rural	RHS	1840	Pavement Failure	N	2	25	50	3
9/11/2016	Edderton Road		Rural	RHS	2050	Pavement Failure	N	2	50	100	3
9/11/2016	Edderton Road		Rural	RHS	2200	Pavement Failure	N	2	250	500	3
9/11/2016	Edderton Road		Rural	LHS	2500	Pavement Failure	N	2	60	120	3
9/11/2016	Edderton Road		Rural	RHS	2740	Pavement Failure	N	2	40	80	3
9/11/2016	Edderton Road		Rural	RHS	2950	Pavement Failure	N	2	20	40	3
9/11/2016	Edderton Road		Rural	RHS	3040	Pavement Failure	N	2	20	40	3
9/11/2016	Edderton Road		Rural	RHS	3420	Pavement Failure	N	2	20	40	3
9/11/2016	Edderton Road		Rural	LHS	4200	Failed Patch	N	3	6	18	3
9/11/2016	Edderton Road		Rural	LHS	4490	Pavement Failure	N	2	3	6	3
9/11/2016	Edderton Road		Rural	LHS	4800	Pavement Failure	N	3	3	9	3
9/11/2016	Edderton Road		Rural	RHS	4870	Pavement Failure	N	2	40	80	3
9/11/2016	Edderton Road		Rural	F/W	5140	Failed Patch	N	6	25	150	3
9/11/2016	Edderton Road		Rural	RHS	6900	Pavement Failure	N	2	210	420	3
9/11/2016	Edderton Road		Rural	RHS	7240	Pavement Failure	N	2	60	120	3
9/11/2016	Edderton Road		Rural	Centre	7765	Dip	N	3	3	9	3
9/11/2016	Edderton Road		Rural	LHS	7840	Pavement Failure	N	2	40	80	3
9/11/2016	Edderton Road		Rural	LHS	8040	Pavement Failure	N	2	20	40	3
9/11/2016	Edderton Road		Rural	RHS	11810	Pavement Failure	N	2	30	60	3
9/11/2016	Edderton Road		Rural	Centre	11860	Pavement Failure	N	2	30	60	3
9/11/2016	Edderton Road		Rural	LHS	11930	Pavement Failure	N	2	50	100	3
9/11/2016	Edderton Road		Rural	RHS	11930	Pavement Failure	N	2	30	60	3
9/11/2016	Edderton Road		Rural	RHS	11995	Pavement Failure	N	3	20	60	3
9/11/2016	Edderton Road		Rural	RHS	12075	Pavement Failure	N	3	3	9	3
9/11/2016	Edderton Road		Rural	LHS	12100	Pavement Failure	N	2	220	440	3
9/11/2016	Edderton Road		Rural	RHS	12180	Pavement Failure	N	2	110	220	3
9/11/2016	Edderton Road		Rural	RHS	12320	Pavement Failure	N	2	50	100	3
9/11/2016	Edderton Road		Rural	LHS	12370	Pavement Failure	N	2	70	140	3
9/11/2016	Edderton Road		Rural	LHS	12470	Pavement Failure	N	2	40	80	3
9/11/2016	Edderton Road		Rural	RHS	12500	Pavement Failure	N	2	290	580	3
9/11/2016	Edderton Road		Rural	LHS	12920	Pavement Failure	N	2	20	40	3
9/11/2016	Edderton Road		Rural	RHS	12940	Pavement Failure	N	2	70	140	3
9/11/2016	Edderton Road		Rural	LHS	13065	Pavement Failure	N	2	50	100	3
9/11/2016	Edderton Road		Rural	RHS	13100	Pavement Failure	N	2	150	300	3
9/11/2016	Edderton Road		Rural	LHS	13200	Pavement Failure	N	2	110	220	3
9/11/2016	Edderton Road		Rural	LHS	13340	Pavement Failure	N	2	80	160	3
9/11/2016	Edderton Road		Rural	LHS	13500	Pavement Failure	N	2	20	40	3
9/11/2016	Edderton Road		Rural	LHS	13610	Pavement Failure	N	2	25	50	3
9/11/2016	Edderton Road		Rural	RHS	13800	Pavement Failure	N	2	50	100	3
9/11/2016	Edderton Road		Rural	LHS	13880	Pavement Failure	N	2	100	200	3
9/11/2016	Edderton Road		Rural	RHS	13960	Pavement Failure	N	2	40	80	3

## Attachment G

HEAVY PATCHING PROGRAM										
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area Priority
19/01/2016	Ely St	Kayuga	Rural	F/W		Pavement Failure	N	3	3	9 3
	2012 Eucalypt Ave	Muswellbrook	Urban		Carpark Entrance	Badly restored trench - width of pavement	N	2	8	16 3
4/04/2016	Fem Close	Denman	Urban		2	Trench next to gutter	N	5	2	10 3
9/05/2013	Femdale Road		Rural	RHS	3205	Pavement Failure	N	2	2.5	5 3
17/01/2017	Femdale Road		Rural	LHS	1830	Pavement Failure	N	2	10	20 3
17/01/2017	Femdale Road		Rural	RHS	1865	Pavement Failure	N	2	3	6 3
17/01/2017	Femdale Road		Rural	RHS	1960	Pavement Failure	N	2	10	20 3
15/01/2014	Femdale Road		Rural	F/W	2865	Pavement Failure	N	4	4	16 3
15/10/2014	Femdale Road		Rural	LHS	2420	Pavement Failure	N	2	2	4 3
25/01/2016	Femdale Road		Rural	F/W	2500	Pavement Failure	N	4	4	16 3
25/01/2016	Femdale Road		Rural	LHS	3200	Pavement Failure	N	2	2	4 3
25/01/2016	Femdale Road		Rural	RHS	3260	Pavement Failure	N	2	10	20 3
25/01/2016	Femdale Road		Rural	RHS	3280	Pavement Failure	N	2	2	4 3
25/01/2016	Femdale Road		Rural	RHS	3290	Pavement Failure	N	2	2	4 3
17/01/2017	Femdale Road		Rural	RHS	3060	Pavement Failure	N	2	3	6 3
17/01/2017	Femdale Road		Rural	LHS	3080	Pavement Failure	N	2	3	6 3
5/04/2017	Flanders Avenue	Muswellbrook	Urban		Int Dumaresq	Pavement Failure	N	3	7	21 3
5/04/2017	Flanders Avenue	Muswellbrook	Urban		14	Pavement Failure	N	3	3	9 3
5/04/2017	Flanders Avenue	Muswellbrook	Urban		15	Pavement Failure	N	3	5	15 3
18/11/2019	Flanders Avenue	Muswellbrook	Urban		18a	Pavement Failure	N	3	3	9 3
	2012 Fleming St	Muswellbrook	Urban		4\6	Pavement Failure	N	2	10	20 3
20/05/2016	Fleming St	Muswellbrook	Urban		12	Pavement Failure	N	3	3	9 3
20/05/2016	Fleming St	Muswellbrook	Urban		14	Pavement Failure	N	5	10	50 3
11/04/2017	Fleming St	Muswellbrook	Urban		16	Pavement Failure	N	3	3	9 3
11/04/2017	Fleming St	Muswellbrook	Urban		8	Pavement Failure	Y	2	5	10 3
11/04/2017	Fleming St	Muswellbrook	Urban		3	Pavement Failure	N	2	2	4 3
11/02/2020	Fleming St	Muswellbrook	Urban		11\15	Pavement Failure	N	2	40	80 3
8/01/2015	Foley Street	Muswellbrook	Urban		Various	Trenches	N			3
12/02/2018	Ford St	Muswellbrook	Urban		opp 52	Failed Patch	N	2	30	60 3
	2012 Ford St	Muswellbrook	Urban		72	Pavement Failure	N	2	2	4 3
23/12/2014	Ford St	Muswellbrook	Urban		72	Trench	N	2	12	24 3
6/03/2017	Ford St	Muswellbrook	Urban		Scout Hall	Pavement Failure	N	5	5	25 3
6/03/2017	Ford St	Muswellbrook	Urban		6	Trench	N	2	10	20 3
6/03/2017	Ford St	Muswellbrook	Urban		Int Wilkins	Pavement Failure	N	3	3	9 3
12/02/2018	Ford St	Muswellbrook	Urban		78	Trench	N	2	8	16 3
12/02/2018	Ford St	Muswellbrook	Urban		63	Failed patch	N	2	2	4 3
12/02/2018	Ford St	Muswellbrook	Urban		39-41	Pavement Failure	N	2	40	80 3
	2012 George St	Muswellbrook	Urban		Cnr Bligh Ln	Failure near K&G	N	4	4	16 3
18/02/2016	Giants Creek Road		Rural	RHS	94	Depression	N	2	2	4 3
18/02/2016	Giants Creek Road		Rural	LHS	1230	Pavement Failure	N	2	2	4 3
17/01/2017	Giants Creek Road		Rural	LHS	1630	Pavement Failure	N	3	3	9 3
17/01/2017	Giants Creek Road		Rural	RHS	2870	Pavement Failure	N	3	3	9 3
24/01/2018	Giants Creek Road		Rural	RHS	7750	Pavement Failure	N	3	3	9 3
17/01/2017	Giants Creek Road		Rural	RHS	9770	Pavement Failure	N	2	2	4 3
17/01/2017	Giants Creek Road		Rural	F/W	12340	Pavement Failure	N	2	5	10 3
18/02/2016	Giants Creek Road		Rural	RHS	14040	Pavement Failure	N	2	2	4 3
24/01/2018	Giants Creek Road		Rural	F/W	14060	Pavement Failure	N	2	4	8 3
18/02/2016	Giants Creek Road		Rural	RHS	16125	Heavy patch needed	N	5	10	50 3
10/04/2017	Glen Munro Road	Muswellbrook	Urban	LHS	Int Thomas Mitchell Drive	Pavement Failure	N	3	3	9 3
5/04/2016	Grevillea St	Muswellbrook	Urban		12	Pavement Failure	N	2	10	20 3
12/01/2015	Grey Gum Road	Denman	Urban		Various	Trenches x 6	N	8	12	96 3
17/04/2018	Hakea Drive	Muswellbrook	Urban		18	Pavement Failure	N	2	3	6 3
5/04/2017	Hastings Street	Muswellbrook	Urban		11\13	Pavement Failure	N	9	20	180 3
9/01/2020	Haydon St	Muswellbrook	Urban		1	Dip	N	2	2	4 3

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
6/12/2017	Hebden Road		Rural	F/W	0-20	Pavement Failure	N	8	20	160	3
6/12/2017	Hebden Road		Rural	LHS	930	Pavement Failure	N	3	2	6	3
6/12/2017	Hebden Road		Rural	LHS	1240	Pavement Failure	N	3	2	6	3
6/12/2017	Hebden Road		Rural	LHS	2760	Pavement Failure	N	3	2	6	3
6/12/2017	Hebden Road		Rural	RHS	2800	Pavement Failure	N	2	40	80	3
6/12/2017	Hebden Road		Rural	LHS	3770	Pavement Failure	N	2	5	10	3
6/12/2017	Hebden Road		Rural	LHS	4960	Pavement Failure	N	2	80	160	3
16/02/2018	Henry Dangar Rd	Muswellbrook	Urban		Roundabout Day St	Pavement Failure	N	3	2	6	3
	2012 Hill St	Muswellbrook	Urban		115/117	Trench 1/2 way across road		2	10	20	3
12/12/2019	Hill St	Muswellbrook	Urban		115	Pavement failure	N	2	8	16	3
6/01/2015	Hill St	Muswellbrook	Urban		122	Trench (W&W)	N	2	8	16	3
6/01/2015	Hill St	Muswellbrook	Urban		123	Trench	N	2	18	36	3
18/04/2018	Hill St	Muswellbrook	Urban		Opp 173	Pavement Failure	N	2	3	6	3
18/04/2018	Hill St	Muswellbrook	Urban		124	Pavement Failure	N	2	5	10	3
4/12/2013	Horseshoe Road		Rural	F/W	3975	Pavement Failure	N	6	75	450	3
23/01/2017	Horseshoe Road		Rural	F/W	680	Over culvert	N	6	2	12	3
23/01/2017	Horseshoe Road		Rural	F/W	708	Pavement Failure	N	6	20	120	3
23/01/2017	Horseshoe Road		Rural	F/W	1090	Over culvert	N	6	10	60	3
23/01/2017	Horseshoe Road		Rural	F/W	2020	Over culvert	N	6	4	24	3
23/01/2017	Horseshoe Road		Rural	RHS	2070	Pavement failure	N	2	3	6	3
22/02/2016	Horseshoe Road		Rural	F/W	2220	Over culvert	N	6	5	30	3
23/01/2017	Horseshoe Road		Rural	F/W	2350	Pavement Failure	N	6	5	30	3
22/02/2016	Horseshoe Road		Rural	F/W	2360	Pavement Failure	N	6	5	30	3
23/01/2017	Horseshoe Road		Rural	F/W	2480	Pavement Failure	N	6	30	180	3
22/02/2016	Horseshoe Road		Rural	F/W	2485	Centre shove	N	6	20	120	3
22/02/2016	Horseshoe Road		Rural		2670	Pavement Failure	N	2	300	600	3
23/01/2017	Horseshoe Road		Rural	RHS	2740	Pavement Failure	N	2	400	800	3
22/02/2016	Horseshoe Road		Rural	F/W	3090	Pavement Failure	N	6	50	300	3
21/03/2017	Humphries St	Muswellbrook	Urban		6	Pavement Failure	N	3	3	9	3
7/02/2017	Hungerford Road		Rural	F/W	1690	Over culvert	N	4	5	20	3
9/12/2014	Hunter Street	Muswellbrook	Urban		Karoola Park	Dip over stormwater drain	N	2	23	46	3
19/02/2018	Hunter Street	Denman	Urban		32	Pavement Failure	N	3	3	9	3
23/12/2014	Hunter Terrace	Muswellbrook	Urban		3	Trench x 2	N	4	18	72	3
23/12/2014	Hunter Terrace	Muswellbrook	Urban		36	Pavement Failure	N	2	10	20	3
23/12/2014	Hunter Terrace	Muswellbrook	Urban		Int Lower Hill Street	2 x Pavement Failures	N	4	4	16	3
7/03/2017	Hunter Terrace	Muswellbrook	Urban		26 (Centre)	Pavement Failure	N	3	10	30	3
22/11/2019	Hunter Terrace	Muswellbrook	Urban		3	Trench	N	2	10	20	3
4/04/2016	Hyde St	Denman	Urban		2	Trench	N	8	2	16	3
4/04/2016	Hyde St	Denman	Urban		Int Macauley St	Pavement Failure	N	3	3	9	3
5/04/2016	Ironbark Rd	Muswellbrook	Urban		Lot 127	Trench	N	8	3	24	3
5/04/2016	Ironbark Rd	Muswellbrook	Urban		Int Calgaroo	Trench	N	8	2	16	3
10/04/2017	Ironbark Rd	Muswellbrook	Urban		1 Rutherford Road	Rippling effect in pavement	N	3	2	6	3
10/04/2017	Ironbark Rd	Muswellbrook	Urban		42	Trench	N	2	10	20	3
10/04/2017	Ironbark Rd	Muswellbrook	Urban		1-1c	Failed patch	N	2	20	40	3
8/12/2014	Isobella St	Muswellbrook	Urban		15	Pavement Failure	Y	2	80	160	3
16/02/2018	Jeans St	Muswellbrook	Urban		5	Trench	N	2	3	6	3
15/01/2015	Jordan St	Muswellbrook	Urban		Length of road	Pavement Failure	Y	2	140	280	3
20/01/2015	Kamilaroi Street	Muswellbrook	Urban		8	Pavement Failure	N	2	3	6	3
7/04/2016	Kamilaroi Street	Muswellbrook	Urban		36	Trench x 2	N	16	2	32	3
12/04/2017	Kamilaroi Street	Muswellbrook	Urban		6	Patch needed		2	3	6	3
12/04/2017	Kamilaroi Street	Muswellbrook	Urban		1	Trench	N	2	9	18	3
5/04/2016	Karri Place	Muswellbrook	Urban		3	Pavement Failure	N	3	3	9	3
18/12/2017	Kayuga Road		Rural	LHS	3790	Pavement Failure	N	2	20	40	3
12/11/2016	Kayuga Road		Rural	LHS	3970	Pavement Failure	N	2	30	60	3
12/11/2016	Kayuga Road		Rural	RHS	4050	Pavement Failure	N	2	10	20	3



## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
12/11/2016	Kayuga Road		Rural	RHS	4130	Pavement Failure	N	2	10	20	3
6/01/2014	Kayuga Road		Rural	LHS	4190	Pavement Failure	N	2	3	6	3
12/11/2016	Kayuga Road		Rural	RHS	4250	Pavement Failure	N	2	50	100	3
12/11/2016	Kayuga Road		Rural	RHS	4440	Pavement Failure	N	2	30	60	3
12/11/2016	Kayuga Road		Rural	LHS	4710	Pavement Failure	N	2	10	20	3
18/12/2017	Kayuga Road		Rural	LHS	4810	Pavement Failure	N	2	5	10	3
18/12/2017	Kayuga Road		Rural	LHS	4830	Pavement Failure	N	2	2	4	3
24/04/2015	Kayuga Road		Rural	RHS	4840	Pavement Failure	N	2	7	14	3
19/01/2016	Kayuga Road		Rural	F/W	4920	Trench	N	2	7	14	3
12/11/2016	Kayuga Road		Rural	LHS	4960	Pavement Failure	N	3	10	30	3
18/12/2017	Kayuga Road		Rural	RHS	4990	Pavement Failure	N	2	10	20	3
12/11/2016	Kayuga Road		Rural	RHS	5010	Pavement Failure	N	2	70	140	3
12/11/2016	Kayuga Road		Rural	LHS	5040	Pavement Failure	N	2	10	20	3
12/11/2016	Kayuga Road		Rural	RHS	5050	Pavement Failure	N	2	100	200	3
12/11/2016	Kayuga Road		Rural	LHS	5090	Pavement Failure	N	2	20	40	3
12/11/2016	Kayuga Road		Rural	LHS	6150	Pavement Failure	N	2	20	40	3
12/11/2016	Kayuga Road		Rural	RHS	6300	Pavement Failure	N	3	5	15	3
4/04/2016	Kenilworth St	Denman	Urban		21	Trench	N	8	2	16	3
2012	King St	Muswellbrook	Urban		3	2x2 heavy patch		2	2	4	3
2012	King St	Muswellbrook	Urban		Int Upper William Street	Pavement Failure		2	2	4	3
6/01/2015	King St	Muswellbrook	Urban		63	Trench	N	2	11	22	3
6/01/2015	King St	Muswellbrook	Urban		65	Trench	N	2	11	22	3
6/01/2015	King St	Muswellbrook	Urban		80	Pavement Failure	N	2	2	4	3
27/05/2016	King Street	Muswellbrook	Urban		214	Pavement Failure	N	2	2	4	3
27/05/2016	King Street	Muswellbrook	Urban		70-72	Pavement Failure	N	2	10	20	3
14/01/2015	Kokoda St	Muswellbrook	Urban		Int Tarakan	Trench	N	2	10	20	3
27/05/2016	Koombahla St	Muswellbrook	Urban		Cnr Roger St	Trench x 2	N	2	18	36	3
20/05/2016	Lae St	Muswellbrook	Urban		Cul de sac	Pavement Failure	N	2	2	4	3
20/03/2017	Lexia St	Muswellbrook	Urban		14	Pavement Failure	N	3	3	9	3
27/03/2018	Lexia St	Muswellbrook	Urban		18	Trench	N	2	8	16	3
15/11/2016	Logues Lane		Rural	LHS	2100	Pavement Failure	N	2	2	4	3
15/01/2015	Lome St	Muswellbrook	Urban		36	Pavement Failure	N	2	5	10	3
15/01/2015	Lome St	Muswellbrook	Urban		42	Trench along gutter		2	10	20	3
11/04/2017	Lome St	Muswellbrook	Urban		3	Pavement failure	N	3	2	6	3
11/04/2017	Lome St	Muswellbrook	Urban		Cnr Forbes	Pavement failure	N	3	3	9	3
18/04/2018	Lome St	Muswellbrook	Urban		32	Pavement failure	N	3	5	15	3
9/04/2018	Lome St	Muswellbrook	Urban		14	Pavement failure	N	3	3	9	3
4/02/2020	Lome St	Muswellbrook	Urban		34	Pavement failure	N	3	3	9	3
23/12/2014	Lower Brook St	Muswellbrook	Urban		17	Trench	N	2	5	10	3
6/03/2017	Lower Brook St	Muswellbrook	Urban		High School Farm	Failed patch	N	3	20	60	3
9/04/2018	Lower Brook St	Muswellbrook	Urban		Verlie Weidmann Village	Pavement Failure	N	3	10	30	3
23/12/2014	Lower Hill St	Muswellbrook	Urban		51	Trench	N	2	10	20	3
23/12/2014	Lower Hill St	Muswellbrook	Urban		79 (at comer)	Cracking	N	2	2	4	3
7/03/2017	Lower Hill St	Muswellbrook	Urban		62	Pavement Failure	N	2	3	6	3
22/11/2019	Lower Hill St	Muswellbrook	Urban		70	Pavement Failure	N	3	3	9	3
22/11/2019	Lower Hill St	Muswellbrook	Urban		54	Trench 1/2 way	N	2	7	14	3
11/04/2017	Maitland St	Muswellbrook	Urban		Repco	Cracking	N	2	5	10	3
20/03/2017	Malbec St	Muswellbrook	Urban		Int Cabernet	Trench	N	2	15	30	3
29/10/2019	Malbec St	Muswellbrook	Urban		9	Pavement Failure	N	2	2	4	3
20/12/2017	Mangoola Road		Rural	LHS	45	Pavement Failure	N	3	3	9	3
21/01/2016	Mangoola Road		Rural	RHS	1150	Pavement Failure	N	2	2	4	3
2012	Mangoola Road		Rural	LHS	1620	Pavement Failure	N	4	5	20	3
8/01/2014	Mangoola Road		Rural	LHS	1960	Pavement Failure	N	2	2	4	3
20/12/2017	Mangoola Road		Rural	F/W	2095	Pavement Failure	N	4	2	8	3
8/01/2014	Mangoola Road		Rural	F/W	2275	Trench	N	2	6	12	3

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
2012	Mangoola Road		Rural	RHS	2409	Pavement Failure	N	2.5	30	75	3
20/12/2017	Mangoola Road		Rural	F/W	2720	Pavement Failure	N	4	2	8	3
2012	Mangoola Road		Rural	LHS	2865	Pavement Failure	N	2.5	2.5	6.25	3
20/12/2017	Mangoola Road		Rural	F/W	2935	Pavement Failure	N	4	2	8	3
26/08/2016	Mangoola Road		Rural	LHS	4740	Pavement Failure	N	2.5	5	12.5	3
21/01/2016	Mangoola Road		Rural	F/W	8417	Trench	N	2	4	8	3
10/09/2014	Mangoola Road		Rural	LHS	10630	Pavement Failure	N	2	3	6	3
21/01/2016	Mangoola Road		Rural	LHS	12157	Pavement Failure	N	2	5	10	3
21/01/2016	Mangoola Road		Rural	RHS	12460	Pavement Failure	N	2	2	4	3
21/01/2016	Mangoola Road		Rural	RHS	12560	Pavement Failure	N	2	3	6	3
21/01/2016	Mangoola Road		Rural	LHS	12603	Pavement Failure	N	2	20	40	3
21/01/2016	Mangoola Road		Rural	RHS	12620	Pavement Failure	N	2	15	30	3
7/04/2017	Market Lane	Muswellbrook	Urban		Zacks Automotive	Trench	N	2	11	22	3
16/10/2014	Martindale Road		Rural	RHS	1080	Pavement Failure	N	2	60	120	3
16/10/2014	Martindale Road		Rural	RHS	1180	Pavement Failure	N	2	5	10	3
23/01/2017	Martindale Road		Rural	RHS	1220	Pavement Failure	N	2	10	20	3
23/01/2017	Martindale Road		Rural	LHS	2790	Pavement Failure	N	2	3	6	3
23/01/2017	Martindale Road		Rural	LHS	4080	Pavement Failure	N	2	5	10	3
18/02/2016	Martindale Road		Rural	RHS	4425	Pavement Failure	N	2	3	6	3
18/02/2016	Martindale Road		Rural	RHS	4570	Pavement Failure	N	2	15	30	3
18/02/2016	Martindale Road		Rural	RHS	4610	Pavement Failure	N	2	20	40	3
18/02/2016	Martindale Road		Rural	RHS	4790	Pavement Failure	N	2	20	40	3
23/01/2017	Martindale Road		Rural	LHS	7810	Pavement Failure	N	2	5	10	3
15/01/2014	Martindale Road		Rural	LHS	7860	Pavement Failure	N	2	3	6	3
16/10/2014	Martindale Road		Rural	LHS	8085	Pavement Failure	N	3	20	60	3
2012	Martindale Road		Rural	LHS	8187	Pavement Failure	N	2.5	5	12.5	3
18/02/2016	Martindale Road		Rural	F/W	8220	Pavement Failure	N	6	10	60	3
18/02/2016	Martindale Road		Rural	RHS	8650	Pavement Failure	N	2	5	10	3
15/01/2014	Martindale Road		Rural	F/W	8780	Trench	N	2	8	16	3
22/02/2016	Martindale Road		Rural	RHS	8910	Pavement Failure	N	2	5	10	3
23/01/2017	Martindale Road		Rural	RHS	9580	Pavement Failure - over culvert	N	2	5	10	3
16/10/2014	Martindale Road		Rural	F/W	9860	Over culvert	N	2	7	14	3
23/01/2017	Martindale Road		Rural	RHS	13455	Pavement failure	N	2	2	4	3
18/02/2016	Martindale Road		Rural	LHS	14400	Pavement Failure	N	3	3	9	3
22/01/2018	Martindale Road		Rural	LHS	16490	Pavement Failure	N	2	2	4	3
16/10/2014	Martindale Road		Rural	RHS	17330	Pavement Failure	N	2	10	20	3
23/01/2017	Martindale Road		Rural		17760	Pavement Failure	N	2	5	10	3
23/01/2017	Martindale Road		Rural	F/W	18384	Pavement Failure	N	6	10	60	3
22/01/2018	Martindale Road		Rural	F/W	19160	Pavement Failure	N	2	6	12	3
15/01/2014	Martindale Road		Rural	RHS	19640	Pavement Failure	N	2	3	6	3
15/01/2014	Martindale Road		Rural	F/W	20260	Pavement Failure	N	5	30	150	3
23/01/2017	Martindale Road		Rural	LHS	21000	Pavement Failure	N	2	5	10	3
15/01/2014	Martindale Road		Rural	RHS	21140	Pavement Failure	N	3	20	60	3
4/04/2016	Martindale Street	Denman	Urban		30	Pavement Failure	N	2	2	4	3
14/03/2017	Martindale Street	Denman	Urban		16	Failed patch	N	2	15	30	3
12/04/2017	McClintock Dr	Muswellbrook	Urban		20\22	Pavement Failure	Y	2	30	60	3
19/02/2018	Merino Street	Denman	Urban		1	Failed patch	N	2	5	10	3
19/02/2018	Merino Street	Denman	Urban		1	Trench	N	2	7	14	3
23/05/2016	Merriwa Road	Sandy Hollow	Urban		Service Station	Pavement Failure	N	2	2	4	3
23/05/2016	Merriwa Road	Sandy Hollow	Urban		Opp Hotel	Pavement Failure	Y	2	20	40	3
2012	Merton Street	Denman	Urban		9	Pavement Sink	N	2	2	4	3
2012	Merton Street	Denman	Urban		12	Pavement Sink	Y	2	2	4	3
2012	Merton Street	Denman	Urban		16	Pavement Sink	Y	2	2	4	3
7/04/2017	Midanga Ave	Muswellbrook	Urban		3	Pavement failure	N	2	3	6	3
7/04/2017	Mill Street	Muswellbrook	Urban		9	Pavement failure	N	4	12	48	3

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
16/10/2013	Milpera Drive		Rural		167	Trench	N	2	6	12	3
16/10/2013	Milpera Drive		Rural		515	Trench	N	2	6	12	3
16/10/2013	Milpera Drive		Rural		550	Trench	N	2	6	12	3
16/10/2013	Milpera Drive		Rural		745	Pavement Failure	N	2	2	4	3
11/01/2016	Milpera Drive		Rural		95	Trench	N	2	6	12	3
15/01/2015	Mitchell Street	Muswellbrook	Urban		27	2 x trenches across road	N	4	10	40	3
15/01/2015	Mitchell Street	Muswellbrook	Urban		30	Pavement Failure x 2	N	2	6	12	3
23/05/2016	Mitchell Street	Muswellbrook	Urban		26	Pavement Failure	N	3	2	6	3
11/04/2017	Mitchell Street	Muswellbrook	Urban		34	Pavement failure	N	3	3	9	3
9/04/2018	Mitchell Street	Muswellbrook	Urban		81 Sydney St	Pavement failure	N	2	5	10	3
4/02/2020	Mitchell Street	Muswellbrook	Urban		5	Pavement failure	N	2	30	60	3
22/05/2013	Mt Dangar Road		Rural		3260	Pavement Failure	N	2	2	4	3
24/01/2018	Mt Dangar Road		Rural	F/W	5170	Pavement Failure	N	3	5	15	3
7/12/2017	Muscle Creek Road		Rural	RHS	3520	Pavement Failure	N	2	5	10	3
23/04/2013	Muscle Creek Road		Rural	RHS	3540	Pavement Failure	N	2	5	10	3
7/12/2017	Muscle Creek Road		Rural	F/W	3665	Trench	N	6	2	12	3
17/10/2013	Muscle Creek Road		Rural	LHS	4880	Pavement Failure	N	2	5	10	3
23/08/2016	Muscle Creek Road		Rural	RHS	6665	Pavement Failure	N	3	10	30	3
23/08/2016	Muscle Creek Road		Rural	LHS	6910	Pavement Failure	N	4	20	80	3
23/08/2016	Muscle Creek Road		Rural	RHS	7530	Heavy Patch	N	2	10	20	3
12/01/2016	Muscle Creek Road		Rural	RHS	7600	Pavement Failure	N	4	2.5	10	3
12/01/2016	Muscle Creek Road		Rural	LHS	8740	Pavement Failure	N	2	2	4	3
17/10/2013	Muscle Creek Road		Rural	RHS	9000	Pavement Failure - at floodway	N	2	2	4	3
11/04/2017	Nowland St	Muswellbrook	Urban		7	Pavement Failure	N	2	3	6	3
11/04/2017	Nowland St	Muswellbrook	Urban		Cnr Forbes	Pavement Failure	N	2	2	4	3
16/04/2018	Nowland St	Muswellbrook	Urban		2\4	Pavement Failure	N	2	20	40	3
9/04/2018	Nowland St	Muswellbrook	Urban		6-10	Pavement Failure	N	2	40	80	3
4/02/2020	Nowland St	Muswellbrook	Urban		13\15	Pavement Failure	N	2	40	80	3
4/04/2016	Ogilvie Street	Denman	Urban		Denman Hotel	Cracking/shove at driveway	N	15	2.5	37.5	3
4/04/2016	Ogilvie Street	Denman	Urban		1	Pavement Failure	N	2	2	4	3
4/04/2016	Ogilvie Street	Denman	Urban		3	Trench	N	10	2	20	3
4/04/2016	Ogilvie Street	Denman	Urban		Paxton-Virginia	6 x Trenches	N	8	12	96	3
17/04/2018	Ooranga St	Muswellbrook	Urban		8	Pavement failure	N	2	2	4	3
28/01/2020	Ooranga St	Muswellbrook	Urban		7	Trench	N	2	8	16	3
11/04/2017	Osborn Ave	Muswellbrook	Urban		Int Ruth White Ave	Trench	N	2	10	20	3
15/11/2016	Overton Road		Rural	LHS	705	Pavement failure	N	2	2	4	3
4/04/2016	Palace Street	Denman	Urban		4	Pavement Failure	N	2	2	4	3
15/03/2017	Palace Street	Denman	Urban		7-33	Trenches x 5?	N	8	10	80	3
16/10/2013	Pamger Drive		Rural	RHS	57	Pavement Failure	N	2	2	4	3
11/01/2016	Pamger Drive		Rural	LHS	1545	Pavement Failure	N	3	3	9	3
20/05/2016	Paterson Pl	Muswellbrook	Urban		Int Wollombi Road	Pavement Failure	N	3	3	9	3
4/04/2016	Paxton St	Denman	Urban		23	Pavement Failure	N	3	3	9	3
15/03/2017	Paxton St	Denman	Urban		64	Trench	N	4	12	48	3
19/02/2018	Paxton St	Denman	Urban		26	Pavement failure	N	2	2	4	3
19/02/2018	Paxton St	Denman	Urban		Cnr Crinoline	Pavement failure	Y	2	5	10	3
19/02/2018	Paxton St	Denman	Urban		School	Trench	N	2	5	10	3
19/02/2018	Paxton St	Denman	Urban		opp 49	Pavement failure	N	3	3	9	3
19/02/2018	Paxton St	Denman	Urban		68	Trench	N	2	12	24	3
10/01/2020	Paxton St	Denman	Urban		71	Pavement failure	N	3	3	9	3
10/01/2020	Paxton St	Denman	Urban		43	Pavement failure	N	2	5	10	3
10/01/2020	Paxton St	Denman	Urban		9	Pavement failure	N	2	5	10	3
2/12/2014	Queen Street	Muswellbrook	Urban		52	Pavement Failure	N	3	10	30	3
2/12/2014	Queen Street	Muswellbrook	Urban		53	Trench	N	2	8	16	3
30/03/2016	Queen Street	Muswellbrook	Urban		65	Pavement Failure	N	2	3	6	3
30/03/2016	Queen Street	Muswellbrook	Urban		Corner Cook Street	Trench	N	3	12	36	3

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
30/03/2016	Queen Street	Muswellbrook	Urban		18	Pavement Failure	N	3	5	15	3
20/03/2017	Queen Street	Muswellbrook	Urban		81 King St	2 x Pavement Failures	N	4	4	16	3
20/03/2017	Queen Street	Muswellbrook	Urban		Cnr King	Trench	N	2	7	14	3
20/03/2017	Queen Street	Muswellbrook	Urban		82 King	Trench	N	2	7	14	3
20/03/2017	Queen Street	Muswellbrook	Urban		Int Bowman St	Pavement failure	N	3	9	27	3
20/03/2017	Queen Street	Muswellbrook	Urban		74	Pavement failure	N	2	2	4	3
4/04/2018	Queen Street	Muswellbrook	Urban		10	Pavement failure	N	7	70	490	3
14/01/2015	Racecourse Road	Muswellbrook	Urban		11	Trench	N	2	6	12	3
11/01/2017	Reedy Creek Road		Rural	RHS	755	Pavement failure	N	2	5	10	3
11/01/2017	Reedy Creek Road		Rural	LHS	1145	Pavement failure	N	2	5	10	3
	2012 Richmond Grove Road		Rural	F/W	0-20	Pavement Failure	N	8	20	160	3
15/01/2014	Richmond Grove Road		Rural	RHS	1710	Pavement Failure	N	2	2	4	3
15/01/2014	Richmond Grove Road		Rural	RHS	2047	Pavement Failure	N	2	2	4	3
15/01/2014	Richmond Grove Road		Rural	LHS	2395	Pavement Failure	N	2	2	4	3
22/10/2014	Richmond Grove Road		Rural	RHS	2780	Pavement Failure	N	2	2	4	3
25/02/2016	Richmond Grove Road		Rural	RHS	2582	Pavement Failure	N	2	2	4	3
5/04/2017	Richmond Street	Muswellbrook	Urban		Int Bligh St	Trench	N	2	9	18	3
4/01/2018	Ridgelands Road		Rural	RHS	100	Pavement failure	N	2	25	50	3
10/02/2016	Ridgelands Road		Rural	F/W	1090	Trench	N	2	7	14	3
26/09/2014	Ridgelands Road		Rural	F/W	1720	Pavement Failure	N	6	2	12	3
10/02/2016	Ridgelands Road		Rural	RHS	2600	Pavement Failure	N	2	2	4	3
26/09/2014	Ridgelands Road		Rural	LHS	3092	Pavement Failure	N	2	3	6	3
10/02/2016	Ridgelands Road		Rural	LHS	4228	Pavement Failure	N	3	3	9	3
10/02/2016	Ridgelands Road		Rural	RHS	4390	Pavement Failure	N	2	2	4	3
26/09/2014	Ridgelands Road		Rural	RHS	6413	Pavement Failure	N	4	4	16	3
	2012 Ridgelands Road		Rural	LHS	7906	Pavement Failure	N	2.5	2.5	6.25	3
4/01/2018	Ridgelands Road		Rural	LHS	8010	Pavement failure	N	2	2	4	3
10/02/2016	Ridgelands Road		Rural	LHS	9000	Pavement Failure	N	3	3	9	3
10/02/2016	Ridgelands Road		Rural	RHS	9180	Pavement Failure	N	3	3	9	3
15/11/2016	Rosebrook Lane		Rural	F/W	540	Pavement Failure	N	6	30	180	3
15/11/2016	Rosebrook Lane		Rural	LHS	605	Pavement Failure	N	3	2	6	3
25/01/2016	Rosemount Road		Rural	RHS	139	Pavement Failure	N	2	3	6	3
22/01/2018	Rosemount Road		Rural	LHS	2660	Pavement Failure	N	2	3	6	3
22/01/2018	Rosemount Road		Rural	RHS	2660	Pavement Failure	N	2	3	6	3
22/01/2018	Rosemount Road		Rural	LHS	2930	Pavement Failure	N	2	2	4	3
22/01/2018	Rosemount Road		Rural	RHS	3100	Pavement Failure	N	2	3	6	3
25/01/2016	Rosemount Road		Rural	F/W	3140	Trench	N	2	8	16	3
22/01/2018	Rosemount Road		Rural	CENTRE	3290	Pavement Failure	N	4	4	16	3
22/01/2018	Rosemount Road		Rural		3300	Pavement Failure	N	2	20	40	3
22/01/2018	Rosemount Road		Rural		3340	Pavement Failure	N	2	20	40	3
22/01/2018	Rosemount Road		Rural		3900	Pavement Failure	N	2	60	120	3
22/01/2018	Rosemount Road		Rural		8075	Pavement Failure	N	2	20	40	3
22/01/2018	Rosemount Road		Rural	RHS	8260	Pavement Failure	N	2	3	6	3
22/01/2018	Rosemount Road		Rural	LHS	8345	Pavement Failure	N	2	20	40	3
22/01/2018	Rosemount Road		Rural	RHS	8345	Pavement Failure	N	2	100	200	3
22/01/2018	Rosemount Road		Rural	RHS	8600	Pavement Failure	N	2	5	10	3
22/01/2018	Rosemount Road		Rural	RHS	8650	Pavement Failure	N	2	10	20	3
22/01/2018	Rosemount Road		Rural	RHS	8850	Pavement Failure	N	2	60	120	3
	2012 Rosemount Road		Rural	LHS	9579	Pavement Failure	N	2.5	10	25	3
	2012 Rosemount Road		Rural	RHS	9637	Pavement Failure	N	7	15	105	3
22/01/2018	Rosemount Road		Rural	RHS	9750	Pavement Failure	N	2	20	40	3
	2012 Rosemount Road		Rural	LHS	9761	Pavement Failure	N	2.5	25	62.5	3
	2012 Rosemount Road		Rural	RHS	9869	Pavement Failure	N	3	15	45	3
1/10/2014	Rosemount Road		Rural	RHS	13890	Pavement Failure	N	2	10	20	3
	2012 Roxburgh Road		Rural	RHS	620	Pavement Failure	N	3	3	9	3

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
25/01/2016	Roxburgh Road		Rural		4250	Trench	N	2.5	5	12.5	3
25/01/2016	Roxburgh Road		Rural	LHS	980	Pavement Failure	N	2	5	10	3
20/12/2017	Roxburgh Road		Rural	F/W	2570	Pavement Failure	N	4	10	40	3
25/01/2016	Roxburgh Road		Rural	LHS	6950	Pavement Failure	N	3	3	9	3
20/12/2017	Roxburgh Road		Rural	LHS	6510	Pavement Failure	N	3	3	9	3
20/12/2017	Roxburgh Road		Rural	F/W	7190	Trench	N	4	2	8	3
20/12/2017	Roxburgh Road		Rural	LHS	7630	Pavement Failure	N	3	2	6	3
14/10/2013	Ruth White Avenue	Muswellbrook	Urban		Sanray Crescent	Trench	N	8	2.5	20	3
20/01/2015	Ruth White Avenue	Muswellbrook	Urban		21	Pavement Failure	N	3	3	9	3
20/01/2015	Ruth White Avenue	Muswellbrook	Urban		Cnr Thompson	Pavement Failure	N	2	10	20	3
19/05/2016	Ruth White Avenue	Muswellbrook	Urban		Int Thompson	Trench	N	15	2	30	3
19/05/2016	Ruth White Avenue	Muswellbrook	Urban		5	Trench/Subsidence	N	8	2	16	3
19/05/2016	Ruth White Avenue	Muswellbrook	Urban		7	Trench	N	8	2	16	3
19/05/2016	Ruth White Avenue	Muswellbrook	Urban		11	2 x trenches across road	N	16	2	32	3
19/05/2016	Ruth White Avenue	Muswellbrook	Urban		16	Trench	N	8	2	16	3
19/05/2016	Ruth White Avenue	Muswellbrook	Urban		29	Trench	N	8	2	16	3
11/04/2017	Ruth White Avenue	Muswellbrook	Urban		45	Pavement failure	N	3	3	9	3
18/04/2018	Ruth White Avenue	Muswellbrook	Urban		49	Pavement failure	N	2	3	6	3
28/01/2020	Rutherford Road	Muswellbrook	Urban		Indoor Sports Centre	Failed Patch	Y	3	40	120	3
12/04/2017	Rutherford Road	Muswellbrook	Urban		Entry to ISC Carpark	Pavement Failure	N	3	3	9	3
1/05/2019	Rutherford Road	Muswellbrook	Urban		cnr Ironbark	trench	N	2	7	14	3
17/11/2016	Sandy Creek Road		Rural	RHS	6280	Pavement Failure	N	2	100	200	3
17/11/2016	Sandy Creek Road		Rural	RHS	6810	Pavement Failure	N	2	50	100	3
17/11/2016	Sandy Creek Road		Rural	CENTRE	7120	Pavement Failure	N	2	10	20	3
17/11/2016	Sandy Creek Road		Rural	LHS	7510	Pavement Failure	N	3	20	60	3
17/11/2016	Sandy Creek Road		Rural	CENTRE	8820	Pavement Failure	N	2	10	20	3
17/11/2016	Sandy Creek Road		Rural	RHS	10960	Pavement Failure	N	2	5	10	3
17/11/2016	Sandy Creek Road		Rural	RHS	11245	Pavement Failure	N	2	10	20	3
14/11/2013	Sandy Creek Road		Rural	RHS	11350	Pavement Failure	N	2	10	20	3
17/11/2016	Sandy Creek Road		Rural	RHS	11460	Pavement Failure	N	2	5	10	3
2012	Sandy Creek Road		Rural	RHS	11570	Pavement Failure	N	2.5	10	25	3
15/01/2015	Sanray Cr	Muswellbrook	Urban		4	Pavement Failure	N	3	2	6	3
23/12/2014	Scott St	Muswellbrook	Urban		10	Trench	N	2	10	20	3
23/12/2014	Scott St	Muswellbrook	Urban		14	Pavement Failure	N	2	2	4	3
23/12/2014	Scott St	Muswellbrook	Urban		15	Trench	N	2	10	20	3
23/12/2014	Scott St	Muswellbrook	Urban		17	Trench	N	2	10	20	3
23/12/2014	Scott St	Muswellbrook	Urban		18	Pavement Failure	N	2	2	4	3
7/03/2017	Scott St	Muswellbrook	Urban		5	Pavement Failure	N	2	2	4	3
12/02/2018	Scott St	Muswellbrook	Urban		51	Pavement Failure	N	2	2	4	3
7/03/2017	Scott St	Muswellbrook	Urban	F/W	43	Trench	N	2	10	20	3
7/03/2017	Scott St	Muswellbrook	Urban	F/W	29	Trench	N	2	10	20	3
7/03/2017	Scott St	Muswellbrook	Urban		25	Pavement Failure	N	2	2	4	3
7/03/2017	Scott St	Muswellbrook	Urban	F/W	16	trench	N	2	10	20	3
11/01/2016	Scrumlo Road		Rural	LHS	2050	Pavement Failure	N	2	2	4	3
11/01/2016	Scrumlo Road		Rural	LHS	2250	Pavement Failure	N	2	2	4	3
7/04/2016	Shaw Cr	Muswellbrook	Urban		15\17	Subsidence at new kerb	N	2	20	40	3
7/04/2016	Shaw Cr	Muswellbrook	Urban		1 (int Thompson)	Trench (W&W)	N	2	8	16	3
27/03/2018	Shiraz St	Muswellbrook	Urban		10\12	Pavement failure	N	3	5	15	3
20/03/2017	Shiraz St	Muswellbrook	Urban		83	Trench	N	2	10	20	3
20/03/2017	Shiraz St	Muswellbrook	Urban		81	Trench	N	2	10	20	3
20/03/2017	Shiraz St	Muswellbrook	Urban		44	Trench	N	2	11	22	3
20/03/2017	Shiraz St	Muswellbrook	Urban		19	Pavement failure	N	2	2	4	3
29/10/2019	Shiraz St	Muswellbrook	Urban		29\31	Pavement failure	N	2	10	20	3
12/02/2018	Silky Oak Cl	Muswellbrook	Urban		10	Pavement failure	N	2	20	40	3
12/01/2015	Skellatar St	Muswellbrook	Urban		Blooms	Cracking	N	3	20	60	3

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
7/04/2017	Skellatar St	Muswellbrook	Urban		1	Pavement failure	N	3	5	15	3
10/04/2017	Skellatar Stock Route	Muswellbrook	Urban		15	Pavement failure	N	3	3	9	3
4/02/2020	Skellatar Stock Route	Muswellbrook	Urban		Opp St James Primary School	Pavement failure in school bus stop	N	2	3	6	3
5/04/2017	Sowerby Ave	Muswellbrook	Urban		6	Pavement failure	N	2	10	20	3
5/04/2018	Sowerby Ave	Muswellbrook	Urban		18	Pavement failure	N	3	3	9	3
5/04/2018	Sowerby St	Muswellbrook	Urban		Int St Heliers SB	Pavement failure	N	4	4	16	3
5/04/2018	Sowerby St	Muswellbrook	Urban		Int St Heliers NB	Pavement failure	N	2	5	10	3
5/04/2018	Sowerby St	Muswellbrook	Urban		54	Trench	N	2	9	18	3
14/01/2016	St Heliers Road		Rural		780	Pavement Failure	N	7	20	140	3
17/11/2016	St Heliers Road		Rural	LHS	680	Pavement failure	N	3	3	9	3
18/04/2018	St James Cr	Muswellbrook	Urban		2	Pavement Failure	N	2	2	4	3
19/01/2016	Stair St	Kayuga	Rural			Pavement Failure	N	3	3	9	3
7/04/2017	Sydney Street	Muswellbrook	Urban		Sydney St Tyre Centre	Pavement Failure	Y	2	20	40	3
7/04/2017	Sydney Street	Muswellbrook	Urban		Upper Hunter Fire Protection	Pavement Failure	N	2	10	20	3
8/12/2014	Sylvana St	Muswellbrook	Urban		Various	2 x trenches across road	N	2	20	40	3
20/05/2016	Tarakan Ave	Muswellbrook	Urban		5	Trench (poorly restored water trench)	N	2	7	14	3
22/07/2016	Thomas Mitchell Drive		Rural	LHS	100	Pavement Failure	N	3	30	90	3
7/12/2017	Thomas Mitchell Drive		Rural	RHS	260	Pavement failure	N	2	5	10	3
7/12/2017	Thomas Mitchell Drive		Rural	RHS	500	Pavement failure	N	2	10	20	3
22/07/2016	Thomas Mitchell Drive		Rural	LHS	740	Pavement Failure	N	3	10	30	3
7/12/2017	Thomas Mitchell Drive		Rural	RHS	810	Pavement failure	N	2	5	10	3
22/07/2016	Thomas Mitchell Drive		Rural	LHS	990	Pavement Failure	N	3	10	30	3
7/12/2017	Thomas Mitchell Drive		Rural	RHS	1150	Pavement failure	N	2	5	10	3
7/12/2017	Thomas Mitchell Drive		Rural	LHS	1540	Pavement failure	N	2	5	10	3
22/07/2016	Thomas Mitchell Drive		Rural	RHS	1590	Pavement Failure	N	3	10	30	3
22/07/2016	Thomas Mitchell Drive		Rural	LHS	1920	Pavement Failure	N	2	10	20	3
	2012 Thompson St	Muswellbrook	Urban		Various	Several trenches across road					3
11/04/2017	Thompson St	Muswellbrook	Urban		44	Trench	N	5	7	35	3
11/04/2017	Thompson St	Muswellbrook	Urban		26	Trench	N	2	9	18	3
11/04/2017	Thompson St	Muswellbrook	Urban		22	Pavement failure	N	2	5	10	3
11/04/2017	Thompson St	Muswellbrook	Urban		29	Pavement failure	Y	2	10	20	3
11/04/2017	Thompson St	Muswellbrook	Urban		10	Pavement failure	Y	2	5	10	3
16/4818	Thompson St	Muswellbrook	Urban		31 to Ruth White Ave	Failed Patch at new kerb and gutter	N	2	90	180	3
11/04/2017	Tindale St	Muswellbrook	Urban		3	Pavement Failure	N	2	5	10	3
12/04/2017	Towarri St	Muswellbrook	Urban		24	Pavement failure	N	2	10	20	3
28/01/2020	Towarri St	Muswellbrook	Urban		32	Pavement failure	N	2	10	20	3
6/04/2016	Tuart Cl	Muswellbrook	Urban		7	Pavement Failure	N	2	2	4	3
6/04/2016	Tuart Cl	Muswellbrook	Urban		Int Hakea	Trench	N	2	8	16	3
	2012 Turanville Ave	Muswellbrook	Urban		22	Pavement Failure	N	2	2	4	3
17/04/2018	Turanville Ave	Muswellbrook	Urban		10	Pavement Failure	N	3	3	9	3
27/05/2016	Turanville Ave	Muswellbrook	Urban		7\11	Pavement Failure	N	2	20	40	3
5/04/2017	Turanville Ave	Muswellbrook	Urban		18	Pavement Failure	N	3	10	30	3
5/04/2017	Turanville Ave	Muswellbrook	Urban		26	Pavement Failure	N	3	5	15	3
17/04/2018	Turanville Ave	Muswellbrook	Urban		24	Pavement Failure	N	4	2	8	3
5/04/2017	Turanville Ave	Muswellbrook	Urban		Int Sowerby	Trench	N	2	8	16	3
17/04/2018	Turanville Ave	Muswellbrook	Urban		20	Pavement failure	N	2	2	4	3
17/04/2018	Turanville Ave	Muswellbrook	Urban		16	Pavement failure	N	2	2	4	3
9/01/2020	Turanville Ave	Muswellbrook	Urban		Sowerby to Bligh	Pavement Failure over watermain	Y	20	2	40	3
4/04/2016	Turner Street	Denman	Urban		Water Tank	Pavement Failure	N	3	30	90	3
4/04/2016	Turner Street	Denman	Urban		22	Pavement Failure		2	5	10	3
23/01/2017	Turnermans Road Nth		Rural	LHS	1090	Pavement Failure	N	2	5	10	3
7/04/2017	Upper William St	Muswellbrook	Urban		Int King St	Trench	N	2	17	34	3
8/01/2015	Victoria St	Muswellbrook	Urban		108	Trench x 2	N	6	5	30	3
8/01/2015	Victoria St	Muswellbrook	Urban		120	Trench x 3	N	2	15	30	3



## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
7/04/2017	Victoria St	Muswellbrook	Urban		114	Trench	N	2	5	10	3
9/01/2020	Victoria St	Muswellbrook	Urban		69	Cracking	N	3	2	6	3
	2012 Virginia St	Denman	Urban		9	Heavy Patch	Y	2	2	4	3
12/01/2015	Virginia St	Denman	Urban		9	Trench x 2	N	4	10	40	3
19/02/2018	Virginia St	Denman	Urban		39	Heavy Patch	Y	2	10	20	3
12/01/2015	Virginia St	Denman	Urban		Cnr Ogilvie Street	Trench x 2	N	4	12	48	3
4/04/2016	Virginia St	Denman	Urban		Works Depot	Failed patch (Water?)	N	3	3	9	3
4/04/2016	Virginia St	Denman	Urban		15	Failed patch (Water?)	N	2	2	4	3
15/03/2017	Virginia St	Denman	Urban		Works Depot	Trench	N	2	12	24	3
15/03/2017	Virginia St	Denman	Urban		55	Pavement failure	N	3	3	9	3
19/02/2018	Virginia St	Denman	Urban		16	Pavement failure	N	3	2	6	3
19/02/2018	Virginia St	Denman	Urban		31	Pavement failure	N	3	2	6	3
19/02/2018	Virginia St	Denman	Urban		38	Trench	N	2	10	20	3
19/02/2018	Virginia St	Denman	Urban		35	Failed patch	N	2	3	6	3
19/02/2018	Virginia St	Denman	Urban		93	Pavement failure	N	2	3	6	3
5/04/2017	Waratah Cl	Muswellbrook	Urban		1	Trench	N	2	8	16	3
24/02/2016	Widden Valley Road		Rural	LHS	10	Pavement Failure	N	3	3	9	3
15/03/2018	Widden Valley Road		Rural	RHS	1990	Pavement Failure	N	2	5	10	3
21/10/2019	Widden Valley Road		Rural	LHS	5240	Pavement Failure	N	2	20	40	3
15/03/2018	Widden Valley Road		Rural	LHS	5335	Pavement Failure	N	2	10	20	3
6/11/2014	Widden Valley Road		Rural	RHS	7538	Pavement Failure	N	2	5	10	3
	2012 Widden Valley Road		Rural	LHS	7662	Pavement Failure	N	2	10	20	3
7/02/2017	Widden Valley Road		Rural	F/W	7880	Pavement Failure	N	2	6	12	3
21/10/2019	Widden Valley Road		Rural	LHS	8610	Pavement Failure	N	2	2	4	3
	2012 Widden Valley Road		Rural	LHS	11060	Pavement Failure	N	2	5	10	3
	2012 Widden Valley Road		Rural	LHS	11094	Pavement Failure	N	2	5	10	3
	2012 Widden Valley Road		Rural	RHS	11102	Pavement Failure	N	2	5	10	3
7/02/2017	Widden Valley Road		Rural	RHS	11404	Pavement Failure	N	3	25	75	3
21/03/2018	Widden Valley Road		Rural	F/W	11655	Pavement Failure	N	5	6	30	3
21/03/2018	Widden Valley Road		Rural	LHS	11845	Pavement Failure	N	2	2	4	3
21/03/2018	Widden Valley Road		Rural	LHS	13150	Pavement Failure	N	3	3	9	3
21/03/2018	Widden Valley Road		Rural	LHS	20068	Pavement Failure	N	2	5	10	3
16/05/2013	Widden Valley Road		Rural	LHS	20530	Pavement Failure	N	2	2	4	3
16/05/2013	Widden Valley Road		Rural	LHS	20630	Pavement Failure	N	2	2	4	3
16/05/2013	Widden Valley Road		Rural	LHS	20690	Pavement Failure	N	2	2	4	3
21/03/2018	Widden Valley Road		Rural	LHS	22100	Pavement Failure	N	2	2	4	3
7/04/2017	Wilder St	Muswellbrook	Urban		7	Trench	N	2	5	10	3
6/03/2017	Wilkins Street	Muswellbrook	Urban		9	Pavement failure	N	3	3	9	3
6/03/2017	Wilkins Street	Muswellbrook	Urban		1	Pavement failure	N	3	3	9	3
6/03/2017	Wilkins Street	Muswellbrook	Urban		Lot 3	Pavement failure	N	2	2	4	3
6/03/2017	Wilkins Street	Muswellbrook	Urban		Lot 3	W&W Trench	N	2	10	20	3
6/03/2017	Wilkins Street	Muswellbrook	Urban		Int Ford St	Cracking	N	5	5	25	3
27/03/2018	Wilkins Street	Muswellbrook	Urban		Hunter Valley Print	Trench	N	2	12	24	3
7/04/2017	Wilkinson Ave	Muswellbrook	Urban		Boronia Building	Pavement failure	N	2	2	4	3
	2012 Wollombi Road	Muswellbrook	Urban		26	Trench 1/3 way across road	N	2	5	10	3
14/01/2015	Wollombi Road	Muswellbrook	Urban		10	Pavement failure	N	2	3	6	3
14/01/2015	Wollombi Road	Muswellbrook	Urban		Tamar Way	Pavement failure (Trench?)	N	2	3	6	3
20/05/2016	Wollombi Road	Muswellbrook	Urban		18	Trench	N	3	5	15	3
20/05/2016	Wollombi Road	Muswellbrook	Urban		32	Pavement failure	N	2	2	4	3
20/05/2016	Wollombi Road	Muswellbrook	Urban		38	Trench	N	10	2	20	3
11/02/2020	Wollombi Road	Muswellbrook	Urban		6	Pavement failure	N	2	2	4	3
18/04/2018	Wollombi Road	Muswellbrook	Urban		2 Patterson Place (123??)	Failed patch next to kerb and gutter	N	2	10	20	3
8/04/2013	Woodlands Ridge Road		Rural	RHS	1340	Pavement Failure	N	2	2	4	3
21/01/2015	Woollybutt Way	Muswellbrook	Urban		11	2 x trenches across road	N	4	8	32	3
5/04/2016	Woollybutt Way	Muswellbrook	Urban		Aldi (near Rutherford Road)	Pavement failure/Cracking	N	2	5	10	3

## Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
18/04/2018	Woollybutt Way	Muswellbrook	Urban		49	Trench (failed Ausgrid patching)	Y	2	9	18	3
16/01/2020	Woollybutt Way	Muswellbrook	Urban		61	Pavement failure	N	2	2	4	3
	2012 Wybong PO Road		Rural	LHS	626	Pavement Failure	N	2	10	20	3
	2012 Wybong PO Road		Rural	RHS	715	Pavement Failure	N	2	5	10	3
	2012 Wybong PO Road		Rural	LHS	4017	Pavement Failure	N	2	5	10	3
14/01/2014	Wybong PO Road		Rural		4025	Pavement Failure	N	2	50	100	3
14/01/2014	Wybong PO Road		Rural	F/W	4125	Pavement Failure	N	5	5	25	3
14/01/2014	Wybong PO Road		Rural	LHS	4150	Pavement Failure	N	2	2	4	3
14/01/2014	Wybong PO Road		Rural	F/W	4180	Pavement Failure	N	5	3	15	3
20/01/2016	Wybong Road		Rural	LHS	670	Pavement Failure	N	2	3	6	3
20/01/2016	Wybong Road		Rural	LHS	1895	Pavement Failure	N	2	2	4	3
20/01/2016	Wybong Road		Rural	RHS	2540	Pavement Failure	N	2	2	4	3
21/01/2016	Wybong Road		Rural		2905	Pavement Failure	N	2	5	10	3
	2012 Wybong Road		Rural	RHS	4550	Pavement Failure	N	2.5	150	375	3
	2012 Wybong Road		Rural	LHS	4565	Pavement Failure	N	2.5	25	62.5	3
8/01/2014	Wybong Road		Rural	RHS	6450	Pavement Failure	N	2	3	6	3
11/01/2017	Wybong Road		Rural	RHS	6610	Pavement Failure	N	2	25	50	3
	2012 Wybong Road		Rural	CENTRE	6645	Pavement Failure	N	2.5	20	50	3
	2012 Wybong Road		Rural	LHS	6850	Pavement Failure	N	2	2	4	3
8/01/2014	Wybong Road		Rural	RHS	6850	Pavement Failure	N	2	10	20	3
12/09/2014	Wybong Road		Rural	LHS	20050	Pavement Failure	N	2	20	40	3
21/01/2016	Wybong Road		Rural	LHS	20330	Pavement Failure	N	5	3	15	3
12/09/2014	Wybong Road		Rural	LHS	21030	Pavement Failure	N	2	3	6	3
21/01/2016	Wybong Road		Rural	RHS	21050	Pavement Failure	N	3	3	9	3
21/01/2016	Wybong Road		Rural	RHS	21270	Pavement Failure	N	2	70	140	3
12/09/2014	Wybong Road		Rural	LHS	21500	Pavement Failure	N	2	2	4	3
20/01/2016	Wybong Road		Rural	LHS	21570	Pavement Failure	N	2	20	40	3
11/01/2017	Wybong Road		Rural	LHS	21640	Pavement Failure	N	2	5	10	3
11/10/2012	Wybong Road		Rural	CENTRE	21650	Pavement Failure	N	2.5	5	12.5	3
20/01/2016	Wybong Road		Rural	LHS	22020	Pavement Failure	N	2	5	10	3
11/01/2018	Wybong Road		Rural	RHS	22180	Pavement Failure	N	2	2	4	3
12/09/2014	Wybong Road		Rural	LHS	22409	Pavement Failure	N	2	2	4	3
12/09/2014	Wybong Road		Rural	LHS	22530	Failed patch	N	2	3	6	3
20/01/2016	Wybong Road		Rural	LHS	22725	Pavement Failure	N	2	2	4	3
8/01/2014	Wybong Road		Rural	LHS	22775	Pavement Failure	N	2	2	4	3
11/01/2018	Wybong Road		Rural	RHS	22970	Pavement Failure	N	2	2	4	3
20/01/2016	Wybong Road		Rural	RHS	23090	Pavement Failure	N	2	10	20	3
8/01/2014	Wybong Road		Rural	RHS	23160	Pavement Failure	N	2	10	20	3
21/01/2016	Wybong Road		Rural	RHS	23200	Pavement Failure	N	2	90	180	3
12/09/2014	Wybong Road		Rural	RHS	23790	Pavement Failure	N	2	5	10	3
12/09/2014	Wybong Road		Rural	LHS	24220	Pavement Failure	N	4	10	40	3
11/01/2018	Wybong Road		Rural	CENTRE	24900	Pavement Failure	N	2	2	4	3
11/01/2018	Wybong Road		Rural	CENTRE	25270	Pavement Failure	N	2	5	10	3
11/01/2017	Wybong Road		Rural	CENTRE	25400	Pavement Failure	N	3	3	9	3
11/01/2018	Wybong Road		Rural	RHS	30060	Pavement Failure	N	2	2	4	3
14/01/2014	Yarraman Road		Rural	LHS	250	Pavement Failure	N	2	2	4	3
14/01/2014	Yarraman Road		Rural	LHS	5060	Pavement Failure	N	3	5	15	3
1/10/2014	Yarraman Road		Rural	F/W	8112	Pavement Failure	N	3	5	15	3
1/10/2014	Yarraman Road		Rural	F/W	8266	Pavement Failure	N	2	5	10	3
19/01/2018	Yarrawa Deviation Road		Rural	LHS	3050	Pavement Failure	N	2	10	20	3
16/10/2014	Yarrawa Road		Rural	LHS	0	Pavement Failure	N	2	30	60	3
9/05/2013	Yarrawa Road		Rural	LHS	1305	Pavement Failure	N	3	3	9	3
8/11/2012	Yarrawa Road		Rural	LHS	1750	Pavement Failure	N	2.5	3	7.5	3
25/01/2016	Yarrawa Road		Rural	RHS	2100	Pavement Failure	N	2	20	40	3
9/05/2013	Yarrawa Road		Rural	RHS	2155	Pavement Failure	N	5	2.5	12.5	3

Attachment G

HEAVY PATCHING PROGRAM											
Date	Street/Road Name	Town Urban	Urban/Rural	LHS/RHS/FW	Location/Chainage	Defect	With K&G?	Width	Length	Area	Priority
25/01/2016	Yarrawa Road		Rural	LHS	2185	Pavement Failure	N	2	10	20	3
17/01/2017	Yarrawa Road		Rural	RHS	2210	Pavement Failure	N	2	2	4	3
10/12/2013	Yarrawa Road		Rural	RHS	2450	Pavement Failure	N	2	15	30	3
8/11/2012	Yarrawa Road		Rural	LHS	2556	Pavement Failure	N	2.5	3	7.5	3
17/01/2017	Yarrawa Road		Rural	LHS	3170	Pavement Failure	N	2	5	10	3
25/01/2016	Yarrawa Road		Rural	RHS	5060	Pavement Failure	N	2	20	40	3
17/01/2017	Yarrawa Road		Rural	RHS	9490	Pavement Failure	N	2	20	40	3
25/01/2016	Yarrawa Road		Rural	RHS	9520	Pavement Failure	N	2	10	20	3
25/01/2016	Yarrawa Road		Rural	LHS	9560	Pavement Failure	N	2	40	80	3
15/10/2014	Yarrawa Road		Rural	LHS	9850	Pavement Failure	N	2	5	10	3
15/10/2014	Yarrawa Road		Rural	RHS	9860	Pavement Failure	N	2	30	60	3
9/05/2013	Yarrawa Road		Rural	LHS	9970	Pavement Failure	N	5	2.5	12.5	3
15/10/2014	Yarrawa Road		Rural	RHS	10270	Pavement Failure	N	2	5	10	3
25/01/2016	Yarrawa Road		Rural	LHS	10285	Pavement Failure	N	2	10	20	3
19/01/2018	Yarrawa Road		Rural	F/W	10465	Pavement Failure	N	7	30	210	3
7/04/2016	Bell St	Muswellbrook	Urban		Cnr Victoria	Trench	N	10	2	20	4
1/12/2014	Queen Street	Muswellbrook	Urban		186	Pavement Failure	N	2	2	4	4
30/05/2016	Semilon St	Muswellbrook	Urban		Karoola Park	2 x trenches across road	N	4	10	40	4
24/05/2016	Victoria St	Muswellbrook	Urban		108	Pavement Failure	N	5	10	50	4

## Attachment H - Road Safety Devices

Date	Inspection No	Road name	Segment No	Chainage	Length	Comments	Priority	Completed
		Bylong Valley Way		37645	40	LHS New Guardrail	1	
		Bylong Valley Way		31810	40	New guard Rails on both sides	1	
		Bureen Road		11923	40	RHS New Guardrail	1	
		Bureen Road		12565	40	LHS New Guardrail	1	
		Bureen Road		12631	90	RHS New Guardrail	1	
		Bureen Road		12642	30	LHS New Guardrail	1	
		Bylong Valley Way		27950	20	New guard Rails on both sides	1	
31/10/2012	IN04397	Rosemount Road	RLROSER070	6200-6300	100	Guardrail RHS	1	
1/10/2014	IN04773	Rosemount Road	RLROSER080	7250-7670	120	Guardrail	1	
20/11/2019		Yarrawa Road	RLYARWR052	4490-4570	80	LHS New Guardrail just after Yarrawa Deviation Road	1	
1/10/2014	IN04776	Rosemount Road	RLROSER110	10000-10600	600	Possible guardrail LHS	1½	
31/10/2012	IN04397	Rosemount Road	RLROSER110	9790-9870	80	Possible site for guardrail - 3m drop either side of major culvert	1½	
12/10/2012	I004401	Yarraman Road	RLYARRR070-71	6160-6400	480	Possible guardrail either side of major culvert	1½	
8/11/2012	IN04402	Yarrawa Road	RLYARWR070	6480-6510	30	Possible guardrail LHS	1½	
8/11/2012	IN04402	Yarrawa Road	RLYARWR080	7000-7300	300	Possible guardrail RHS	1½	
8/11/2012	IN04402	Yarrawa Road	RLYARWR180	15900-16000	100	Possible guardrail RHS	1½	
10/12/2012	IN04387	Bylong Valley Way	RLBYLOW180	19000-19130	130	Right - possible site for guardrail	1½	

## Attachment I

## Car Park Inspections

Town	Name	Location	Area	Seal Type	K&G	Pavement	Linemarking	Median	Wheel Stops	TCD	Comments
Marketplace	Hill St	2556.1	Unknow	OK	Cracking, surfa needed		OK	1 x repair	Needs painting		
Muswellbrook	Brook Street Plaza	Brook St	883	Unknown	OK	In poor condition - part of CBD Redevelopment	Very poor	Nil	nil	Needs painting	
Muswellbrook	Campbells Corner	Brook St	1565	Unknown	Nil	In good condition	Very poor	Nil	nil	Needs painting	
Muswellbrook	From William St to IGA C/P	William St/Brook St	3866.6	AC14 mix	Nil	Minor cracking	good	Nil	nil	OK	
Muswellbrook	Karoola Park	Hastings St	2230.5	Unknown	OK	Weeds in a couple of parking spaces otherwise ok	OK	Nil	nil	nil	
Muswellbrook	Indoor Sports Centre	Rutherford Road	3891.4	14mm agg spray seal	OK	Patch failure 25 x 5	OK	OK	nil	nil	Trees removed from Median but stumps still there
Muswellbrook	Highbrook Playground	Ironbark Road	1017	Concrete	Good	Good condition	Very poor	Nil	Good	nil	Reflective strips needed on wheel stops
Muswellbrook	Highbrook lower carpark	Ironbark Road	2003	7mm agg spray seal	Nil	Minor cracking	good	nil	nil	nil	Scour next to driveway from Ironbark
Muswellbrook	Highbrook upper carpark	Ironbark Road	2123.4	Unknown	Nil	Good condition	Poor	Nil	nil	nil	Scour next to entrance driveway
Muswellbrook	Bowman Park	Skellatar St	358.5	Concrete	OK	Concrete - good	Very poor	Nil	nil	nil	
Muswellbrook	Aquatic Centre	Wilkinson Ave	2394.7	AC14 mix	OK	some cracking	Very poor	OK	nil	nil	Signage needs refreshing and realigning
Muswellbrook	Tennis Club	Wilkinson Ave			Good	New carpark	good	Nil	Good	Nil	
Muswellbrook	Olympic Park	Wilkinson Ave			Good	New carpark	good	Nil	Good	nil	
Muswellbrook	Works Depot Carpark	Carramere Rd	550	Unknown	OK	Some cracking in approach road, carpark in good condition	good	OK	nil	nil	Not inspected
Muswellbrook	Victoria Park	Hill St	1011	Unknown	Nil	Good condition	Very good	Nil	nil	nil	
Muswellbrook	Stan Thies Centre	Hill St	985	Unknown	Nil	Good condition	Very good	Nil	Good	Nil	
Muswellbrook	Admin Centre - Employee	Maitland St	1824.8	Unknown	OK	OK Condition	Poor	Nil	nil	nil	Edge repairs needed at lead in road
Muswellbrook	Admin Centre - Eucalypt	Eucalypt Ave	495.2	Unknown	OK	Some cracking	Poor	OK	nil	nil	
Muswellbrook	Admin Centre - Public	Maitland St	2224.2	Unknown	OK	Some cracking	good	OK	nil	nil	Stormwater pit needs repairs
Muswellbrook	Admin - HACC	Maitland St	504	Unknown	OK	Good condition	Poor	Nil	nil	nil	
Muswellbrook	Transport Interchange	Market St	2080	Unknown	OK	Couple of potholes, some cracking - poor condition	needed	OK	nil	nil	Rehab list?
Muswellbrook	Railway Carpark	Market St	686	Unknown	OK	Reasonable condition	needed	Nil	nil	nil	
Muswellbrook	QEI Building	Market St	159	Gravel	Nil	Unsealed pavement	Not possible	Nil	nil	nil	
Muswellbrook	Market House	William St	850	AC10?	OK	Good condition	Poor	Nil	nil	nil	
Muswellbrook	Senior Citizens	Market Ln	489.3	AC14?	OK	Some cracking, some surface texture loss	Poor	Fair	nil	nil	
Muswellbrook	Art Gallery	William St	173.2	Concrete	Nil	Good condition	needed	Nil	nil	nil	Trim tree from exit sign
Muswellbrook	Court House to Solicitors	Brook St	2418.3	AC14 mix	OK	Cracking in the lane next to the solicitor's office	needed	Nil	nil	Needs painting	Replace pedestrian sign against court wall
Muswellbrook											
Muswellbrook	Rutherford Park	Scott St	300.3	Unknown	Nil	Pavement surface poor - W&W have repaired valves	nil	nil	nil	Needs painting	Area holds water when it rains
Muswellbrook	Brook St median to library	Brook St			OK	Some cracking	good		nil	nil	
Muswellbrook	Library	Hill St	4731	AC14 mix	Nil	Reasonable condition	needed	Good	nil	Needs painting	Burnt out bin has damaged surface
Muswellbrook	Education Institution	Hill St		AC14 mix	Good	Good condition	good	Good		nil	
Muswellbrook	Cemetery	Brecht St	565.6	14mm agg spray seal	OK	Reasonable condition	nil	OK	nil	nil	linemarking needed?
Muswellbrook	Weeraman Fields	Thiess Cr	1186.8	Unknown	OK	Some minor potholes.	needed	nil	nil	Needs painting	Speed hump signage missing, no stopping needs replacement
Muswellbrook	Golf Club (front carpark)	Bell St			Nil	Poor condition	needed	OK	nil	Nil	Rehab list?
Denman	Community Centre	Palace St	334.4	AC14 mix	OK	Some distortion to pavement surface, couple of potholes	OK	nil	nil	nil	Reseal?
Denman	Memorial Hall	Paxton St	641.4	Unknown	Nil	Potholes	Needed	nil	nil	nil	Small shed has been removed. Scope to expand the carpark?
											Some trees have been removed, old beds and stumps need to be removed/smoothed. HI Vis posts for remaining trees installed? Kerb and gutter needs to be removed and access for mobility issue people through to the pram ramp on opposite side plus a possible pedestrian crossing for safe access. Clean up leaf litter/debris in corner nearest golf tee off. Hold line needed for exit onto Turner St
Denman	Denman Pool	Ogilvie St	1744.3	Unknown	Minor damage	Good condition	hold line	OK	nil	Good	
Denman	Childcare Centre	Turner St	1751.5	Unknown	OK	Scours next to roadway	good	OK	Nil	needs painting	Road speed humps need painting
Denman	Denman Oval	Turner St			Nil	OK	Dots	nil	nil	Nil	
Denman	Golf Course	Turner St	820.6	Unknown	Nil	Good condition	good	nil	OK	needs painting	Road speed humps need painting
Denman	Roy Cawsey Park	SH27	1843.3	7mm agg spray seal	nil	Some distortion to pavement surface, couple of potholes	nil	nil	nil	Nil	Reseal?
Denman	Cemetery	Rosemount Rd	1147.3	Unknown	OK	Good condition	nil	OK	nil	nil	May need linemarking
Denman	Paxton to Palace	Palact St			OK	New Carpark	good	nil	OK	Good	

**11.7 LIQUID TRADE WASTE REVIEW FOR 2019-2020**

<b>Attachments:</b>	<b>A. Liquid Trade Waste Register as of June 2020</b>
<b>Responsible Officer:</b>	<b>Derek Finnigan - Director - Community Infrastructure</b>
<b>Author:</b>	<b>Irene Chetty - Operations &amp; Processing Engineer</b>
<b>Community Plan Issue:</b>	<b><i>A safe, secure and reliable water supply and sewerage services are provided to all residents that will ensure public health</i></b>
<b>Community Plan Goal:</b>	<b><i>Provide safe, secure, efficient and effective water, sewerage and waste services in compliance with regulatory requirements.</i></b>
<b>Community Plan Strategy:</b>	<b><i>Undertake inspections and approvals of liquid trade waste premises and agreements.</i></b>

**PURPOSE**

This report provides a brief review of the current status of the monitoring and management of Council's Liquid Trade Waste duties.

**OFFICER'S RECOMMENDATION**

**Council notes the information contained in the report.**

**Moved:** \_\_\_\_\_ **Seconded:** \_\_\_\_\_

**BACKGROUND**

Council's Water and Wastewater team is responsible for managing Liquid Trade Waste, that is, discharge of all liquid waste, other than sewage of a domestic nature, that it accepts into its sewerage network. This is done through the application and enforcement of Council's Liquid Trade Waste Discharge Policy.

Muswellbrook Shire Council's sewerage network accepts discharge from around 291 businesses, which in accordance with the *Liquid Trade Waste Regulation Guidelines* (2009) and Section 68 of the *Local Government Act 1993*, requires approval, monitoring and at least annual inspection by a Council Officer.

An effectively enforced Liquid Trade Waste Policy is one of the Best Practice criteria set out by the Water Directorate in the *Best Practice Management of Water Supply and Sewerage Guidelines 2007*, which allows Council to seek assistance under the Country Towns Water Supply and Sewerage Program.

Through the Upper Hunter Water Alliance, Council has engaged temporary assistance from Singleton Council's certified Liquid Trade Waste Officer to perform critical liquid trade waste tasks. Council is also in the process of training staff members in assisting with liquid trade waste duties.

With the commissioning of the new Recycled Water Treatment Works, emphasis is in place to protect this critically important public asset from unauthorised or high-strength sewer discharges.

**CONSULTATION**

This report was prepared in consultation with:

- Council's Water, Sewer and Works and Property & Building Staff,
- Singleton Council's Liquid Trade Waste Officer (also representing Upper Hunter Shire Council and Muswellbrook Council)



## CONSULTATION WITH COUNCILLOR SPOKESPERSON

A copy of this report has been forwarded to the Mayor, Councillor Rush, the Deputy Mayor, Councillor Scholes, and the Councillor Spokesperson for Infrastructure, Councillor Woodruff, for review.

### REPORT

Council's sewerage network accepts discharge from around 291 businesses which discharge Liquid Trade Waste (LTW) as listed in the current LTW register, each of which represents a risk to the new Recycled Water Treatment Works, Denman Sewer Treatment Plant and other elements of the sewerage network. These risks include:

- fats, oils and food waste solids from commercial cooking;
- oils and potential fuels from automotive industries;
- bacteria harmful to the treatment process from brewing;
- coolant and other chemicals from industrial processes; and
- modified acidity and other chemistry from all processes listed above.

In accordance with the *Liquid Trade Waste Regulation Guidelines (2009)* and Section 68 of the *Local Government Act (1993)*, the Recycled Water Treatment Works Environment Protection Licence EPL 1593 and Denman Sewer Environment Protection Licence EPL 5059, Council is required to regulate these discharges through:

1. Approvals.
2. Annual inspection of each premises annually **at minimum** (based on risk).
3. Assessment of Development Applications with liquid trade waste implications.

Council currently utilises Singleton Council's LTW Officer one day per week (approximately 40 days/year). The current state of LTW in Muswellbrook Shire is set out in Table 1, below. Details are in attachment 1.

Table 1 - Muswellbrook Liquid Trade Waste Register (LTW) in Authority at 25/06/2020

<b>Total Businesses recorded in the Liquid Trade Waste Register</b>	<b>291</b>
Closed or redundant LTW agreements	133
Completed Site Inspections & Approvals since Jan 20 – June 20	17
Development Applications as LTW dischargers	3
Businesses at Muswellbrook Market Place and Muswellbrook Fair – Agreement review is currently in progress in concurrence with Department of Planning, Industry and Environment	23
LTW businesses requiring site inspections	92
Old LTW Agreements requiring reviews and site inspections	23

**Note** that the time estimate for compliance inspections represents "business as usual" – this level of inspections is required to meet Council's obligations under the Act, Regulation, Guidelines and our own Policy.

### OPTIONS

This report is submitted to Council for information purposes.

### CONCLUSION

The report provides a summary of all businesses that are in Council's liquid trade waste register and provides a brief summary on what the current status is with regards to applications, approvals and inspections.

**SOCIAL IMPLICATIONS**

The report provides information in relation to Council's monitoring and approvals of liquid trade waste discharges into the sewerage system in Muswellbrook Shire and highlights Council's proactive approach towards maintaining and establishing statutory requirements of the Muswellbrook Environment Protection Licence EPL 1593, Denman Environment Protection Licence EPL 5059, and the *Protection of the Environment Operations Act 1997*.

**FINANCIAL IMPLICATIONS**

LTW fees and charges are already levied as part of Water and Sewer billing. Council may see an increase in revenue, raised as part of applications, approvals and site inspection fees, and repeated inspections if businesses are classified as high risk.

**POLICY IMPLICATIONS**

Council is seeking to fully enforce and support its Liquid Trade Waste Policy. Council's Liquid Trade Waste Policy is currently under review to ensure that the policy aligns with the Liquid Trade Waste Regulation Guidelines so that approvals and monitoring of liquid trade waste discharged into Council's sewerage system meets the NSW Framework for Regulation of Sewerage and Trade Waste.

**STATUTORY IMPLICATIONS**

Businesses or a person conducting a business or undertaking (PCBU) that wishes to discharge liquid trade waste into Council's sewerage system must, under *Section 68 of the Local Government Act 1993*, obtain approval from Council. It is an offence under *section 626 of the Act* to discharge liquid trade waste into Council's sewerage system without obtaining approval from Council.

The *Liquid Trade Waste Regulation Guidelines 2009*, which expand on this section of the Act, set out how Local Water Utilities are to achieve this.

NSW Best Practice Management of Water Supply and Sewerage Guidelines 2007 sets out the standards by which Local Water Utilities may be assessed for assistance, and the criteria for Utilities to pay dividends to their Councils. Appropriate LTW monitoring and enforcement is one of these criteria.

**LEGAL IMPLICATIONS**

Council must commit to ensuring that all business or PCBU that discharge liquid trade waste into Council's sewerage system must have a liquid trade waste agreement and that annual inspections are undertaken to mitigate potential risks to Council's sewerage reticulation system and the sewer treatment plants.

**OPERATIONAL PLAN IMPLICATIONS**

The submission of the report complies with item 20.01.04 and 20.01.05 of the Operational Plan – "Monitor high risk zones and treatment works within the sewer system exposed to businesses with liquid trade waste discharges" and "Undertake inspections and approvals of liquid trade waste premises and agreements".

**RISK MANAGEMENT IMPLICATIONS**

Liquid Trade Waste poses a risk to sewerage transport systems through:

- increased wear on pumps due to fats, oils and solids;
- damage to pipework and pump station structures due to changing acidity and formation of sulphuric acid; and
- increased blockages due to fatty build-up and deposit of rags, wet wipes and other solids in pipes.

It is a risk to the sewer treatment plants through:

- introduction of hazardous biological contaminants;

- altered chemistry of sewerage lowering the efficiency of treatment, therefore increasing cost;
- potentially hazardous chemicals such as PFAS or heavy metals, which accumulate in sludge; and
- damage to complex equipment and instrumentation.

Liquid Trade Waste can pose risks to the health and safety of the sewer network team, from flammable fuels discharged from service stations and industrial chemicals from other sites.

It is further a risk to Council's operations, in that breaches of the Environmental Protection Licences could result in significant penalties.

## Redundant LTW businesses

Liquid Trade Waste Register - Redundant Agreements or Closed Businesses				
Year	Number	Document Precs	Address	Description
2015	3	Muswell Wash	47C Maitland Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	58	AARON HENRY OPTOMETRIST	72-78 Brook Street MUSWELLBROOK NSW 2333	
2014	56	FARM FRESH MEATS	72-78 Brook Street MUSWELLBROOK NSW 2333	BUTCHERY
2014	55	SHAMROCK HOTEL	30 William Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2014	52	DENMAN PIE SHOP	28 Ogilvie Street DENMAN NSW 2328	OTHER BUSINESS
2014	51	Denman Cafe	20 Ogilvie Street DENMAN NSW 2328	TAKEAWAY
2014	49	Nigels Quality Meats	60-82 Bridge Street MUSWELLBROOK NSW 2333	BUTCHERY
2014	48	Coffee at Campbels Corner	60-82 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2014	46	Farmers Barn	1 Hunter Terrace MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	44	Bohemian Harmony	88 Hill Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	38	ROBERT'S MEATS	19-29 Rutherford Road MUSWELLBROOK NSW 2333	BUTCHERY
2014	34	KIMS BAKERY	72-78 Brook Street MUSWELLBROOK NSW 2333	TAKEAWAY
2014	32	Wendys	72-78 Brook Street MUSWELLBROOK NSW 2333	TAKEAWAY
2014	25	HEADLINE HAIR DESIGN	26 Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	24	CHEMISTWORKS	72-78 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	17	SHEAR INDULGENCE HAIR	30 Ogilvie Street DENMAN NSW 2328	OTHER BUSINESS
2014	16	Leahs Ooh La La	51 Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	12	JJ HAIR DESIGN & SPRAY	72-78 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	11	HUNTER SKIN & BEAUTY	54 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	9	MEGANS HIPPIY HOUSE	54 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	8	GARRY CUMMINS BARBER (SHOP 15)	Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	7	LA BELLA BEAUTY SALON	Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	6	RENEE JOHNSON HAIR SALON	Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2013	18	BRIDGE STREET CAFE	58 Bridge Street MUSWELLBROOK NSW 2333	
2012	19	HUNTER WINE SERVICES	Hunter Street MUSWELLBROOK NSW 2333	COMMERCIAL
2013	15	DENMAN PUBLIC SCHOOL	52 Paxton Street DENMAN NSW 2328	EDUCATION
2013	4	MILKWOOD TEA HOUSE	26 Sowerby Street MUSWELLBROOK NSW 2333	RESTAURANT
2012	27	SOUTHSIDE CARWASH - 45 MAITLAND STREET, MUSWELLBROOK	47A Maitland Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2012	26	MAD HATTERS CAFE	63 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2012	22	AUSGRID - 41 THOMAS MITCHELL DRIVE, MUSWELLBROOK	41 Thomas Mitchell Drive MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2012	19	HUNTER WINE SERVICES (AUSTRALIA) PTY LTD	Hunter Street MUSWELLBROOK NSW 2333	
2012	11	ARTISTE ESPRESSO BAR	1-3 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2012	9	CAROB CAFE	20 Ogilvie Street DENMAN NSW 2328	TAKEAWAY
2011	27	MOUNT PROVIDENCE HOSTEL	59 Tindale Street MUSWELLBROOK NSW 2333	HEALTH CARE
2011	26	MUSWELLBROOK HOTEL	46 Market Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2011	25	MUSWELLBROOK HOLDEN	116 Bridge Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2011	24	BRIDGE STREET BRASSERIE (NOAH'S)	91 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2011	23	PRINCE OF WALES HOTEL	36 Sydney Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2011	18	THE GRAPEVINE HOTEL - 1 OGILVIE STREET, DENMAN	1-3 Ogilvie Street DENMAN NSW 2328	ACCOMMODATION
2011	6	COLES MUSWELLBROOK - 19-29 RUTHERFORD ROAD, MUSWELLBROOK	19-29 Rutherford Road MUSWELLBROOK NSW 2333	OTHER BUSINESS
2011	13	KING'S GOURMET CHINESE RESTAURANT	29 Sydney Street MUSWELLBROOK NSW 2333	RESTAURANT
2004	126	Denman Childrens Centre	Turner Street DENMAN NSW 2328	OTHER BUSINESS
2004	124	Ogilvie's Cafe	25 Ogilvie Street DENMAN NSW 2328	TAKEAWAY
2004	123	Cobbler's Patisserie	21 Ogilvie Street DENMAN NSW 2328	TAKEAWAY
2004	122	Muswellbrook Public School	Roger Street MUSWELLBROOK NSW 2333	EDUCATION
2004	119	St James Primary School	111 Skellatar Stock Route MUSWELLBROOK NSW 2333	EDUCATION
2004	118	Verona Vineyard	75 Aberdeen Street MUSWELLBROOK NSW 2333	OTHER BUSINESS

## Redundant LTW businesses

2004	117	Eagle Boys Pizza	27-29 Maitland Street MUSWELLBROOK NSW 2333	TAKEAWAY
2004	116	Sharah's Family Store	19 Lorne Street MUSWELLBROOK NSW 2333	TAKEAWAY
2004	114	Bensons Hospital Shop	25 Brentwood Street MUSWELLBROOK NSW 2333	TAKEAWAY
2004	111	Pearly Shell Cafe	18 Bridge Street MUSWELLBROOK NSW 2333	TAKEAWAY
2004	110	Mr P D O'Neill - Dental Surgeon	6 Maitland Street MUSWELLBROOK NSW 2333	HEALTH CARE
2004	107	Tyre Power Muswellbrook	13 Maitland Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	106	P & R Quality Meats	57 Sydney Street MUSWELLBROOK NSW 2333	BUTCHERY
2004	105	Southside Car Wash	47C Maitland Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	101	Lowes Petroleum	45-51 Ford Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	100	Muswellbrook Veterinary Hospital	14 Aberdeen Street MUSWELLBROOK NSW 2333	VETERINARY CLINIC
2004	99	J GRAHAM SUTTON - DENTAL SURGEON	81 Sydney Street MUSWELLBROOK NSW 2333	ACCOMMODATION
2004	98	Paul O'Neill - Dental Surgeon	52 Bridge Street MUSWELLBROOK NSW 2333	HEALTH CARE
2004	97	Malcolm R Campbell - Dental Surgeon	80 Brook Street MUSWELLBROOK NSW 2333	HEALTH CARE
2004	96	TAFE	59 Maitland Street MUSWELLBROOK NSW 2333	EDUCATION
2004	95	Muswellbrook High School	Clendinning Street MUSWELLBROOK NSW 2333	EDUCATION
2004	91	MARKET STREET DRY CLEANERS	18 Market Street MUSWELLBROOK NSW 2333	CLEANING
2004	90	SHELL SERVICE STATION	148 Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	89	DENMAN DAPKOS	36-40 Ogilvie Street DENMAN NSW 2328	OTHER BUSINESS
2004	88	Inspirational Imaging	100-106 Bridge Street MUSWELLBROOK NSW 2333	PHOTOGRAPHY
2004	86	GARRY PASSFIELD PHOTOGRAPHY	16 Lorne Street MUSWELLBROOK NSW 2333	PHOTOGRAPHY
2004	85	Hi Exposure Portraits	49 Humphries Street MUSWELLBROOK NSW 2333	PHOTOGRAPHY
2004	84	Margins Bus Service Pty Ltd	7 Ogilvie Street DENMAN NSW 2328	OTHER BUSINESS
2004	83	Mount Providence Hostel	59 Tindale Street MUSWELLBROOK NSW 2333	HEALTH CARE
2004	82	Merton Court Hostel	91 Virginia Street DENMAN NSW 2328	HEALTH CARE
2004	80	Hunter Valley Printing	6 Wilkins Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	79	Muswellbrook Bowling Club	3 Wilkinson Avenue MUSWELLBROOK NSW 2333	CLUB
2004	78	Denman Print Works	23 Ogilvie Street DENMAN NSW 2328	OTHER BUSINESS
2004	77	G & JM James Automotive	47 Ogilvie Street DENMAN NSW 2328	AUTOMOTIVE/MECHANICAL
2004	76	White Memorial Swimming Pool	1 Wilkinson Avenue MUSWELLBROOK NSW 2333	SPORTS
2004	75	Denman Motor Inn	6 Crinoline Street DENMAN NSW 2328	ACCOMMODATION
2004	74	BP Brook Service Station	42-50 Sydney Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	73	Bogas Service Station	84-86 Maitland Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	71	Syers Smash Repairs	1 Industrial Close MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	68	Grant Fleming Mechanical Repairs	2-4 Common Road MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	66	JOHN SCOTT RESTORATIONS AND AUTOMOTIVE REPAIRS	12 Ogilvie Street DENMAN NSW 2328	AUTOMOTIVE/MECHANICAL
2004	65	Muswellbrook Laundrette	4 Market Lane MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	64	Denman Hotel	Ogilvie Street DENMAN NSW 2328	HOTEL/RESTAURANT
2004	63	Royal Hotel Denman	10 Ogilvie Street DENMAN NSW 2328	HOTEL/RESTAURANT
2004	62	MUSWELLBROOK HOTEL	46 Market Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2004	61	Shamrock Hotel	30 William Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2004	59	Muswellbrook Golf Club	Maitland Street MUSWELLBROOK NSW 2333	CLUB/RESTAURANT
2004	57	RJ Corbett	108 Sydney Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	55	Centabrook Motor Inn	105 Bridge Street MUSWELLBROOK NSW 2333	ACCOMMODATION
2004	54	Cross Country Ford	147/151 Bridge Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	53	Brook Bakery	24 Bridge Street MUSWELLBROOK NSW 2333	TAKEAWAY
2004	51	Thrifty Car Rentals	139-145 Maitland Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	48	Muswellbrook Motor Inn	24 Maitland Street MUSWELLBROOK NSW 2333	ACCOMMODATION
2004	46	Palatinos	142 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2004	45	John Hunter Motel	91-93 Maitland Street MUSWELLBROOK NSW 2333	ACCOMMODATION
2004	44	HEAWOODS POOL SERVICES	161 Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS

## Redundant LTW businesses

2004	43	Eatons Hotel	184 Bridge Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2004	42	Muswellbrook Marketplace	72-78 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	41	Heritage Tea House	208 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2004	40	The Wayfarer Motel	124 Maitland Street MUSWELLBROOK NSW 2333	ACCOMMODATION
2004	39	Valley Hotel Motel	33-39 Sydney Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2004	38	Pizza Power and Muswellbrook Seafood	168-170 Bridge Street MUSWELLBROOK NSW 2333	TAKEAWAY
2004	37	Brook Street Arcade	30 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	35	Mobile Service Station	Maitland Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	34	Campbells Corner	60-82 Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	60	Denman Bowling Club	46-48 Palace Street DENMAN NSW 2328	CLUB
2004	50	Denman Butchery	14 Ogilvie Street DENMAN NSW 2328	BUTCHERY
2004	31	Phillippe's Brasserie	155 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2004	30	Upper Hunter Holden	116 Bridge Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	29	Southtown Shopping Centre - BILO	Rutherford Road MUSWELLBROOK NSW 2333	COMMERCIAL
2004	28	Muswellbrook Smash Repairs - CLOSED	7 Common Road MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	26	SOVEREIGN INN MOTEL	62 Maitland Street MUSWELLBROOK NSW 2333	ACCOMMODATION
2004	25	Gidley-Baird & Associates	47 Maitland Street MUSWELLBROOK NSW 2333	VETERINARY CLINIC
2004	24	Hunter Valley Filters & The Chilli Man & BJ Mowers and Chainsaws	15 Industrial Close MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	23	Lucky Inn Chinese Restaurant	40 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2004	22	Carobs Cafe	20 Ogilvie Street DENMAN NSW 2328	TAKEAWAY
2004	21	KFC	129 Bridge Street MUSWELLBROOK NSW 2333	TAKEAWAY
2004	19	MUSWELLBROOK DISTRICT HOSPITAL	30 Brentwood Street MUSWELLBROOK NSW 2333	HEALTH CARE
2004	17	OAK WINE SERVICES	Hunter Street MUSWELLBROOK NSW 2333	COMMERCIAL
2004	16	Galvo's Tasty Tukka	56 Bridge Street MUSWELLBROOK NSW 2333	TAKEAWAY
2004	15	Michele's Cafe	16-17 Market Street MUSWELLBROOK NSW 2333	TAKEAWAY
2004	14	Kin's Garden Chinese Restaurant	29 Sydney Street MUSWELLBROOK NSW 2333	RESTAURANT
2004	13	Prince of Wales Tavern	28 Sydney Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2004	11	Muswellbrook Chinese Restaurant	55 Sydney Street MUSWELLBROOK NSW 2333	RESTAURANT
2004	10	Denman Cafe	16-18 Ogilvie Street DENMAN NSW 2328	TAKEAWAY
2004	8	Muswellbrook RSL Sub-branch Club	113 Bridge Street MUSWELLBROOK NSW 2333	CLUB
2004	7	Hub Coffee Lounge - CLOSED	88-96 Bridge Street MUSWELLBROOK NSW 2333	TAKEAWAY
2004	6	Denman RSL Sub-branch	2-4 Isobel Street DENMAN NSW 2328	CLUB
2004	5	Yvonne's Coffee Lounge and Takeaway	63 Bridge Street MUSWELLBROOK NSW 2333	TAKEAWAY
2004	2	Muswellbrook & District Workers Club Ltd	15 Sydney Street MUSWELLBROOK NSW 2333	CLUB
2004	3	BAYBROOK MOTOR INN & APARTMENTS	147-151 Maitland Street MUSWELLBROOK NSW 2333	ACCOMMODATION
2004	1	Denman Hospital	53 Ogilvie Street DENMAN NSW 2328	HEALTH CARE



Completed Inspections

Liquid Trade Waste Register - Completed Annual Site Inspections from Jan 2020 - June 2020				
Year	Number	Document Precis	Address	Description
2018	1	CRJ Maintenance Pty Ltd	1-3 Wallarah Road MUSWELLBROOK NSW 2333	
2015	5	AGGREKO GENERATOR RENTALS PTY LTD	19 Enterprise Crescent MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2014	59	COUNTRY NOODLES	60-82 Bridge Street MUSWELLBROOK NSW 2333	TAKEAWAY
2014	50	CAMPBELLS CORNER	60-82 Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	47	MUNCHIES	60-82 Bridge Street MUSWELLBROOK NSW 2333	TAKEAWAY
2014	4	LA LUNA CAFE	168-170 Bridge Street MUSWELLBROOK NSW 2333	TAKEAWAY
2013	13	SUPA IGA MUSWELLBROOK	60-82 Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2012	34	MUSWELLBROOK GOLF CLUB	Bell Street MUSWELLBROOK NSW 2333	CLUB/RESTAURANT
2012	33	LIHANGS GENERAL STORE	19 Lorne Street MUSWELLBROOK NSW 2333	TAKEAWAY
2012	32	MUSWELLBROOK RSL	113 Bridge Street MUSWELLBROOK NSW 2333	CLUB/RESTAURANT
2012	2	MCDONALDS MUSWELLBROOK	83-89 Maitland Street MUSWELLBROOK NSW 2333	RESTAURANT
2012	1	LUCKY INN CHINESE RESTAURANT	40 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2011	22	BENSONS HOSPITAL SHOP	25 Brentwood Street MUSWELLBROOK NSW 2333	TAKEAWAY
2011	16	HUNTER BELLE CHEESE	75 Aberdeen Street MUSWELLBROOK NSW 2333	RESTAURANT
2011	14	KFC MUSWELLBROOK	143-145 Maitland Street MUSWELLBROOK NSW 2333	RESTAURANT
2011	9	MUSWELLBROOK CHINESE RESTAURANT	55 Sydney Street MUSWELLBROOK NSW 2333	RESTAURANT
2011	5	ARTHURS	16-17 Market Street MUSWELLBROOK NSW 2333	TAKEAWAY

Liquid Trade Waste Register - Current Development Applications as of 25 June 2020				
Year	Number	Document Precs	Address	Description
2019	1	Service Station and Zambrero's Food Outlet	147 Bridge Street MUSWELLBROOK NSW 2333	
2017	11	Liquid Trade Waste - Classification B	22 Wallarah Road MUSWELLBROOK NSW 2333	
2012	28	THE ROYAL HOTEL	10-16 Bridge Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT

Liquid Trade Waste Register : Businesses at Muswellbrook Market Place and Muswellbrook Fair – Agreement review is currently in progress in concurrence with Department of Planning, Industry and Environment				
Year	Number	Document Precs	Address	Description
2014	63	SUBWAY MUSWELLBROOK FAIR	19-29 Rutherford Road MUSWELLBROOK NSW 2333	TAKEAWAY
2014	60	MUSWELLBROOK FLORIST & GIFTS	19-29 Rutherford Road MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	53	SOUTH SIDE CAR WASH - New record 3/2015 (Muswell Wash)	47C Maitland Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	42	TRISTAR MEDICAL GROUP	19-29 Rutherford Road MUSWELLBROOK NSW 2333	HEALTH CARE
2014	37	COLES SUPERMARKET	19-29 Rutherford Road MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	36	DOMINOS PIZZA	19-29 Rutherford Road MUSWELLBROOK NSW 2333	TAKEAWAY
2014	35	THE RITE BITE	19-29 Rutherford Road MUSWELLBROOK NSW 2333	TAKEAWAY
2014	33	NOODLE XPRESS	72-78 Brook Street MUSWELLBROOK NSW 2333	TAKEAWAY
2014	31	HUNTER KEBABS	72-78 Brook Street MUSWELLBROOK NSW 2333	TAKEAWAY
2014	30	BIG W	72-78 Brook Street MUSWELLBROOK NSW 2333	PHOTOGRAPHY
2014	29	WOOLWORTHS	72-78 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	28	SUBWAY - MARKETPLACE	72-78 Brook Street MUSWELLBROOK NSW 2333	TAKEAWAY
2014	21	OPEN HAIR CUT FOR MEN	55 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	20	OPEN CUT HAIR FOR WOMEN	55 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	18	EVOLVE HAIR & SPA	142 Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	10	RAZOR EDGE HAIR DESIGN	19-29 Rutherford Road MUSWELLBROOK NSW 2333	OTHER BUSINESS
2013	9	BLUE FLAME MODERN INDIAN CUISINE	142 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2012	24	MUSWELLBROOK MARKET PLACE	72-78 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2012	12	AMBULANCE SERVICE OF NSW	Market Street MUSWELLBROOK NSW 2333	CLEANING
2011	17	MUSWELLBROOK FAIR	19-29 Rutherford Road MUSWELLBROOK NSW 2333	OTHER BUSINESS
2011	4	WOOLWORTHS PETROL	72-78 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	109	MUSWELLBROOK AMBULANCE STATION	Market Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	108	KASHA'S COFFEE LOUNGE	Bridge Street MUSWELLBROOK NSW 2333	TAKEAWAY

Site Inspections to be done

Liquid Trade Waste Register - Active businesses with Existing Agreements that require site inspections				
Year	Number	Document Precis	Address	Description
2017	6	LANCASTER MOTORS	15-17 Rutherford Road MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2017	2	CALVARY AGED CARE	15 Cassidy Avenue MUSWELLBROOK NSW 2333	HEALTH CARE
2017	1	MUSWELLBROOK AMUSEMENT CENTRE	5 Mill Street MUSWELLBROOK NSW 2333	COMMERCIAL
2016	3	CribBreak Takeaway	13 Thomas Mitchell Drive MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2015	4	MUSWELLBROOK HOTEL	46 Market Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2015	2	COMPRINT SOLUTIONS	14 Strathmore Road MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	64	MUSWELLBROOK PRE-SCHOOL KINDERGARTEN INC.	25-27 Sowerby Street MUSWELLBROOK NSW 2333	EDUCATION
2014	62	DENMAN CHILDRENS CENTRE	Turner Street DENMAN NSW 2328	EDUCATION
2014	61	EYECARE PLUS	51 Bridge Street MUSWELLBROOK NSW 2333	HEALTH CARE
2014	57	HAPPY TOOTH MUSWELLBROOK	87 Bridge Street MUSWELLBROOK NSW 2333	HEALTH CARE
2014	54	DENMAN BOWLING CLUB	46-48 Palace Street DENMAN NSW 2328	CLUB
2014	45	MUSWELLBROOK OUT OF SCHOOL HOURS	24-26 Skellatar Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	43	SHERYNS BEAUTY SPOT	17 Ogilvie Street DENMAN NSW 2328	OTHER BUSINESS
2014	41	GOODSTART EARLY LEARNING	14-18 Brentwood Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	40	TYRE POWER	13 Maitland Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2014	39	BROOK MEDICAL CENTRE	62/68 Brook Street MUSWELLBROOK NSW 2333	HEALTH CARE
2014	26	WISHES HAIR BODY BEAUTY	31 Ogilvie Street DENMAN NSW 2328	OTHER BUSINESS
2014	23	COLOUR WORKS FOR HAIR	54-56 Sydney Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	19	Q HAIR & BEAUTY	15 Market Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	15	ALLURE HAIR & BEAUTY	36 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	5	MARKET STREET DRY CLEANERS	18 Market Street MUSWELLBROOK NSW 2333	CLEANING
2014	2	REMINGTON MUSWELLBROOK PTY LTD	28 Maitland Street MUSWELLBROOK NSW 2333	RESTAURANT
2014	1	GRAZE	25 Ogilvie Street DENMAN NSW 2328	RESTAURANT
2013	21	GRIIDA'S BUTCHERY	14 Ogilvie Street DENMAN NSW 2328	BUTCHERY
2013	20	GERARDS PLACE	16-18 Ogilvie Street DENMAN NSW 2328	RESTAURANT
2013	19	TARTUCA EXPRESSO BAR BISTRO	21 Ogilvie Street DENMAN NSW 2328	TAKEAWAY
2013	17	HUNTER INSTITUTE TAFE	59 Maitland Street MUSWELLBROOK NSW 2333	EDUCATION
2013	16	MATILDA SERVICE STATION	42-50 Sydney Street MUSWELLBROOK NSW 2333	TAKEAWAY
2013	14	BARN VETENARY SERVICES	127-133 Sydney Street MUSWELLBROOK NSW 2333	VETERINARY CLINIC
2013	12	HUNTER VALLEY PRINTING	6 Wilkins Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2013	11	CENTABROOK MOTOR INN	105 Bridge Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2013	10	DENMAN RSL SUB BRANCH CLUB	2-4 Isobel Street DENMAN NSW 2328	CLUB/RESTAURANT
2013	8	TILLYS PLAY AND DEVELOPMENT CENTRE	32 Bimbadeen Drive MUSWELLBROOK NSW 2333	EDUCATION
2013	7	DENMAN DISTRICT HEALTH SERVICES	53 Ogilvie Street DENMAN NSW 2328	HEALTH CARE
2013	6	MUSWELLBROOK DISTRICT HEALTH SERVICES	Bowman Street MUSWELLBROOK NSW 2333	HEALTH CARE
2013	5	CALTEX SERVICE STATION	84-86 Maitland Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2013	3	COLES EXPRESS PETROL STATION	148 Bridge Street MUSWELLBROOK NSW 2333	ACCOMMODATION
2013	2	THE DENMAN HOTEL	5 Ogilvie Street DENMAN NSW 2328	HOTEL/RESTAURANT
2013	1	THE GRAPEVINE MOTEL	1-3 Ogilvie Street DENMAN NSW 2328	OTHER BUSINESS
2012	31	PET MEDICAL	13 Aberdeen Street MUSWELLBROOK NSW 2333	VETERINARY CLINIC
2012	30	AUSGRID	41 Thomas Mitchell Drive MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2012	29	ROYAL HOTEL DENMAN	10 Ogilvie Street DENMAN NSW 2328	HOTEL/RESTAURANT
2012	25	MUSWELLBROOK SOUTH PUBLIC SCHOOL	57 Maitland Street MUSWELLBROOK NSW 2333	EDUCATION
2012	23	HERMITAGE MOTEL	62 Maitland Street MUSWELLBROOK NSW 2333	RESTAURANT
2012	21	UNITED MUSWELLBROOK	49-51 Maitland Street MUSWELLBROOK NSW 2333	TAKEAWAY
2012	20	AAH PIZZABELLA	27-29 Maitland Street MUSWELLBROOK NSW 2333	TAKEAWAY
2012	18	MUSWELLBROOK PUBLIC SCHOOL	3-9 Roger Street MUSWELLBROOK NSW 2333	EDUCATION

## Site Inspections to be done

2012	17	ST JAMES PRIMARY SCHOOL	111 Skellatar Stock Route MUSWELLBROOK NSW 2333	EDUCATION
2012	16	BEAUREPAIRES AND REG OSBORN PTY LTD	53-55 Maitland Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2012	15	FM MECHANICAL P/L	142-144 Sydney Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2012	14	WIDELAND MOTOR GROUP	113 Maitland Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2012	13	MUSWELLBROOK LAUNDRETTE	24 Bridge Street MUSWELLBROOK NSW 2333	CLEANING
2012	10	MUSWELLBROOK CHILDCARE CENTRE INC	24-26 Skellatar Street MUSWELLBROOK NSW 2333	EDUCATION
2012	8	G & JM JAMES	47 Ogilvie Street DENMAN NSW 2328	AUTOMOTIVE/MECHANICAL
2012	7	BAYBROOK MOTOR INN	147-151 Maitland Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2012	6	MUSWELLBROOK MOTOR INN	21 Maitland Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2012	5	MUSWELLBROOK AQUATIC CENTRE	1 Wilkinson Avenue MUSWELLBROOK NSW 2333	TAKEAWAY
2012	4	EATONS HOTEL	184 Bridge Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2012	3	MUSWELLBROOK HIGH SCHOOL	9-43 King Street MUSWELLBROOK NSW 2333	EDUCATION
2011	21	JOHN HUNTER MOTEL	91-93 Maitland Street MUSWELLBROOK NSW 2333	RESTAURANT
2011	20	DENMAN MOTOR INN	6 Crinoline Street DENMAN NSW 2328	OTHER BUSINESS
2011	19	TEA SHRINE	39 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2011	12	VALLEY HOTEL MOTEL	33-39 Sydney Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT
2011	11	DENMAN HOTEL	1-3 Ogilvie Street DENMAN NSW 2328	HOTEL/RESTAURANT
2011	10	MUSWELLBROOK NISSAN	108 Sydney Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2011	8	MUSWELLBROOK & DISTRICT WORKERS CLUB	15 Sydney Street MUSWELLBROOK NSW 2333	CLUB/RESTAURANT
2011	7	WAYFARER MOTEL	124 Maitland Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2011	3	MERTON COURT HOSTEL	91 Virginia Street DENMAN NSW 2328	HEALTH CARE
2011	2	HUNGRY JACKS	129-145 Bridge Street MUSWELLBROOK NSW 2333	RESTAURANT
2011	1	RELiance PETROLEUM - BP SERVICE STATION	139-141 Maitland Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	125	RADIANCE CLEANING SERVICES	29 Cousins Street MUSWELLBROOK NSW 2333	CLEANING
2004	121	DENMAN PUBLIC SCHOOL	52 Paxton Street DENMAN NSW 2328	EDUCATION
2004	120	ST JOSEPHS PRIMARY SCHOOL	80 Palace Street DENMAN NSW 2328	EDUCATION
2004	115	PINAROO LEISURE PARK	9080 New England Highway MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	104	BOGAS SERVICE STATION	12-20 Sydney Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	102	JOHN BELFIELD - DENTAL PROSTHETIST	Market Street MUSWELLBROOK NSW 2333	HEALTH CARE
2004	94	SOUTH MUSWELLBROOK PUBLIC SCHOOL	57 Maitland Street MUSWELLBROOK NSW 2333	EDUCATION
2004	93	TRIPLE J'S SUPERMARKET	29 Ogilvie Street DENMAN NSW 2328	OTHER BUSINESS
2004	92	IGA SUPERMARKET	44-46 Ogilvie Street DENMAN NSW 2328	COMMERCIAL
2004	87	VERLIE WEIDMAN VILLAGE	25 Brook Street MUSWELLBROOK NSW 2333	HEALTH CARE
2004	81	DENMAN RECREATION AREA (SWIMMING POOL AND INDOOR SPORTS CENTRE)	Virginia Street DENMAN NSW 2328	SPORTS
2004	70	GOODYEAR AUTO SERVICE CENTRE	27-29 Maitland Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	69	SYDNEY STREET TYRE AND BATTERY CENTRE	58-66 Sydney Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	67	R & A DUGGAN (NRMA DEPOT)	8 Ogilvie Street DENMAN NSW 2328	AUTOMOTIVE/MECHANICAL
2004	56	MUSWELLBROOK FUNERAL SERVICES	1 St Heliers Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2004	52	MUSWELLBROOK AUTOMOTIVE (Previously known as Zack's Automotive)	33 Market Lane MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	49	RED CEDAR MOTEL	12 Maitland Street MUSWELLBROOK NSW 2333	ACCOMMODATION
2004	47	Noah's in the Valley	91 Bridge Street MUSWELLBROOK NSW 2333	ACCOMMODATION
2004	36	CHAUVEL ARMY DEPOT	Francis Street MUSWELLBROOK NSW 2333	EDUCATION
2004	32	MUSWELLBROOK CITY TOYOTA	113 Maitland Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	9	PET MEDICAL (DENMAN)	24 Ogilvie Street DENMAN NSW 2328	VETERINARY CLINIC
2004	4	RAILWAY HOTEL	10-14 Market Street MUSWELLBROOK NSW 2333	HOTEL/RESTAURANT

Liquid Trade Waste Register - Old Agreements that require review and site inspections and renewal of agreements				
Year	Number	Document Precs	Address	Description
2017	10	LTW	5-7 Enterprise Crescent MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2017	9	Mine Maintenance and Fabrication	17 Thomas Mitchell Drive MUSWELLBROOK NSW 2333	
2017	8	Feels Good Pty Ltd	6-8 Wallarah Road MUSWELLBROOK NSW 2333	OTHER BUSINESS
2017	7	UPPER HUNTER APIARIES	24 Wallarah Road MUSWELLBROOK NSW 2333	OTHER BUSINESS
2017	4	QUALTEST LABORTARY (NSW) PTY LTD	9 Glen Munro Road MUSWELLBROOK NSW 2333	OTHER BUSINESS
2017	3	BUREAU VERTIS	31 Wallarah Road MUSWELLBROOK NSW 2333	OTHER BUSINESS
2016	5	22 Strathmore Road Muswellbrook - Sign Making - Workshop & Bus Storage	22 Strathmore Road MUSWELLBROOK NSW 2333	
2016	4	Scotty's Fabricate & Paint	13 Thomas Mitchell Drive MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2016	2	Northwest Mining Auto Electrical and Mechanical	15 Thomas Mitchell Drive MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2016	1	25 Strathmore Road, Muswellbrook	25 Strathmore Road MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2015	1	CUMMINS	14 Strathmore Road MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2014	65	CLASSIFICATION B - COMMERCIAL LIQUID TRADE WASTE APPLICATION	Thomas Mitchell Drive MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2014	27	LINDA'S BEAUTY THERAPY	23 Mitchell Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	22	DR RUSSELL KIFT	30 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	14	FOREVER BEAUTY	23 Sowerby Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	13	MUSWELLBROOK NAIL & BEAUTY	25 Maitland Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2014	3	CATHIES HAIR CTR (SHOP 11)	30 Brook Street MUSWELLBROOK NSW 2333	OTHER BUSINESS
2011	15	HUNTER VALLEY FILTER SALES	16 Common Road MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	103	Newton Bus Service	18-22 Kenilworth Street DENMAN NSW 2328	OTHER BUSINESS
2004	72	REG OSBORN PTY LTD	53-55 Maitland Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	58	MUSWELLBROOK GREYHOUND CLUB	Sydney Street MUSWELLBROOK NSW 2333	CLUB
2004	33	Bonds Auto Wreckers	7 Common Road MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL
2004	20	SOUTHSIDE MECHANICAL REPAIRS & MUSWELLBROOK TAXI SERVICE	47B Maitland Street MUSWELLBROOK NSW 2333	AUTOMOTIVE/MECHANICAL



## 11.8 REGIONAL TENDER MATTRESS RECYCLING

<b>Attachments:</b>	Nil
<b>Responsible Officer:</b>	Fiona Plesman - General Manager
<b>Author:</b>	Derek Finnigan - Deputy General Manager
<b>Community Plan Issue:</b>	<i>A safe, secure and reliable water supply and sewerage services are provided to all residents that will ensure public health</i>
<b>Community Plan Goal:</b>	<i>Provide safe, secure, efficient and effective water, sewerage and waste services in compliance with regulatory requirements.</i>
<b>Community Plan Strategy:</b>	<i>Tender for new waste and recoverables collection service.</i>

### PURPOSE

To Request Council's approval to participate in a Hunter Joint Organisation Regional Tender for Mattress Recycling.

### OFFICER'S RECOMMENDATION

Subject to Australian Consumer & Competition Commission approval, Council approves participating in the Hunter Joint Organisation Regional Tender for mattress recycling as detailed in the report.

Moved: \_\_\_\_\_ Seconded: \_\_\_\_\_

### BACKGROUND

Council receives old mattresses at its Muswellbrook Waste & Recycling Facility (MWRF). Council has been diverting waste mattresses from landfill since the success of an initial trial during 2013-14 where the Muswellbrook Men's Shed disassembled 181 mattresses from the MWRF over 12 months to recover the timber for their own projects, and returning the metals to the MWRF for recycling and the textiles for landfilling.

Council's mattress diversion from landfill since this time has been reliant upon the services of Muswellbrook Men's Shed. However, notably and increasingly during the bi-annual kerbside bulky waste collection events, the volume of mattresses received at the MWRF exceeds the capacity of Muswellbrook Men's Shed to disassemble. It is these mattresses that end up stockpiled for want of a larger scale recycling option.

Recent data in the following table shows the number of mattresses received to the MWRF over the past two years, along with the portion of these that the Muswellbrook Men's Shed disassembled over the same time period. Counts include an estimated portion collected during bulky waste events, based on the weight of loads;

<b>Mattresses collected &amp; processed</b>	<b>Number 2018-19</b>	<b>Number 2019-20</b>
Total received at MSC Waste & Recycling Centre	1050	1154
The portion disassembled by Muswellbrook Men's Shed	564	367

Keeping mattresses out of landfill has a multitude of benefits, as mattresses compact very poorly, and present risks to staff & equipment in the landfill due to the potential for springs to tangle around compaction machinery. The disproportionate space taken up in landfills by mattresses also wastes

valuable airspace. When recovered, however, the timber, steel and foam materials in mattresses all have high reuse potential.

Staff are committed to ensuring that all mattresses received are appropriately treated to realise full material recovery, rather than disposed of as landfill, or by stockpiling the mattresses, which is unsightly and can attract vermin. Therefore, while the presently utilised services of the Men's Shed may certainly continue within their capacity, Council requires a solution for the ever-increasing remainder in order to guarantee these reusable materials are responsibly recovered on a regular and ongoing basis.

## **CONSULTATION**

Procurement Coordinator

Waste Coordinator

Waste Officers of the Hunter Joint Organisation

Sustainability Officer

Muswellbrook Men's Shed

Deputy General Manager

## **CONSULTATION WITH COUNCILLOR SPOKESPERSON**

A copy of the report has been forwarded to the Mayor, Councillor Rush, the Deputy Mayor, Councillor Scholes, and the Councillor Spokesperson for Infrastructure, Councillor Woodruff, for review.

## **REPORT**

Staff are committed to managing waste mattresses in an environmentally sustainable manner that results in real material recovery. Staff note that the number of waste mattresses received at the Muswellbrook Waste & Recycling Facility (MWRF) has been increasing over time, and wish that all received mattresses be responsibly recycled.

The mattress recycling and processing market in NSW is presently constrained, with very few companies providing collection and processing services in this market. The lack of mattress collection service providers has been of concern to a number of Hunter Joint Organisation (HJO) member Councils. Council representatives from these Councils agree there would be significant benefits for Councils to enter into a regional contract with a longer contract term. Longer term contracts and access to the significant number of mattresses collectively on offer provide certainty to potential service providers, which in turn may:

- provide incentives for new service providers to establish a mattress processing facility in the Hunter Region and, as such, increase competition;
- reduce investment risks particularly for new service providers;
- ensure competitive tender prices;
- provide regional employment opportunities; and
- minimise material transportation requirement.

The issue impacting rural and lower population Councils of the HJO (including Muswellbrook, Dungog, Upper Hunter, and Singleton) when seeking waste services individually is the high proportionate cost of transport and logistics due to the large distance between collection facilities combined with smaller tonnages. These challenges significantly increase the cost of services. Benefits of participation in a regional waste tender include economy of scale and security of supply for service providers.

## **PARTICIPATING HUNTER JOINT ORGANISATION (HJO) MEMBER COUNCILS**

All of the HJO member Councils intend to participate in the proposed regional contract:

- Muswellbrook Shire Council;
- Maitland City Council;

- Cessnock City Council;
- Singleton Council;
- Lake Macquarie City Council;
- Port Stephens Council;
- Dungog Shire Council;
- Upper Hunter Shire Council;
- MidCoast Council; and
- Newcastle City Council

Participants intend to seek best value for money in this tender as well as a superior resource recovery outcome, resulting in maximum possible reuse of mattress components and avoidance of landfill.

#### SERVICE COMPONENTS OF PROPOSED CONTRACT:

The proposed regional contract will provide for the following components, as required by each participant council:

- Collection of Mattresses from Waste Management Centres;
- kerbside collection of mattresses (for those Councils that offer kerbside collection of mattresses);
- Provision of Mattress Muster campaigns (for Councils that offer this service); and
- Processing and recycling of collected mattresses

#### ACCC AUTHORISATION

Prior to inviting tenders for the regional contract, the Hunter Joint Organisation is required to seek authorisation to proceed from the Australian Consumer and Competition Commission (ACCC), as the proposed regional arrangement could be seen to affect competition. Officers from Maitland Council are pursuing this component pending all member Councils confirmation of participation.

#### PROPOSED CONTRACT TERM

The tender documents will provide for two Contract Term Options:

Option 1: Contract Term of five (5) years

Option 2: Contract Term of eight (8) years

It will be at the sole discretion of the individual Councils as to which Contract Term will be adopted.

#### CONTRACT EXECUTION CONDITIONAL ON REGIONAL APPROACH

The regional contract can only be executed if all participating Councils resolve to award the contract to the same tenderer.

This will be the subject of a future report, requesting Council's consideration of an approved tenderer.

#### CONTRACT ADMINISTRATION

It is proposed that each Council will sign the contract with the successful Contractor, and then administer their respective contracts individually. The Councils will still work closely on overarching and regional matters such as promotion of the service and Work, Health and Safety aspects of the contract.

#### ANTICIPATED TIME FRAMES

Tasks	Timeframes
Seek ACCC authorisation	August 2020
Invite tenders (subject to ACCC authorisation)	September 2020

---

Tasks	Timeframes
Contract Award	October 2020
Contract commencement	1 January 2021

The report was submitted to the MANEX meeting conducted Tuesday, 14 July 2020, where participation in the Regional Tender for mattress recycling was supported.

### OPTIONS

Council may choose not to participate in the Regional Tender for Mattress Recycling.

### CONCLUSION

Cessnock City Council, Dungog Shire Council, Lake Macquarie City Council, Maitland City Council, MidCoast Council, Muswellbrook Shire Council, Newcastle City Council, Port Stephens Council, Singleton Council and Upper Hunter Shire Council have cooperated and developed tender documentation for a regional contract for the collection and processing of mattresses. It is recommended that tenders be invited for this regional contract.

### SOCIAL IMPLICATIONS

It is understood that one of the existing mattress recycling service providers provides employment opportunity for people who experience barriers entering the open labour market. Historically, large scale recycling facilities in the Hunter region that require work by hand, have provided similar opportunity.

It is intended to maintain the current arrangement with Muswellbrook Men's Shed as a local service provider, as this enterprise has community social benefits. The establishment of a regional contract will complement this as it will insure that all mattresses end up recycled, either locally or regionally.

Additionally, a regional tender for mattress recycling may increase employment in the region.

### FINANCIAL IMPLICATIONS

The proposed regional contract is not expected to increase the current costs for mattress recycling. There is a strong possibility that the regional arrangement and the relatively long contract term provide for greater competition. There is no doubt that the proposed collective contract will attract more competitive tender rates than Muswellbrook Shire Council could achieve by inviting tenders individually.

### POLICY IMPLICATIONS

MSC01E Procurement Policy – item 5.1

### STATUTORY IMPLICATIONS

Local Government Act 1993, Section 355 of the Act provides that a function of Council may, subject to certain conditions, be exercised in a number of ways. For example, a Council may exercise its functions:

- jointly by the council and another council or councils, or
- By a delegate of the Council, which may be a voluntary regional organisations of councils of which the council is a member.

### LEGAL IMPLICATIONS

A regional tender as described is subject to Australian Consumer & Competition Commission approval.

**OPERATIONAL PLAN IMPLICATIONS**

The submission of the report complies with item 20.1.1 of the Operational Plan: 'Operate in accordance with New South Wales Environment Protection Agency (EPA) Licences'.

**RISK MANAGEMENT IMPLICATIONS**

A regional tender for mattress recycling would provide collection infrastructure (mobile cages), and the regular collection of mattresses will prevent the stockpiling of mattresses at the Muswellbrook Waste & Recycling Facility, reducing unsightliness and pests.

The current arrangements with Muswellbrook Men's Shed are insufficient to process the entire quantity of waste mattresses received. Stockpiles grow until Council can procure a hired shredding machine to reduce volume and then landfill. While the ongoing operation in this manner is possible, it is a costly exercise that does not facilitate landfill diversion.

## 12 CORPORATE AND COMMUNITY SERVICES

### 12.1 2019/20 ANNUAL FINANCIAL STATEMENTS

<b>Attachments:</b>	<b>Nil</b>
<b>Responsible Officer:</b>	<b>Fiona Plesman - General Manager</b>
<b>Author:</b>	<b>Natalia Cowley - Executive Manager - Office of the Chief Financial Officer</b>
<b>Community Plan Issue:</b>	<b><i>Progressive leadership</i></b>
<b>Community Plan Goal:</b>	<b><i>That Muswellbrook Shire is well led and managed</i></b>
<b>Community Plan Strategy:</b>	<b><i>A financially sustainable council</i></b>

#### PURPOSE

The 2019/20 Financial Reports are nearing completion and require a Council resolution in order to have them presented for audit in accordance with Section 413 of the Local Government Act.

#### OFFICER'S RECOMMENDATION

##### Council Authorise:

- 1. The submission of the 2019/20 General Purpose Financial Reports, Special Purpose Financial Reports and Special Schedules for Muswellbrook Shire Council to the Auditor; and**
- 2. The Mayor, one other Councillor, General Manager and Responsible Accounting Officer be authorised to sign the reports upon completion of the audit.**

**Moved:** \_\_\_\_\_ **Seconded:** \_\_\_\_\_

#### BACKGROUND

Section 413 (1) of the Local Government Act states that a council must prepare financial reports for each year and must refer them for audit as soon as practicable after the end of that year.

Section 416 (1) states that a council's financial reports for a year must be audited within the period of four months after the end of that year i.e. by 31 October.

#### CONSULTATION

Mayor, Rush

#### REPORT

Council's finance staff are working on the 2019/20 Financial Reports and seek authorisation for them to be submitted to the auditor and then signed once completed, including the receipt of the Auditor's Report.

Council's audit is scheduled for the period 21 September to 2 October 2020 with the aim of ensuring that the completed and audited statements can be submitted to the Office of Local Government by the due date of 31 October 2020. Council will be presented with a final audited set of accounts for adoption at the November 2020 Council meeting.



**OPTIONS**

This resolution is necessary in order to allow the finalisation of the 2019/20 Annual Financial Statements.

**SOCIAL IMPLICATIONS**

Not applicable.

**FINANCIAL IMPLICATIONS**

Not applicable.

**POLICY IMPLICATIONS**

Not applicable.

**STATUTORY IMPLICATIONS**

Section 413 of the Local Government Act provides for the referral of the draft financial statements for audit.

**LEGAL IMPLICATIONS**

Not applicable.

**OPERATIONAL PLAN IMPLICATIONS**

Section 2.2 – Provide high quality and timely financial reporting.

**RISK MANAGEMENT IMPLICATIONS**

Not applicable

.

**12.2 AUTHORISATION PURSUANT TO SECTION 226 OF THE LOCAL GOVERNMENT ACT 1993**

<b>Attachments:</b>	<b>Nil</b>
<b>Responsible Officer:</b>	<b>Fiona Plesman - General Manager</b>
<b>Author:</b>	<b>Joshua Brown - Manager - Integrated Planning &amp; Governance</b>
<b>Community Plan Issue:</b>	<b><i>Progressive leadership</i></b>
<b>Community Plan Goal:</b>	<b><i>That Muswellbrook Shire is well led and managed</i></b>
<b>Community Plan Strategy:</b>	<b><i>Effective governance of Muswellbrook Shire</i></b>

**PURPOSE**

To advise of the Mayor's exercise of his delegation under section 226(d) of the *Local Government Act 1993* (NSW).

**OFFICER'S RECOMMENDATION**

**Council note the information provided.**

**Moved:** \_\_\_\_\_ **Seconded:** \_\_\_\_\_

**BACKGROUND**

Section 226 (d) of the *Local Government Act 1993* provides for the Mayor to exercise, in cases of necessity, the policy-making functions of the governing body of the council between meetings of the council. Since the last ordinary meeting of the Council, the Mayor has exercised his delegation on three occasions.

**CONSULTATION**

General Manager

**REPORT**

The first authorisation was made on 30 June 2020 to adjust budget allocations as necessary to allow Council to fulfil the grant requirements under the Office of Sport – Local Sport Defibrillator Program; to approve the carrying over to 2020-21 of the funding from the Muswellbrook Mechanical Coursing Club Contribution ledger; and approve any unexpended funding from the Job Creation, Materials and Contracts ledger to be carried over to 2020-21. The reason for urgency is to ensure that purchase orders can be raised to purchase defibrillators for across Muswellbrook Shire sporting fields before 30 June 2020. It is noted that Council resolved on allocating funding to support the Local Sport Defibrillator Grant Programme and purchase of defibrillators at its Ordinary Council meeting on 26 November 2019. The next Ordinary Council meeting does not take place until 30 June 2020.

Owing to an administrative oversight, no report was prepared for the June Council meeting regarding the carrying over of funds from the Muswellbrook Mechanical Coursing Club Contribution and the Job Creation, Materials and Contracts.

**CONCLUSION**

It is recommended that Council notes the information provided in the report.

**12.3 POLICY REVIEW**

<b>Attachments:</b>	<b>A. Policy MSCxxx - Councillor meetings with Developers and Lobbyists Policy draft for exhibition</b> <b>B. Policy MSC00E - Governance Policy review for exhibition</b>
<b>Responsible Officer:</b>	<b>Fiona Plesman - General Manager</b>
<b>Author:</b>	<b>Joshua Brown - Manager - Integrated Planning &amp; Governance</b>
<b>Community Plan Issue:</b>	<b><i>Collaborative and responsive community leadership that meets the expectations and anticipates the needs of the community</i></b>
<b>Community Plan Goal:</b>	<b><i>Enhanced collaboration with Council's community and stakeholders to ensure Council and its elected arm is best placed to make decisions in the best interest of the community.</i></b>
<b>Community Plan Strategy:</b>	<b><i>Implement and maintain a diverse range of communication channels between Council and community stakeholders..</i></b>

**PURPOSE**

This report provides Councillors with details of a recommended amendment to an existing policy and a new policy initiative, which are recommended for endorsement for public exhibition.

**OFFICER'S RECOMMENDATION**

- 1. Council endorse the Draft Councillor Meeting with Property Developers and Lobbyists Policy and the endorsed revised Governance Policy on public exhibition for a period of not less than twenty-eight (28) days.**
- 2. Council adopt the Draft Councillor Meeting with Property Developers and Lobbyists Policy and the endorsed revised Governance Policy at the conclusion of the public exhibition period if no substantive changes are recommended in any submissions received.**

**Moved:** \_\_\_\_\_ **Seconded:** \_\_\_\_\_

**BACKGROUND**

The following report aligns with Council's commitment to reduce its governance risk and follows recommendations outlined in the Internal Auditor's report.

**CONSULTATION**

Each of the listed policies have been subject to public exhibition and have been discussed with relevant Council staff and MANEX.

**REPORT****Councillor Meeting with Property Developers and Lobbyists Policy**

The Councillor Meetings with Property Developers and Lobbyists Policy informs the community of the requirement for the disclosure of all meetings held between elected councillors and proponents of property development activities or their delegates, officers, employees or agents; and registered lobbyists advocating on behalf of third party interests. The Policy aims to uphold the principle that all councillors determining a Planning Application or other matter in Council's capacity as a consent or regulatory authority, must give such matters proper, genuine and realistic consideration and have access

to the same information to assist in the determination of that matter. Council is committed to a transparent and accountable process of development planning assessment and determination. This Policy strengthens the rigour of that transparency and accountability.

**Review of Governance Policy**

A review of the Governance Policy has been undertaken and two changes are recommended. The first is the delegation to the General Manager to set a corporate logo from time to time. The second is the inclusion of provisions governing the assessment and determination of planning applications made by Council, Councillors and Designated Staff Members.

Changes in the Governance Policy are marked up for the convenience of Councillors.

**OPTIONS**

Council may or may not endorse one or any of the policies recommended for adoption.

**CONCLUSION**

It is recommended that those policies be placed on public exhibition.

**SOCIAL IMPLICATIONS**

No known social implications.

**FINANCIAL IMPLICATIONS**

No known financial implications.

**POLICY IMPLICATIONS**

No known policy implications.

**STATUTORY IMPLICATIONS**

No known statutory implications.

**LEGAL IMPLICATIONS**

No known legal implications.

**OPERATIONAL PLAN IMPLICATIONS**

No known Operational Plan implications.

**RISK MANAGEMENT IMPLICATIONS**

No known risk management implications.



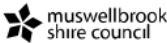
# muswellbrook shire council

## Councillor Meetings with Property Developers and Lobbyists Policy

MSCxxx

Draft for Exhibition

This document is a controlled document. Before using this document, check it is the latest version by referring to Council's EDRMS and ensuring you are using the Last Approved Version. Printed or downloaded versions of this document are uncontrolled.



# Table of Contents

Policy Context .....	3
Policy Objectives .....	3
Policy Principles .....	3
Definitions .....	3
Policy Statement .....	4
Associated Council Documentation .....	5
Authorisation Details .....	6





## Policy Context

The Councillor Meetings with Property Developers and Lobbyists Policy informs the community of the requirement for the disclosure of all meetings held between elected councillors and proponents of property development activities or their delegates, officers, employees or agents; and registered lobbyists advocating on behalf of third party interests. The Policy aims to uphold the principle that all councillors determining a Planning Application or other matter in Council's capacity as a consent or regulatory authority, must give such matters proper, genuine and realistic consideration and have access to the same information to assist in the determination of that matter.

Council is committed to a transparent and accountable process of development planning assessment and determination. This Policy strengthens the rigour of that transparency and accountability.

## Policy Objectives

The objectives of this Policy are to:

- A. Enhance Council's open, transparent and accountable assessment and determination of Planning Applications; and
- B. Establish a public register of all meetings between elected councillors and Property Developers; and Lobbyists.

## Policy Principles

The following principles set the foundation of this Policy:

- Council is transparent and accountable to its community and partners;
- Statutory, regulatory, policy and public interest considerations will always inform Council's decision-making processes; and
- The community must have confidence in Council's decision-making processes and that Council will exercise its statutory functions appropriately in the circumstances

## Definitions

**Lobbyist** means a third-party lobbyist, or any other individual or body that lobbies Muswellbrook Shire Council councillors and Council officials (including an individual engaged to undertake lobbying for a third-party lobbyist).

**Lobbyists Code of Conduct** means the NSW Lobbyist Code of Conduct pursuant to the *Lobbying of Government Officials (Lobbyists Code of Conduct) Regulation 2014*, as amended from time to time.

**Planning Application** means an application or request by a person (other than a public authority within the meaning of the Environmental Planning and Assessment Act 1979):

(a) to initiate the making of an environmental planning instrument or plan under that Act in relation to any development, project or activity on a particular site, or



(b) for consent to, or approval of, any development, project or activity under that Act or for the modification of any such consent or approval.

**Political Donation** has the meaning given to it by the *Electoral Funding Act 2018* (as amended)..

**Property Developer** means an individual or a corporation (including his, her or its delegates, officers, employees, agents or contractors) if--

(i) the individual or a corporation carries on a business mainly concerned with the residential or commercial development of land, with the ultimate purpose of the sale or lease of the land for profit, and

(ii) in the course of that business--

(A) 1 Planning Application has been made by or on behalf of the individual or corporation and is pending, or

(B) 3 or more Planning Applications made by or on behalf of the individual or corporation have been determined within the preceding 7 years, or

(iii) the individual or a corporation has lodged a Planning Application with Council, or has requested or undertaken a pre-lodgement meeting with Council officials with respect to a Planning Application for which no final determination has been made, or it is otherwise known that a Planning Application is likely to be lodged with Council.

## Policy Statement

### 1. Transparency of Council's decision-making governance processes

Councillors receive many requests from their constituents, industry leaders, Property Developers, objectors to developments and others for meetings and opportunities to discuss a broad range of issues. Each person making such a request expects to be listened to and will often seek to influence a decision of Council. A crucial principle of open democratic governance is that all participants have a right to have their say and that all views are given due consideration. It is equally important that the views of some not have more bearing on decision-making than those of all others. To fortify confidence in Council's decision-making, this Policy sets a standard requiring the disclosure of meetings held between Councillors and those who may be seen to exert undue influence on Councillors, including Property Developers and Lobbyists. This Policy should not be seen to impugn Property Developers or Lobbyists, but to provide a higher level of transparency to their dealings and maintain public confidence in Council's exercise of its functions as a planning and regulatory authority.

All Property Developers and Lobbyists seeking meetings with councillors must complete a Lobbyist and Property Developer Meeting Request Disclosure Form, which the General Manager will adopt and may amend from time to time. All meeting attendees must undertake to abide by the Lobbyists Code of Conduct.

All meetings with councillors must take place at Council's Administration Centre or at a location agreed to by the General Manager. The General Manager and/or their appointed member/s of staff must attend and record notes of the meeting, which must be stored in Council's Electronic Document and Records Management System<sup>1</sup>. A Councillor must neither meet with nor discuss any Planning Application in the absence of the General Manager and/or their appointed member/s of staff.

<sup>1</sup> Pursuant to Council's Records Management (Councillors) Policy and the *State Records Act 1998*



Council will publish on its website and in its Annual Report a register of all meetings with Councillors attended by Property Developers and Lobbyists, which will record the following:

- Date of meeting;
- Name of organisation(s);
- Name(s) of people in attendance and their position(s);
- Name of any lobbying firm personnel in attendance;
- Development Application reference (if applicable); and
- Details of all political donations made pursuant to State and Commonwealth law made by the organisation and/or its ten largest shareholders in the five (5) years prior to the meeting.

#### **Administrative arrangements**

The General Manager may prepare and amend from time to time procedures, guidelines, forms and templates to assist in the administration of this Policy.

#### **Associated Legislation and Regulations**

*Commonwealth Electoral Act 1918*

*Government Information (Public Access) Act 2009*

*Government Information (Public Access) Regulation 2018*

*Electoral Funding Act 2018*

*Environmental Planning and Assessment Act 1979*

*Environmental Planning and Assessment Regulation 2000*

*Local Government Act 1993*

*State Records Act 1998*

*Independent Commission Against Corruption Act 1988*

#### **Associated Council Documentation**

Governance Policy

Code of Conduct

Records Management Administrative Guidelines

Records Management (Councillors) Policy

## Authorisation Details

<b>Authorised by:</b>	Council
<b>Minute No:</b>	
<b>Date:</b>	
<b>Review timeframe:</b>	2 years
<b>Department:</b>	Integrated Planning, Risk and Governance
<b>Document Owner:</b>	Manager Integrated Planning, Risk and Governance

## Details History

Version No.	Date changed	Policy type	Modified by	Amendments made

# Councillor Meetings with Property Developers and Lobbyists Policy

## MSCxxx

Draft for Exhibition

# Table of Contents

Policy Context .....	3
Policy Objectives .....	3
Policy Principles .....	3
Definitions.....	3
Policy Statement .....	4
Associated Council Documentation .....	5
Authorisation Details .....	6



## Policy Context

The Councillor Meetings with Property Developers and Lobbyists Policy informs the community of the requirement for the disclosure of all meetings held between elected councillors and proponents of property development activities or their delegates, officers, employees or agents; and registered lobbyists advocating on behalf of third party interests. The Policy aims to uphold the principle that all councillors determining a Planning Application or other matter in Council's capacity as a consent or regulatory authority, must give such matters proper, genuine and realistic consideration and have access to the same information to assist in the determination of that matter.

Council is committed to a transparent and accountable process of development planning assessment and determination. This Policy strengthens the rigour of that transparency and accountability.

## Policy Objectives

The objectives of this Policy are to:

- A. Enhance Council's open, transparent and accountable assessment and determination of Planning Applications; and
- B. Establish a public register of all meetings between elected councillors and Property Developers; and Lobbyists.

## Policy Principles

The following principles set the foundation of this Policy:

- Council is transparent and accountable to its community and partners;
- Statutory, regulatory, policy and public interest considerations will always inform Council's decision-making processes; and
- The community must have confidence in Council's decision-making processes and that Council will exercise its statutory functions appropriately in the circumstances

## Definitions

**Lobbyist** means a third-party lobbyist, or any other individual or body that lobbies Muswellbrook Shire Council councillors and Council officials (including an individual engaged to undertake lobbying for a third-party lobbyist).

**Lobbyists Code of Conduct** means the NSW Lobbyist Code of Conduct pursuant to the *Lobbying of Government Officials (Lobbyists Code of Conduct) Regulation 2014*, as amended from time to time.

**Planning Application** means an application or request by a person (other than a public authority within the meaning of the Environmental Planning and Assessment Act 1979):

(a) to initiate the making of an environmental planning instrument or plan under that Act in relation to any development, project or activity on a particular site, or

(b) for consent to, or approval of, any development, project or activity under that Act or for the modification of any such consent or approval.

**Political Donation** has the meaning given to it by the *Electoral Funding Act 2018* (as amended)..

**Property Developer** means an individual or a corporation (including his, her or its delegates, officers, employees, agents or contractors) if--

(i) the individual or a corporation carries on a business mainly concerned with the residential or commercial development of land, with the ultimate purpose of the sale or lease of the land for profit, and

(ii) in the course of that business--

(A) 1 Planning Application has been made by or on behalf of the individual or corporation and is pending, or

(B) 3 or more Planning Applications made by or on behalf of the individual or corporation have been determined within the preceding 7 years, or

(iii) the individual or a corporation has lodged a Planning Application with Council, or has requested or undertaken a pre-lodgement meeting with Council officials with respect to a Planning Application for which no final determination has been made, or it is otherwise known that a Planning Application is likely to be lodged with Council.

## Policy Statement

### 1. Transparency of Council's decision-making governance processes

Councillors receive many requests from their constituents, industry leaders, Property Developers, objectors to developments and others for meetings and opportunities to discuss a broad range of issues. Each person making such a request expects to be listened to and will often seek to influence a decision of Council. A crucial principle of open democratic governance is that all participants have a right to have their say and that all views are given due consideration. It is equally important that the views of some not have more bearing on decision-making than those of all others. To fortify confidence in Council's decision-making, this Policy sets a standard requiring the disclosure of meetings held between Councillors and those who may be seen to exert undue influence on Councillors, including Property Developers and Lobbyists. This Policy should not be seen to impugn Property Developers or Lobbyists, but to provide a higher level of transparency to their dealings and maintain public confidence in Council's exercise of its functions as a planning and regulatory authority.

All Property Developers and Lobbyists seeking meetings with councillors must complete a Lobbyist and Property Developer Meeting Request Disclosure Form, which the General Manager will adopt and may amend from time to time. All meeting attendees must undertake to abide by the Lobbyists Code of Conduct.

All meetings with councillors must take place at Council's Administration Centre or at a location agreed to by the General Manager. The General Manager and/or their appointed member/s of staff must attend and record notes of the meeting, which must be stored in Council's Electronic Document

and Records Management System<sup>1</sup>. A Councillor must neither meet with nor discuss any Planning Application in the absence of the General Manager and/or their appointed member/s of staff.

Council will publish on its website and in its Annual Report a register of all meetings with Councillors attended by Property Developers and Lobbyists, which will record the following:

- Date of meeting;
- Name of organisation(s);
- Name(s) of people in attendance and their position(s);
- Name of any lobbying firm personnel in attendance;
- Development Application reference (if applicable); and
- Details of all political donations made pursuant to State and Commonwealth law made by the organisation and/or its ten largest shareholders in the five (5) years prior to the meeting.

### **Administrative arrangements**

The General Manager may prepare and amend from time to time procedures, guidelines, forms and templates to assist in the administration of this Policy.

---

<sup>1</sup> Pursuant to Council's Records Management (Councillors) Policy and the *State Records Act 1998*

## Associated Legislation and Regulations

*Commonwealth Electoral Act 1918*

*Government Information (Public Access) Act 2009*

*Government Information (Public Access) Regulation 2018*

*Electoral Funding Act 2018*

*Environmental Planning and Assessment Act 1979*

*Environmental Planning and Assessment Regulation 2000*

*Local Government Act 1993*

*State Records Act 1998*

*Independent Commission Against Corruption Act 1988*

## Associated Council Documentation

Governance Policy

Code of Conduct

Records Management Administrative Guidelines

Records Management (Councillors) Policy

## Authorisation Details

<b>Authorised by:</b>	Council
<b>Minute No:</b>	
<b>Date:</b>	
<b>Review timeframe:</b>	2 years
<b>Department:</b>	Integrated Planning, Risk and Governance
<b>Document Owner:</b>	Manager Integrated Planning, Risk and Governance

## Details History

<b>Version No.</b>	<b>Date changed</b>	<b>Policy type</b>	<b>Modified by</b>	<b>Amendments made</b>

## 12.4 AMENDMENT TO COUNCIL'S GOVERNANCE POLICY AND CODE OF CONDUCT

<b>Attachments:</b>	Nil
<b>Responsible Officer:</b>	Fiona Plesman - General Manager
<b>Author:</b>	Joshua Brown - Manager - Integrated Planning & Governance
<b>Community Plan Issue:</b>	<i>A Council that is well managed, efficient and properly resourced and that is responsive to its communities and stakeholders</i>
<b>Community Plan Goal:</b>	<i>Maintain a strong focus on financial discipline to enable Council to properly respond to the needs of the communities it serves.</i>
<b>Community Plan Strategy:</b>	<i>Appropriate matters are reported to Council in a timely manner in accordance with the Financial Control and Reporting Policy.</i>

### PURPOSE

To endorse amendments to the Governance Policy specifically the inclusion of a new section entitled “*Probity – lobbying by tenderers or prospective tenderers*” and the inclusion of a new clause in the Code of Conduct.

### OFFICER’S RECOMMENDATION

Council endorse the inclusion of a new section in the Governance Policy titled “**Probity – lobbying by tenderers or prospective tenderers**” and include in Council’s Code of Conduct a new provision which reads as follows: “**Councillors must comply with the adopted Governance Policy (as amended)**”.

Moved: \_\_\_\_\_ Seconded: \_\_\_\_\_

### BACKGROUND

The recommended amendments to the Governance Policy will require written notification by Councillors of complaints or lobbying in relation tenders and for that notification to include relevant particulars. That includes identification of the party that lobbied the Councillor, which deals with the issue of so-called “anonymous lobbying”. The amendments also impose an obligation on the General Manager to consider and act upon notifications of impermissible lobbying.

The amendment to the Code of Conduct links the Governance Policy with the Code of Conduct to ensure that noncompliance with Governance Policy can consequently be dealt under, the Code of Conduct.

### CONSULTATION

Council’s Corporate Lawyer

Manager Integrated Planning Governance and Risk

General Manager

### CONSULTATION WITH COUNCILLOR SPOKESPERSON

Mayor Rush



**REPORT**

The recommended amendment to the Governance Policy will provide a mechanism for dealing with complaints that are made by or on behalf of prospective tenderers. The focus of the new requirement is to direct such complaints received by either Councillors or Council Officers through either the Code of Conduct Procedures or otherwise Council's complaints handling policy.

Summary of the amendments:

- The definition of "Compliant Documents" captures complaints made under the Procedures (for code of conduct complaints) and Council's general complaints policy/procedure (for non-code of conduct complaints). Although the latter is dealt with pursuant to the procedures, it is critical to ensure that the Code of Conduct is also captured by the definition;
- Clause e. of the new section of the Governance Policy (*Probity – lobbying by tenderers or prospective tenderers*) sets out a prescriptive regime for dealing with complaints that are made by or on behalf of prospective tenderers. The nub of that regime is to direct such complaints through either the Procedures or otherwise Council's complaints handling policy.
- Clauses e.ii. and e.iii., strikes a balance between the facilitation of legitimate complaints (which are self-evidently essential) and the adverse consequences of baseless or illegitimate complaints made by tenderers or prospective tenderers motivated by some improper purpose.

**The new section of the Governance Policy** "*Probity – lobbying by tenderers or prospective tenderers*".

- a. "For the purposes of this section:
  - i. "Complaint Documents" includes Council's adopted Code of Conduct, Procedures for the Administration of the Code of Conduct and complaints handling policy or procedure as in force for the time being.
  - ii. "Impermissible Lobbying" means lobbying of any member of the Governing Body by or on behalf of a tenderer or a prospective tenderer and includes lobbying by any agents, servants, officers, delegates or contractors of that tenderer or prospective tenderer, including any attempt to so lobby and any other grammatical form of that phrase has a corresponding meaning;
  - iii. "Lobbying Notice" means a notice required under paragraph c. below
  - iv. "prospective tenderer" includes any person, corporation, or entity howsoever described that a member of the Governing Body knows or ought to know submits, in the general course of its business, responses to requests for tenders or invitations for expressions of interest in respect of particular contracts or projects;
  - v. "tenderer" includes any person, corporation, or entity howsoever described that has submitted or proposes to submit a response to a request for tenders or an invitation for expressions of interest in respect of a particular contract or project.
- b. Council does not permit or condone Impermissible Lobbying by any tenderer or prospective tenderer.
- c. Where any tenderer or prospective tenderer Impermissibly Lobbies a member(s) of the Governing Body, that member(s) must, as soon as reasonably practicable, notify the General Manager (or the General Manager's nominee) in writing of the following:
  - i. that Impermissible Lobbying has occurred;
  - ii. the identity of the party that Impermissibly Lobbied the member;
  - iii. details of the terms of the Impermissible Lobbying and the member's response to it;
  - iv. whether, in the member's opinion, the tenderer or prospective tenderer has breached this Policy or Council's Procurement Policy; and
  - v. any interests the member must declare as a result of the Impermissible Lobbying.

- d. Following provision of the Lobbying Notice as required by paragraph c., the General Manager (or nominee) must, as soon as reasonably practicable:
- i. confirm receipt of the Lobbying Notice;
  - ii. consider the terms of the Lobbying Notice and request any further information from the member as may reasonably be required;
  - iii. consider whether the tenderer or prospective tenderer has breached this Policy or Council's Procurement Policy;
  - iv. notify the Governing Body of the action taken under this Policy in respect of any Impermissible Lobbying."
- e. Where a complaint is made to a member of the Governing Body by or on behalf of a tenderer or prospective tenderer, that member must:
- i. refer the complainant to the Complaint Documents;
  - ii. notify the complainant that any complaint is to be made in accordance with the Complaint Documents as appropriate;
  - iii. notify the complainant that making a frivolous, illegitimate or vexatious complaint may result in the complainant's tender becoming ineligible for acceptance by Council;
  - iv. refrain from acting as the complainant's agent, representative or support person in respect of any complaint;
  - v. if the complaint is in substance Impermissible Lobbying: deal with it in accordance with paragraph c. above.
- f. The obligations in clause e. i., ii and iv. apply to any complaint, regardless of the subject matter of that complaint.
- (ii) **To Council's Code of Conduct:** a new provision which reads as follows:  
"Councillors must comply with the adopted Governance Policy (as amended)"

## OPTIONS

No alternative option is recommended

## CONCLUSION

**The recommendation is to endorse amendments to the Governance Policy** specifically the inclusion of a new section entitled "*Probity – lobbying by tenderers or prospective tenderers*" and the inclusion of a new clause in the Code of Conduct.

## SOCIAL IMPLICATIONS

Not applicable

## FINANCIAL IMPLICATIONS

There are no financial implications

## POLICY IMPLICATIONS

The proposed amendments link compliance with the Governance Policy and the Code of Conduct

## STATUTORY IMPLICATIONS

No implications anticipated

## LEGAL IMPLICATIONS

Legal implications relate to issues of noncompliance with the proposed amendments of the Code of Conduct and/or the Governance Policy

---

**OPERATIONAL PLAN IMPLICATIONS**

No Implications

**RISK MANAGEMENT IMPLICATIONS**

No implications

## 12.5 REPORT ON INVESTMENTS HELD AS AT 30 JUNE 2020

Attachments:	A. Investment Portfolio and Cash as at 30 June 2020 B. Issuer Trading Limits as at 30 June 2020
Responsible Officer:	Fiona Plesman - General Manager
Author:	Natalia Cowley - Executive Manager - Office of the Chief Financial Officer
Community Plan Issue:	<i>A Council that is well managed, efficient and properly resourced and that is responsive to its communities and stakeholders</i>
Community Plan Goal:	<i>Maintain a strong focus on financial discipline to enable Council to properly respond to the needs of the communities it serves.</i>
Community Plan Strategy:	<i>Work towards the achievement of a sustainable Operating Budget result in the General Fund.</i>

### PURPOSE

To present the list of financial investments currently held by Council in accordance with the Regulation.

### OFFICER'S RECOMMENDATION

The information showing Council's investments as at 30 June 2020 be noted.

Moved: \_\_\_\_\_ Seconded: \_\_\_\_\_

### REPORT

Clause 212 (1) of the Local Government (General) Regulation 2005, requires details of funds invested, as at the end of the preceding month, to be reported to an ordinary meeting of Council.

The funds, invested under Section 625 of the Local Government Act, as at 30 June 2020 are shown in the attachments.

### COMMENT:

As at 30 June 2020, there are three issuers whose individual portfolio trading limits have been exceeded, as a result of not reinvesting a couple of matured investments, in order to cover year-end creditors. Overall, however, none of the security rating group trading limits have been exceeded and on that basis, it is recommended that special action is not taken to resolve these individual trading limit discrepancies.

The three issuers are noted in the table below:

Issuer name	Trading limit exceeded (\$)	Trading limit exceeded (%)	Type of security	Detail
AMP Bank Ltd	\$8k	0.1%	Term deposit	The earliest-maturing holding is a \$1m term deposit which matures on 07 August 2020. It is recommended that Council allows this issuer to remain in discrepancy as this security offers some of Council's better yielding interests and overall Council's security rating group trading limits have not been exceeded.

Issuer name	Trading limit exceeded (\$)	Trading limit exceeded (%)	Type of security	Detail
Bank of Queensland Ltd	\$8k	0.1%	Floating Rate Note	The earliest-maturing holding is a \$1m floating rate note which matures on 26 October 2020. It is recommended that Council allows this issuer to remain in discrepancy as this security offers some of Council's better yielding interests and overall Council's security rating group trading limits have not been exceeded.
Bendigo and Adelaide Bank Ltd	\$8k	0.1%	Floating Rate Note	The earliest-maturing holding is a \$3m floating rate note which matures on 18 August 2020. It is recommended that Council allows this issuer to remain in discrepancy as this security offers some of Council's better yielding interests and overall Council's security rating group trading limits have not been exceeded.

Council's weighted running yield is 1.41% for the month.

The Responsible Accounting Officer certifies that, apart from the above trading limit discrepancies, the investments listed have been made in accordance with the Act, the Regulations and Council's Investment Policy. This includes investments that have been made in accordance with Minister's Orders that have been subsequently amended. "Grandfathering" provisions still allow the holding of these investments. A detailed list of investments is attached.

			Face Value Original	Bond Factor	Face Value Current	Capital Price	Accrued Interest Price	Market Value	% Total Value	Running Yield	Weighted Running Yield
<b>At Call Deposit</b>											
	ME Bank At Call		2,500,000.00	1.00000000	2,500,000.00	100.000	0.000	2,500,000.00	4.16%	0.85%	
	Westpac Bus Prem At Call		2,566,416.77	1.00000000	2,566,416.77	100.000	0.000	2,566,416.77	4.27%	0.34%	
	Westpac Muswellbrook Trading Acct At Call		100,000.00	1.00000000	100,000.00	100.000	0.000	100,000.00	0.17%	0.34%	
			5,166,416.77		5,166,416.77			5,166,416.77	8.60%		0.59%
<b>Fixed Rate Bond</b>											
	BENAU 1.7 06 Sep 2024 Fixed	AU3CB0266377	2,500,000.00	1.00000000	2,500,000.00	101.365	0.536	2,547,525.00	4.24%	1.68%	
			2,500,000.00		2,500,000.00			2,547,525.00	4.24%		1.68%
<b>Floating Rate Deposit</b>											
	ANZ 1.2 21 Jul 2022 2557DAY FRD		1,000,000.00	1.00000000	1,000,000.00	100.000	0.254	1,002,544.16	1.67%	1.33%	
			1,000,000.00		1,000,000.00			1,002,544.16	1.67%		1.33%
<b>Floating Rate Note</b>											
	AMP 1.08 10 Sep 2021 FRN	AU3FN0044657	2,000,000.00	1.00000000	2,000,000.00	100.000	0.065	2,001,300.00	3.33%	1.18%	
	Auswide 1.1 06 Nov 2020 FRN	AU3FN0045621	1,000,000.00	1.00000000	1,000,000.00	100.130	0.182	1,003,120.00	1.67%	1.21%	
	Auswide 1.05 17 Mar 2023 FRN	AU3FN0053567	2,000,000.00	1.00000000	2,000,000.00	99.920	0.041	1,999,220.00	3.33%	1.15%	
	BOQ 1.17 26 Oct 2020 FRN	AU3FN0033023	1,000,000.00	1.00000000	1,000,000.00	100.214	0.225	1,004,390.00	1.67%	1.28%	
	BOQ 1.02 16 Nov 2021 FRN	AU3FN0039418	1,000,000.00	1.00000000	1,000,000.00	100.278	0.132	1,004,100.00	1.67%	1.11%	
	BOQ 1.05 03 Feb 2023 FRN	AU3FN0040549	500,000.00	1.00000000	500,000.00	100.358	0.179	502,685.00	0.84%	1.15%	
	BOQ 1.03 18 Jul 2024 FRN	AU3FN0049094	1,000,000.00	1.00000000	1,000,000.00	99.961	0.225	1,001,860.00	1.67%	1.15%	
	BOQ 1.1 30 Oct 2024 FRN	AU3FN0051272	500,000.00	1.00000000	500,000.00	100.210	0.200	502,050.00	0.84%	1.20%	
	BENAU 1.1 18 Aug 2020 FRN	AU3FN0028361	3,000,000.00	1.00000000	3,000,000.00	100.098	0.141	3,007,170.00	5.00%	1.19%	
	BENAU 1.05 25 Jan 2023 FRN	AU3FN0040523	500,000.00	1.00000000	500,000.00	100.432	0.204	503,180.00	0.84%	1.16%	
	CredSuis 1.95 09 Mar 2021 FRN	AU3FN0030458	1,000,000.00	1.00000000	1,000,000.00	101.067	0.118	1,011,850.00	1.68%	2.05%	
	CUA 1.25 06 Sep 2021 FRN	AU3FN0044269	1,500,000.00	1.00000000	1,500,000.00	100.638	0.078	1,510,740.00	2.51%	1.35%	
	CUA 1.12 24 Oct 2024 FRN	AU3FN0051033	1,500,000.00	1.00000000	1,500,000.00	99.335	0.227	1,493,430.00	2.49%	1.23%	
	RACB 0.93 24 Feb 2023 FRN	AU3FN0053146	1,000,000.00	1.00000000	1,000,000.00	99.299	0.101	994,000.00	1.65%	1.03%	
	NAB 0.93 26 Sep 2023 FRN	AU3FN0044996	2,000,000.00	1.00000000	2,000,000.00	101.346	0.011	2,027,140.00	3.37%	1.04%	
	NPBS 1.4 06 Feb 2023 FRN	AU3FN0040606	500,000.00	1.00000000	500,000.00	100.664	0.227	504,455.00	0.84%	1.48%	
	NPBS 1.12 04 Feb 2025 FRN	AU3FN0052627	4,500,000.00	1.00000000	4,500,000.00	99.161	0.190	4,470,795.00	7.44%	1.12%	
	Qld Police 1.4 22 Mar 2021 FRN	AU3FN0041638	1,500,000.00	1.00000000	1,500,000.00	100.284	0.033	1,504,755.00	2.50%	1.50%	
	Qld Police 1.5 14 Dec 2021 FRN	AU3FN0046389	750,000.00	1.00000000	750,000.00	100.519	0.066	754,387.50	1.26%	1.60%	
	Qld Police 1.15 06 Dec 2022 FRN	AU3FN0052072	2,000,000.00	1.00000000	2,000,000.00	99.666	0.072	1,994,760.00	3.32%	1.25%	
	RABOBK 1.5 04 Mar 2021 FRN	AU3FN0030409	1,000,000.00	1.00000000	1,000,000.00	100.761	0.114	1,008,750.00	1.68%	1.60%	
	SunBank 1.25 20 Oct 2020 FRN	AU3FN0029195	4,000,000.00	1.00000000	4,000,000.00	100.272	0.267	4,021,560.00	6.69%	1.35%	
	UBS Aust 1.05 21 Nov 2022 FRN	AU3FN0054151	1,000,000.00	1.00000000	1,000,000.00	100.613	0.125	1,007,380.00	1.68%	1.14%	
			34,750,000.00		34,750,000.00			34,833,077.50	57.97%		1.25%
<b>Term Deposit</b>											



Fixed Interest Security	ISIN	Face Value Original	Bond Factor	Face Value Current	Capital Price	Accrued Interest Price	Market Value	% Total Value	Running Yield	Weighted Running Yield
AMP 1.75 07 Aug 2020 122DAY TD		1,000,000.00	1.00000000	1,000,000.00	100.000	0.403	1,004,027.40	1.67%	1.75%	
AMP 1.65 29 Oct 2020 183DAY TD		2,000,000.00	1.00000000	2,000,000.00	100.000	0.280	2,005,605.48	3.34%	1.65%	
AMP 1.6 30 Nov 2020 185DAY TD		1,000,000.00	1.00000000	1,000,000.00	100.000	0.140	1,001,402.74	1.67%	1.60%	
Auswide 1.55 10 Nov 2020 180DAY TD		1,500,000.00	1.00000000	1,500,000.00	100.000	0.200	1,502,993.84	2.50%	1.55%	
BOQ 1.5 30 Oct 2020 178DAY TD		1,000,000.00	1.00000000	1,000,000.00	100.000	0.230	1,002,301.37	1.67%	1.50%	
BOQ 3.4 21 Jun 2021 1826DAY TD		1,000,000.00	1.00000000	1,000,000.00	100.000	0.075	1,000,745.21	1.67%	3.40%	
BVIC 1.86 01 Jul 2020 92DAY TD		2,000,000.00	1.00000000	2,000,000.00	100.000	0.464	2,009,274.52	3.34%	1.86%	
ME Bank 1.4 13 Nov 2020 184DAY TD		3,000,000.00	1.00000000	3,000,000.00	100.000	0.184	3,005,523.30	5.00%	1.40%	
MYS 1.8 17 Sep 2020 182DAY TD		2,000,000.00	1.00000000	2,000,000.00	100.000	0.508	2,010,158.90	3.35%	1.80%	
P&NB 3.53 21 Jun 2021 1826DAY TD		2,000,000.00	1.00000000	2,000,000.00	100.000	0.077	2,001,547.40	3.33%	3.53%	
		16,500,000.00		16,500,000.00			16,543,580.16	27.53%		1.97%
Fixed Interest Total		59,916,416.77		59,916,416.77			60,093,143.59	100.00%		1.41%

## Section 2: FI Portfolio Valuation With Associated Latest Deal Information

Fixed Interest Security	ISIN	Latest FI Deal Settlement Date	WAL / Maturity Date Interim	Face Value Original	Bond Factor	Face Value Current	Capital Price	Accrued Interest Price	Market Value	% Total Value	Latest FI Deal Code	Notes of Latest FI Deal
<b>At Call Deposit</b>												
ME Bank At Call		30 Jun 2020	31 Dec 2020	2,500,000.00	1.00000000	2,500,000.00	100.000	0.000	<b>2,500,000.00</b>	4.16%	LC90188	
Westpac Bus Prem At Call		30 Jun 2020	31 Dec 2020	2,566,416.77	1.00000000	2,566,416.77	100.000	0.000	<b>2,566,416.77</b>	4.27%	LC90440	
Westpac Muswellbrook Trading Acct At Call		31 Oct 2018	31 Dec 2020	100,000.00	1.00000000	100,000.00	100.000	0.000	<b>100,000.00</b>	0.17%	LC64506	
				<b>5,166,416.77</b>		<b>5,166,416.77</b>			<b>5,166,416.77</b>	<b>8.60%</b>		
<b>Fixed Rate Bond</b>												
BENAU 1.7 06 Sep 2024 Fixed	AU3CB0266377	9 Jun 2020	6 Sep 2024	2,500,000.00	1.00000000	2,500,000.00	101.365	0.536	<b>2,547,525.00</b>	4.24%	LC88979	
				<b>2,500,000.00</b>		<b>2,500,000.00</b>			<b>2,547,525.00</b>	<b>4.24%</b>		
<b>Floating Rate Deposit</b>												
ANZ 1.2 21 Jul 2022 2557DAY FRD		21 Jul 2015	21 Jul 2022	1,000,000.00	1.00000000	1,000,000.00	100.000	0.254	<b>1,002,544.16</b>	1.67%	LC37873	
				<b>1,000,000.00</b>		<b>1,000,000.00</b>			<b>1,002,544.16</b>	<b>1.67%</b>		
<b>Floating Rate Note</b>												
AMP 1.08 10 Sep 2021 FRN	AU3FN0044657	23 Oct 2018	10 Sep 2021	2,000,000.00	1.00000000	2,000,000.00	100.000	0.065	<b>2,001,300.00</b>	3.33%	LC63758	
Auswide 1.1 06 Nov 2020 FRN	AU3FN0045621	7 Nov 2018	6 Nov 2020	1,000,000.00	1.00000000	1,000,000.00	100.130	0.182	<b>1,003,120.00</b>	1.67%	LC64354	
Auswide 1.05 17 Mar 2023 FRN	AU3FN0053567	17 Mar 2020	17 Mar 2023	2,000,000.00	1.00000000	2,000,000.00	99.920	0.041	<b>1,999,220.00</b>	3.33%	LC84611	
BOQ 1.17 26 Oct 2020 FRN	AU3FN0033023	26 Oct 2016	26 Oct 2020	1,000,000.00	1.00000000	1,000,000.00	100.214	0.225	<b>1,004,390.00</b>	1.67%	LC41646	
BOQ 1.02 16 Nov 2021 FRN	AU3FN0039418	16 Jan 2018	16 Nov 2021	1,000,000.00	1.00000000	1,000,000.00	100.278	0.132	<b>1,004,100.00</b>	1.67%	LC54760	
BOQ 1.05 03 Feb 2023 FRN	AU3FN0040549	5 Feb 2018	3 Feb 2023	500,000.00	1.00000000	500,000.00	100.358	0.179	<b>502,685.00</b>	0.84%	LX55025	
BOQ 1.03 18 Jul 2024 FRN	AU3FN0049094	18 Jul 2019	18 Jul 2024	1,000,000.00	1.00000000	1,000,000.00	99.961	0.225	<b>1,001,860.00</b>	1.67%	LC74377	
BOQ 1.1 30 Oct 2024 FRN	AU3FN0051272	2 Jun 2020	30 Oct 2024	500,000.00	1.00000000	500,000.00	100.210	0.200	<b>502,050.00</b>	0.84%	LX88585	
BENAU 1.1 18 Aug 2020 FRN	AU3FN0028361	24 Oct 2017	18 Aug 2020	3,000,000.00	1.00000000	3,000,000.00	100.098	0.141	<b>3,007,170.00</b>	5.00%	LC52384	
BENAU 1.05 25 Jan 2023 FRN	AU3FN0040523	25 Jan 2018	25 Jan 2023	500,000.00	1.00000000	500,000.00	100.432	0.204	<b>503,180.00</b>	0.84%	LX54945	
CredSuis 1.95 09 Mar 2021 FRN	AU3FN0030458	9 Mar 2016	9 Mar 2021	1,000,000.00	1.00000000	1,000,000.00	101.067	0.118	<b>1,011,850.00</b>	1.68%	LC37862	
CUA 1.25 06 Sep 2021 FRN	AU3FN0044269	3 Dec 2018	6 Sep 2021	1,500,000.00	1.00000000	1,500,000.00	100.638	0.078	<b>1,510,740.00</b>	2.51%	LC65378	
CUA 1.12 24 Oct 2024 FRN	AU3FN0051033	29 Nov 2019	24 Oct 2024	1,500,000.00	1.00000000	1,500,000.00	99.335	0.227	<b>1,493,430.00</b>	2.49%	LX80050	
RACB 0.93 24 Feb 2023 FRN	AU3FN0053146	24 Feb 2020	24 Feb 2023	1,000,000.00	1.00000000	1,000,000.00	99.299	0.101	<b>994,000.00</b>	1.65%	LX83602	
NAB 0.93 26 Sep 2023 FRN	AU3FN0044996	18 Mar 2020	26 Sep 2023	2,000,000.00	1.00000000	2,000,000.00	101.346	0.011	<b>2,027,140.00</b>	3.37%	LX84919	
NPBS 1.4 06 Feb 2023 FRN	AU3FN0040606	21 Nov 2019	6 Feb 2023	500,000.00	1.00000000	500,000.00	100.664	0.227	<b>504,455.00</b>	0.84%	LC79854	
NPBS 1.12 04 Feb 2025 FRN	AU3FN0052627	14 Apr 2020	4 Feb 2025	4,500,000.00	1.00000000	4,500,000.00	99.161	0.190	<b>4,470,795.00</b>	7.44%	LC85944	
Qld Police 1.4 22 Mar 2021 FRN	AU3FN0041638	20 Mar 2018	22 Mar 2021	1,500,000.00	1.00000000	1,500,000.00	100.284	0.033	<b>1,504,755.00</b>	2.50%	LC56706	
Qld Police 1.5 14 Dec 2021 FRN	AU3FN0046389	14 Dec 2018	14 Dec 2021	750,000.00	1.00000000	750,000.00	100.519	0.066	<b>754,387.50</b>	1.26%	LC65779	
Qld Police 1.15 06 Dec 2022 FRN	AU3FN0052072	6 Dec 2019	6 Dec 2022	2,000,000.00	1.00000000	2,000,000.00	99.666	0.072	<b>1,994,760.00</b>	3.32%	LC80115	
RABOBK 1.5 04 Mar 2021 FRN	AU3FN0030409	4 Mar 2016	4 Mar 2021	1,000,000.00	1.00000000	1,000,000.00	100.761	0.114	<b>1,008,750.00</b>	1.68%	LC37861	
SunBank 1.25 20 Oct 2020 FRN	AU3FN0029195	24 Oct 2017	20 Oct 2020	4,000,000.00	1.00000000	4,000,000.00	100.272	0.267	<b>4,021,560.00</b>	6.69%	LC52427	

		Latest FI Deal Settlement Date	WAL / Maturity Date Interim	Face Value Original	Bond Factor	Face Value Current	Capital Price	Accrued Interest Price	Market Value	% Total Value	Latest FI Deal Code	Notes of Latest FI Deal
Fixed Interest Security	ISIN											
UBS Aust 1.05 21 Nov 2022 FRN	AU3FN0054151	21 May 2020	21 Nov 2022	1,000,000.00	1.00000000	1,000,000.00	100.613	0.125	1,007,380.00	1.68%	LC87861	
				34,750,000.00		34,750,000.00			34,833,077.50	57.97%		
Term Deposit												
AMP 1.75 07 Aug 2020 122DAY TD		7 Apr 2020	7 Aug 2020	1,000,000.00	1.00000000	1,000,000.00	100.000	0.403	1,004,027.40	1.67%	LC85852	
AMP 1.65 29 Oct 2020 183DAY TD		29 Apr 2020	29 Oct 2020	2,000,000.00	1.00000000	2,000,000.00	100.000	0.280	2,005,605.48	3.34%	LC86511	
AMP 1.6 30 Nov 2020 185DAY TD		29 May 2020	30 Nov 2020	1,000,000.00	1.00000000	1,000,000.00	100.000	0.140	1,001,402.74	1.67%	LC88554	
Auswide 1.55 10 Nov 2020 180DAY TD		14 May 2020	10 Nov 2020	1,500,000.00	1.00000000	1,500,000.00	100.000	0.200	1,502,993.84	2.50%	LC87856	
BOQ 1.5 30 Oct 2020 178DAY TD		5 May 2020	30 Oct 2020	1,000,000.00	1.00000000	1,000,000.00	100.000	0.230	1,002,301.37	1.67%	LX86912	
BOQ 3.4 21 Jun 2021 1826DAY TD		21 Jun 2016	21 Jun 2021	1,000,000.00	1.00000000	1,000,000.00	100.000	0.075	1,000,745.21	1.67%	LC38034	
BVIC 1.86 01 Jul 2020 92DAY TD		31 Mar 2020	1 Jul 2020	2,000,000.00	1.00000000	2,000,000.00	100.000	0.464	2,009,274.52	3.34%	LC85382	
ME Bank 1.4 13 Nov 2020 184DAY TD		13 May 2020	13 Nov 2020	3,000,000.00	1.00000000	3,000,000.00	100.000	0.184	3,005,523.30	5.00%	LC87793	
MYS 1.8 17 Sep 2020 182DAY TD		19 Mar 2020	17 Sep 2020	2,000,000.00	1.00000000	2,000,000.00	100.000	0.508	2,010,158.90	3.35%	LX84950	
P&NB 3.53 21 Jun 2021 1826DAY TD		21 Jun 2016	21 Jun 2021	2,000,000.00	1.00000000	2,000,000.00	100.000	0.077	2,001,547.40	3.33%	LC51535	
				16,500,000.00		16,500,000.00			16,543,580.16	27.53%		
Fixed Interest Total				59,916,416.77		59,916,416.77			60,093,143.59	100.00%		

**Disclaimer:**

Laminar Capital Pty Ltd ABN 33 134 784 740 (AFSL 476886), its officers, employees, agents and associates ("Associates") from time to time hold interests in securities of, or earn brokerage, fees and other benefits from, corporations or investment vehicles referred to in documents provided to clients. All information contained herein is confidential and proprietary to Laminar Capital and, accordingly, this material is not to be reproduced in whole or in part or used for any purpose except as authorised by Laminar Capital. It is to be treated as strictly confidential and not disclosed directly or indirectly to any other person, firm or entity.

Distribution of this information to anyone other than the original recipient and that party's advisers is unauthorised. Any reproduction of these materials, in whole or in part, or the divulgence of any of its contents, without the prior consent of Laminar Capital is prohibited. Any securities recommendation or comments (including an opinion) contained in this document is general advice only and does not take into account your personal objectives, financial situation or needs. Laminar Capital is not acting in a fiduciary capacity. Recommendations or statements of opinion expressed may change without notice. You should not act on a recommendation or statement of opinion without first considering the appropriateness of the general advice to your personal circumstances or consulting your investment advisor to determine whether the recommendation or statement of opinion is appropriate for your investment objectives, financial situation or needs.

Laminar Capital believes that the information contained in this document is accurate when issued. Laminar Capital does not warrant that the information contained herein is accurate, reliable, complete or up-to-date, and, to the fullest extent permitted by law, disclaims all liability of Laminar Capital and its Associates for any loss or damage suffered by any person by reason of the use by that person of, or their reliance on, any information contained in this document or any error or defect in this document, whether arising from the negligence of Laminar Capital or its Associates or otherwise. No action should be taken on the basis of or in reliance on the information, opinions or conclusions contained in this document.

Laminar Capital acts as principal when we buy and sell fixed interest securities in the secondary markets. The yield that we quote to you incorporates any margin that we may receive. The margin is the difference between the price at which we, as principal, buy the security and the price at which we sell the security to you. Laminar Capital may also receive placement fees from Issuers for distributing securities on their behalf.

This document is not, and is not intended to be, an offer or invitation for subscription or sale, or a recommendation, with respect to any securities, nor is it to form the basis of any contract or commitment. This document does not purport to identify the nature of the specific market or other risks associated with these products. Before entering into any transaction in relation to the products, the investor should ensure that it fully understands the terms of the products and the transaction, relevant risk factors, the nature and extent of the investor's risk of loss and the nature of the contractual relationship into which the investor is entering. Prior to investing in these products, an investor should determine, based on its own independent review and such professional advice as it deems appropriate, the economic risks and merits, the legal, tax accounting characteristics and risk, and the consequences of an investment in them. This is not a substantive commentary or analysis by Laminar Capital and has not been prepared as a research product or comments by a research analyst.

LAMINAR CAPITAL PTY LTD  
ACN 134 784 740  
WWW.LAMINARCAPITAL.COM.AU

MELBOURNE OFFICE: LEVEL 2, 546 COLLINS STREET, MELBOURNE, VIC 3000 T 61 3 9001 6990 F 61 3 9001 6933  
SYDNEY OFFICE: LEVEL 42, GATEWAY TOWER, 1 MACQUARIE PLACE, SYDNEY NSW, 2000 T 61 2 8094 1230 F 61 2 8094 1233  
BRISBANE OFFICE: LEVEL 18, RIVERSIDE CENTRE 123 EAGLE STREET, BRISBANE QLD, 4000 T 61 7 3123 5370 F 61 7 3123 5371

Report Code: TBSBP100EXT-01.15  
Report Description: Portfolio Valuation As At Date  
Parameters:  
Term Deposit Interest Included  
Cash Excluded

## 1 Issuer Trading Limits

Issuer	Issuer Rating Group (Long Term)	Issuer Parent	Already Traded (with Issuer Group) Face Value Notional	Limit For Book or Entity	Trading Limit	Trading Limit Type	Trading Limit Value	Trading Limit Used (%)	Trading Limit Available (%)	Trading Limit Available (Value)	Trading Limit Exceeded (%)	Trading Limit Exceeded (\$)
AMP Bank Ltd	BBB+ to BBB-		6,000,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	100.00	0.00	0	0.00	8,358
ANZ Banking Group Ltd	AA+ to AA-		1,000,000.00	Book	30.00	% of 59,916,416.77	17,974,925.03	6.00	94.00	16,974,925	0.00	0
Auswide Bank Limited	BBB+ to BBB-		4,500,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	75.00	25.00	1,491,642	0.00	0
Bank of Queensland Ltd	A+ to A-		6,000,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	100.00	0.00	0	0.00	8,358
BankVic	BBB+ to BBB-		2,000,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	33.00	67.00	3,991,642	0.00	0
Bendigo & Adelaide Bank Ltd	A+ to A-		6,000,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	100.00	0.00	0	0.00	8,358
Credit Suisse Sydney	A+ to A-		1,000,000.00	Book	20.00	% of 59,916,416.77	11,983,283.35	8.00	92.00	10,983,283	0.00	0
Credit Union Australia Ltd	BBB+ to BBB-		3,000,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	50.00	50.00	2,991,642	0.00	0
Members Banking Group Limited t/as RACQ Bank	BBB+ to BBB-		1,000,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	17.00	83.00	4,991,642	0.00	0
Members Equity Bank Ltd	BBB+ to BBB-		5,500,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	92.00	8.00	491,642	0.00	0
MyState Bank Ltd	BBB+ to BBB-		2,000,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	33.00	67.00	3,991,642	0.00	0
National Australia Bank Ltd	AA+ to AA-		2,000,000.00	Book	30.00	% of 59,916,416.77	17,974,925.03	11.00	89.00	15,974,925	0.00	0
Newcastle Permanent Building Society Ltd	BBB+ to BBB-		5,000,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	83.00	17.00	991,642	0.00	0
P&N Bank Ltd	BBB+ to BBB-		2,000,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	33.00	67.00	3,991,642	0.00	0
QPCU LTD t/a QBANK	BBB+ to BBB-		4,250,000.00	Book	10.00	% of 59,916,416.77	5,991,641.68	71.00	29.00	1,741,642	0.00	0
Rabobank Nederland Australia Branch	AA+ to AA-		1,000,000.00	Book	20.00	% of 59,916,416.77	11,983,283.35	8.00	92.00	10,983,283	0.00	0
Suncorp Bank	A+ to A-		4,000,000.00	Book	20.00	% of 59,916,416.77	11,983,283.35	33.00	67.00	7,983,283	0.00	0
UBS Australia Ltd	AA+ to AA-		1,000,000.00	Book	20.00	% of 59,916,416.77	11,983,283.35	8.00	92.00	10,983,283	0.00	0
Westpac Banking Corporation Ltd	AA+ to AA-		2,666,416.77	Book	30.00	% of 59,916,416.77	17,974,925.03	15.00	85.00	15,308,508	0.00	0
			59,916,416.77				173,757,608.63			113,866,268		25,074

## 2 Security Rating Group Trading Limits

Security Rating Group	Already Traded Face Value Notional	Limit For Book or Trading Entity	Trading Limit	Trading Limit Type	Trading Limit Value	Trading Limit Used (%)	Trading Limit Available (%)	Trading Limit Available (Value)	Trading Limit Exceeded (%)	Trading Limit Exceeded (\$)
AA+ to AA-	3,000,000.00	Book	100.00	% of 59,916,416.77	59,916,416.77	5.00	95.00	56,916,417	0.00	0
A+ to A-	4,500,000.00	Book	70.00	% of 59,916,416.77	41,941,491.74	11.00	89.00	37,441,492	0.00	0
A1+	2,666,416.77	Book	100.00	% of 59,916,416.77	59,916,416.77	4.00	96.00	57,250,000	0.00	0
A1	6,000,000.00	Book	70.00	% of 59,916,416.77	41,941,491.74	14.00	86.00	35,941,492	0.00	0
A2	21,500,000.00	Book	60.00	% of 59,916,416.77	35,949,850.06	60.00	40.00	14,449,850	0.00	0
A3	1,500,000.00	Book	60.00	% of 59,916,416.77	35,949,850.06	4.00	96.00	34,449,850	0.00	0
BBB+ to BBB-	20,750,000.00	Book	60.00	% of 59,916,416.77	35,949,850.06	58.00	42.00	15,199,850	0.00	0
	59,916,416.77				311,565,367.20			251,648,951		0

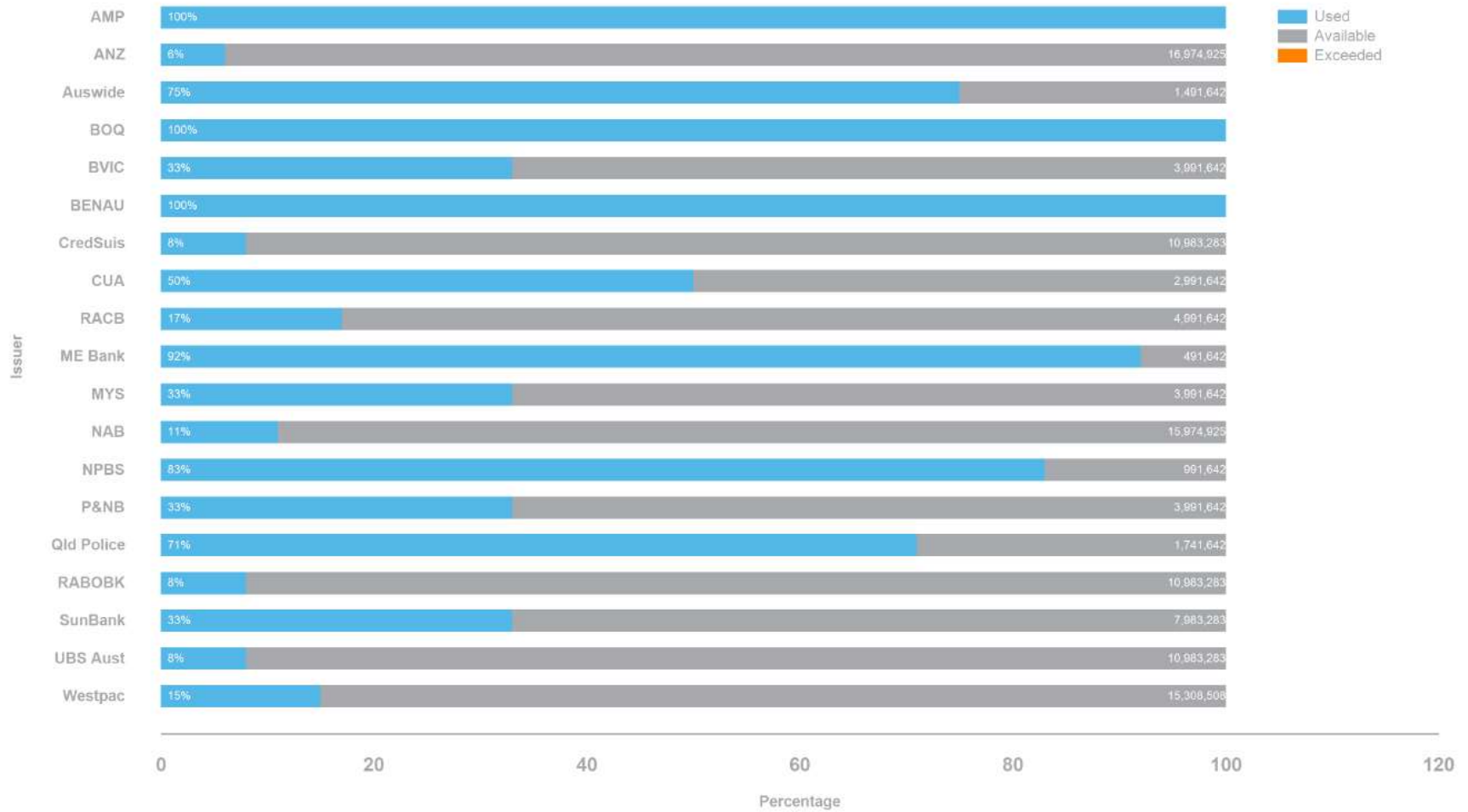
### Notes

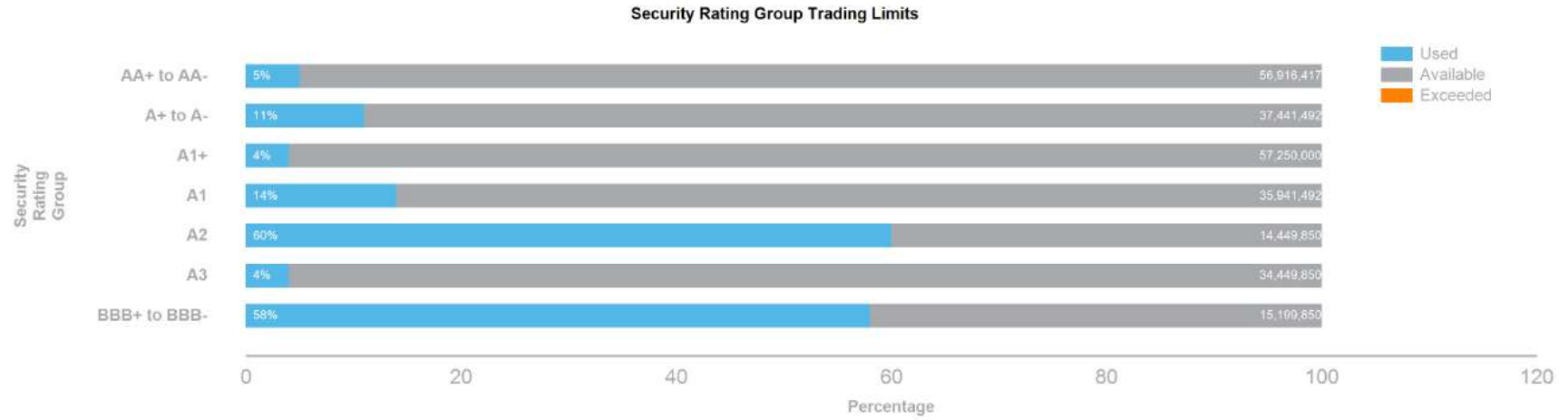
1. In instances where long securities have a term remaining which is less than 365 days, the issuer's short term rating is used instead of the security's (presumably long term) rating.



### 3 Term Group Trading Limits

Term Group	Already Traded Face Value Notional	Limit For Book or Trading Entity	Trading Limit	Trading Limit Type	Trading Limit Value	Trading Limit Used (%)	Trading Limit Available (%)	Trading Limit Available (Value)	Trading Limit Exceeded (%)	Trading Limit Exceeded (\$)
0-1 Year	34,166,416.77	Book	100.00	% of 59,916,416.77	59,916,416.77	57.00	43.00	25,750,000	0.00	0
1-3 Year	13,750,000.00	Book	100.00	% of 59,916,416.77	59,916,416.77	23.00	77.00	46,166,417	0.00	0
3-5 Year	12,000,000.00	Book	100.00	% of 59,916,416.77	59,916,416.77	20.00	80.00	47,916,417	0.00	0
	59,916,416.77				179,749,250.31			119,832,834		0

**Issuer Trading Limits**






**Disclaimer:**

Laminar Capital Pty Ltd ABN 33 134 784 740 (AFSL 476886), its officers, employees, agents and associates ("Associates") from time to time hold interests in securities of, or earn brokerage, fees and other benefits from, corporations or investment vehicles referred to in documents provided to clients. All information contained herein is confidential and proprietary to Laminar Capital and, accordingly, this material is not to be reproduced in whole or in part or used for any purpose except as authorised by Laminar Capital. It is to be treated as strictly confidential and not disclosed directly or indirectly to any other person, firm or entity.

Distribution of this information to anyone other than the original recipient and that party's advisers is unauthorised. Any reproduction of these materials, in whole or in part, or the divulgence of any of its contents, without the prior consent of Laminar Capital is prohibited. Any securities recommendation or comments (including an opinion) contained in this document is general advice only and does not take into account your personal objectives, financial situation or needs. Laminar Capital is not acting in a fiduciary capacity. Recommendations or statements of opinion expressed may change without notice. You should not act on a recommendation or statement of opinion without first considering the appropriateness of the general advice to your personal circumstances or consulting your investment advisor to determine whether the recommendation or statement of opinion is appropriate for your investment objectives, financial situation or needs.

Laminar Capital believes that the information contained in this document is accurate when issued. Laminar Capital does not warrant that the information contained herein is accurate, reliable, complete or up-to-date, and, to the fullest extent permitted by law, disclaims all liability of Laminar Capital and its Associates for any loss or damage suffered by any person by reason of the use by that person of, or their reliance on, any information contained in this document or any error or defect in this document, whether arising from the negligence of Laminar Capital or its Associates or otherwise. No action should be taken on the basis of or in reliance on the information, opinions or conclusions contained in this document.

Laminar Capital acts as principal when we buy and sell fixed interest securities in the secondary markets. The yield that we quote to you incorporates any margin that we may receive. The margin is the difference between the price at which we, as principal, buy the security and the price at which we sell the security to you. Laminar Capital may also receive placement fees from Issuers for distributing securities on their behalf.

This document is not, and is not intended to be, an offer or invitation for subscription or sale, or a recommendation, with respect to any securities, nor is it to form the basis of any contract or commitment. This document does not purport to identify the nature of the specific market or other risks associated with these products. Before entering into any transaction in relation to the products, the investor should ensure that it fully understands the terms of the products and the transaction, relevant risk factors, the nature and extent of the investor's risk of loss and the nature of the contractual relationship into which the investor is entering. Prior to investing in these products, an investor should determine, based on its own independent review and such professional advice as it deems appropriate, the economic risks and merits, the legal, tax accounting characteristics and risk, and the consequences of an investment in them. This is not a substantive commentary or analysis by Laminar Capital and has not been prepared as a research product or comments by a research analyst.

LAMINAR CAPITAL PTY LTD  
ACN 134 784 740  
WWW.LAMINARCAPITAL.COM.AU

MELBOURNE OFFICE: LEVEL 2, 546 COLLINS STREET, MELBOURNE, VIC 3000 T 61 3 9001 6990 F 61 3 9001 6933  
SYDNEY OFFICE: LEVEL 42, GATEWAY TOWER, 1 MACQUARIE PLACE, SYDNEY NSW, 2000 T 61 2 8094 1230 F 61 2 8094 1233  
BRISBANE OFFICE: LEVEL 18, RIVERSIDE CENTRE 123 EAGLE STREET, BRISBANE QLD, 4000 T 61 7 3123 5370 F 61 7 3123 5371

Report Code: TSSBP125EXT-00.16  
Report Description: Trading Limit Performance As At Date  
Parameters:  
As At/Scenario Date: 30 June 2020  
Balance Date: 13 July 2020 (but 30 Jun 2020 used instead)  
Trading Entity: Muswellbrook Shire Council  
Trading Book: Muswellbrook Shire Council  
Report Mode: Bal Only  
Using Face Value  
Trading Entity and Book Limits  
Effects of Parent/Child Issuers Ignored  
Hide Zero Holdings

## 12.6 PLANNING AND ENVIRONMENT SERVICES

<b>Attachments:</b>	Nil
<b>Responsible Officer:</b>	Sharon Pope - Assistant Director - Environment & Community Services
<b>Author:</b>	Ziggy Andersons - Ecologist and Sustainability Team Leader Michael Brady - Sustainability Officer Tracy Ward - Sustainability Officer Chloe Wuiske - Administration Officer Jo Barker - Records Officer
<b>Community Plan Issue:</b>	<i>A Council that is well managed, efficient and properly resourced and that is responsive to its communities and stakeholders</i>
<b>Community Plan Goal:</b>	<i>Maintain a strong focus on financial discipline to enable Council to properly respond to the needs of the communities it serves.</i>
<b>Community Plan Strategy:</b>	<i>Appropriate matters are reported to Council in a timely manner in accordance with the Financial Control and Reporting Policy.</i>

### PURPOSE

To provide an update on activities in the Planning and Environmental Services sections.

### OFFICER'S RECOMMENDATION

The information contained in this report be noted.

Moved: \_\_\_\_\_ Seconded: \_\_\_\_\_

### REPORT

#### PLANNING & ENVIRONMENTAL SERVICES

##### 1. Statistical Information

**Note:** Statistics for Section 10.7 Planning Certificates, Development Applications, Construction Certificates and Complying Development Certificates are distributed separately to Councillors with whole of month data prior to the Council meeting. The statistics are also available on Council's website.

#### ***Schedule 1: Development Applications Approved (13 June to 14 July 2020)***

DA No.	DESCRIPTION	PROPERTY	VALUE (\$)
2019/88/2	S4.55(1A) Modification - Commercial fitout additions/alterations	60-82 Bridge Street Muswellbrook	-
2020/50	Residential Shed	103 Virginia Street Denman	29,600
2020/47	Dwelling House	36-38 Finnegan Crescent, Muswellbrook	353,165
2020/46	HAY SHED	111 Turnermans (N) Road, Martindale	61,369
2020/42	Demolition of a Dwelling	24 Martindale Street, Denman	25,000



DA No.	DESCRIPTION	PROPERTY	VALUE (\$)
2020/40	Geotechnical Investigations (Earthworks) and Vegetation Clearing	Limestone Road, Muswellbrook	1,093,332
2020/39	New Dwelling	1984 Martindale Road, Denman	482,925
2020/38	New Dwelling	Sandy Creek Road, Muswellbrook	256,700
2020/37	New Dwelling, Swimming Pool and Alfresco Area	18 Angus Peebles Close, Muswellbrook	507,420
2020/31	Hay Shed	1040 Bureen Road, Denman	52,945
2020/30	Partial Demolition and Rebuild of Fire Damaged Dwelling	148 Sydney Street, Muswellbrook	200,933
2018/54	Demolition of a Commercial Building and the Construction of a Three (3) Storey Building for Use as a Tertiary Education Establishment and Food and Drink Premises, Alterations and Additions to the 'Loxton House', Heritage Listed Item, and the Use of Level 1 of this Building as a Tertiary Education Establishment, Alterations and Additions to the Muswellbrook Public Library and Ancillary Works.	140 Bridge Street, Muswellbrook	4,983,672

***Schedule 2: Development Applications Currently Being Assessed***

DA No.	DESCRIPTION	PROPERTY	RECEIVED	VALUE (\$)
2020/70	Residential Shed	45 Wanaruah Circuit Muswellbrook	13/07/2020	9,500
2019/24/2	S4.55(1) Modification - New Dwelling and Ancillary Development (Shed)	15 Babbler Crescent Muscle Creek	13/07/2020	-
2020/68	Residential Shed	52 Calgaroo Avenue Muswellbrook	08/07/2020	14,500
2020/69	Change of use - Bakery/Cafe	72-78 Brook Street Muswellbrook	07/07/2020	100,000
2020/67	Residential Shed	36 Top Knot Place Muswellbrook	07/07/2020	32,900
2020/66	Dwelling Alterations (Attached Outdoor Leisure Room)	85 Humphries Street Muswellbrook	07/07/2020	35,950
2020/65	Residential Shed	13b Grey Gum Road Denman	06/07/2020	35,000
2020/64	Change of use of a restaurant to a health services facility (chiropractic practice)	26 Sowerby Street Muswellbrook	03/07/2020	10,000
2020/38/2	S4.55(1) Modification - New Dwelling	Sandy Creek Road Muswellbrook	03/07/2020	-
2020/63	Change of use of a shop to a recreation facility (indoor)	72-78 Brook Street Muswellbrook	25/06/2020	-
2020/62	Residential Shed	3 Jillaroo Way Muswellbrook	24/06/2020	24,000

DA No.	DESCRIPTION	PROPERTY	RECEIVED	VALUE (\$)
2020/61	Residential Shed	15 Grey Gum Road Denman	22/06/2020	49,700
2020/60	Dwelling House	190 Woodland Ridge Road Muscle Creek	19/06/2020	377,993
2020/59	Signage	49-51 Maitland Street Muswellbrook	19/06/2020	7,500
2020/58	Commercial Alterations and Additions	4 Lorne Street Muswellbrook	16/06/2020	45,000
2020/57	Amenities Building	St James Crescent Muswellbrook	16/06/2020	242,000
2020/55	Commercial Fitout Additions and Alterations	20 Ogilvie Street Denman	12/06/2020	49,720
2020/56	Dwelling House	86 Babbler Crescent Muscle Creek	10/06/2020	452,700
2020/54	Change of use - refrigeration and electrical sales, service and warehousing (light industry)	74 Aberdeen Street Muswellbrook	05/06/2020	50,000
2020/53	Dwelling House	81 Babbler Crescent Muscle Creek	02/06/2020	425,995
2020/52	Dwelling House	20 Tindale Street Muswellbrook	02/06/2020	283,094
2020/51	Residential Shed	5 Arlingham Close Muswellbrook	29/05/2020	25,350
2020/49	Electricity Generating Works (Solar Farm)	Denman Road Muswellbrook	21/05/2020	26,131,000
2020/48	Use of Shipping Container as a Residential Shed	54 Ironbark Road Muswellbrook	18/05/2020	1,200
2020/45	Replacement of current Holden Signage with new Ford Signage	15-17 Rutherford Road Muswellbrook	15/05/2020	85,000
2008/66/3	S4.55 (1A) Modification to change the use of a shed to a dwelling	1060 Sandy Creek Road McCullys Gap	15/05/2020	-
2020/43	Animal Boarding Establishment and Community Facility	127-129 Sydney Street Muswellbrook	07/05/2020	2,550,000
2020/41	Residential Shed and Carport	53 Virginia Street Denman	06/05/2020	18,000
2020/36	Residential Outbuilding - Storage Space	540 Sandy Creek Road Muswellbrook	17/04/2020	20,000
2020/35	599 Lot Residential Subdivision and associated Civil Construction Work	8911 New England Highway Muswellbrook	15/04/2020	21,200,572
2020/34	Change of Use to Indoor Recreational Facility (Gymnastics Centre)	5 Glen Munro Road Muswellbrook	15/04/2020	10,000
2020/32	Change of Use of Existing Buildings for Vehicle Repair Station	Golden Highway Sandy Hollow	08/04/2020	3,500
2003/72/5	S4.55(2) Modification - To Make Previously Approved 12 Month Trial of Extended Hours of Operation of Restaurant Drive-thru (McDonald's) Permanent.	83-89 Maitland Street Muswellbrook	06/04/2020	-

DA No.	DESCRIPTION	PROPERTY	RECEIVED	VALUE (\$)
2020/28	Alterations and additions to two (2) existing dwellings and their strata subdivision	39 Market Street Muswellbrook	19/03/2020	181,610
2020/22	Industrial Hard Stand Area	38-40 Enterprise Crescent Muswellbrook	19/02/2020	73,500
2020/7	Additions and Alterations to existing Hotel	184 Bridge Street Muswellbrook	24/01/2020	110,000
2020/5	Change of Use from a Restaurant to a Community Facility	29 Sydney Street Muswellbrook	20/01/2020	-
2019/104	Change of use to warehouse and distribution premises with a new office and storage/shade structure	Thomas Mitchell Drive Muswellbrook	27/11/2019	75,000
2019/93	Change of use to Retail Outlet, Alterations to front of building and Signage	19 Bridge Street MUSWELLBROOK	24/10/2019	20,000
2019/90	Subdivision of One (1) Lot into Twenty (20) Lots	9 Yarrawa Road Denman	30/09/2019	2,828,700
2019/54	Subdivision One (1) Lot into Three (3) Lots	52 Palace Street Denman	28/06/2019	10,000
2019/53	Subdivision of Two (2) Lots into Seventy Five (75) Lots	9027 New England Highway Muswellbrook	27/06/2019	4,875,600
2019/16	Information and Education Facility (Museum)	Turner Street Denman	15/02/2019	265,000
2019/13	Subdivision One (1) Lot into One Hundred and Twenty Three (123) Lots and the carrying out of associated civil work including roads, drainage and tree removal	8911 New England Highway Muswellbrook	14/02/2019	5,142,236
2019/2	The construction of multi dwelling housing comprising a total of Sixteen (16) units and the carrying out of associated site works, including internal private roads, stormwater drainage, landscaping and tree removal.	19 John Howe Circuit Muswellbrook	14/01/2019	4,414,300
2017/58/2	S96 (1A) Modification - Modify Condition 8	Jerdan Street Denman	19/02/2018	-
2017/60	Change of Use - Building Materials Recycling Depot	7 Glen Munro Road Muswellbrook	23/06/2017	-
2000/212/6	S96(1a) Modification - Extension of timeframe of Operations & Extraction Area, Site Plan, Removal of Conditions	2449 Denman Road Muswellbrook	29/05/2017	-
2016/32	Placement of Fill	110 Merriwa Road Denman	12/04/2016	5,000

**20.1.12 Inspect onsite wastewater sewerage systems to ensure they are installed and maintained in compliance with regulatory requirements.**

**On-site Wastewater Statistics - 13 Month Analysis (2019/2020)**

	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun
Applications Received (new installation)	2	0	0	1	2	0	0	0	2	0	3	0	3
Applications Approved (new installation)	0	0	0	0	1	3	1	0	3	1	0	0	3
Inspections (new system)	0	0	0	1	1	1	0	0	3	0	1	1	0
Inspections (existing system)	47	2	3	0	11	1	0	1	0	6	0	0	0

**24.1.5 Registration and inspection of regulated premises (caravan parks, food outlets, skin penetration premises, hairdressers, mortuaries, air handling systems) in accordance with regulatory requirements to ensure public health and safety is protected.**

	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun
Applications Received (new businesses)	0	1	1	0	0	1	2	0	1	1	0	1	2
Inspections (new businesses)	0	1	0	0	1	0	2	1	1	1	0	1	0
Inspections (existing businesses)	30	0	0	0	6	1	0	10	0	4	1	1	35
Reinspections	1	2	0	0	0	0	0	0	0	0	0	0	0

**4.01.01.1 Reduce the environmental impact of development on our community by carrying out regular inspection of building sites and monitoring waste.**

**Building Site Compliance Inspection Statistics – 13 Month Analysis (2019/2020)**

	Jun	Jul	Aug	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun
Total Sites Inspected	7	7	6	7	7	6	7	4	4	6	5	4
Total non-compliant and educated	2	0	1	0	1	0	0	0	0	0	1	0
Total compliance after education	2	0	1	0	1	0	0	0	0	0	1	0
Total Penalty Notices Issued	0	0	0	0	0	0	0	0	0	0	0	0

**14.1.11 Continue surveillance and regulation of illegal dumping on an ongoing basis through participation in the Hunter Central Coast Regional Illegal Dumping Squad**

**Illegal Dumping Statistics – 13 Month Analysis (2019/2020)**

	Jun	Jul	Aug	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun
Total Investigations	7	7	4	3	2	4	7	8	2	9	4	6
Total Clean up by Council - insufficient evidence	7	3	4	2	2	4	7	2	0	5	1	5
Total Clean Up by individual	0	4	0	0	0	0	0	6	2	0	0	1
Total Penalty Notices Issued	0	0	0	0	0	0	0	0	0	0	0	0
Court Attendance Notice Issued	0	0	0	0	0	0	0	0	0	0	0	0
Still under investigation	0	0	0	0	0	0	0	0	0	0	0	0

**24.1.8 Ensure statutory requirements under the Private Swimming Pools Program (Swimming Pool Act 1992) are implemented.**

**Swimming Pool Compliance Statistics – 13 Month Analysis (2019/2020)**

	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun
Applications for Compliance Certs.	2	2	3	6	3	3	3	2	4	4	3	1	4

Total compliance inspections (not inc. Final Insp. for Occ. Certs)	8	7	10	8	3	2	5	7	6	6	3	6	8
Initial Inspections	6	5	8	8	2	1	3	7	5	5	2	5	8
Re-inspections	2	2	2	0	1	1	2	0	1	1	1	1	0

Compliance Certs / Occ. Certs issued	3	4	5	4	6	8	7	3	0	4	1	6	5
--------------------------------------	---	---	---	---	---	---	---	---	---	---	---	---	---

**Total Pools in Council's Swimming Pool Register = 923**

**Compliance as at 30 June 2017 = 63.7%**

**Compliance as at 30 June 2018 = 65.7%**

**Compliance as at 30 June 2019 = 43.0%**

**Compliance as at 30 June 2020 = 26.7%**

That is, 246 out of 923 pools have a valid Compliance Certificate or Occupation Certificate.  
N.b. Certificate is valid for 3 years.

**Percentage of pools inspected during Financial Year 2019/2020 = 6.4%**

That is, 59 out of 923 pools have had at least one inspection carried out during FY 2019/2020.

---

**SUSTAINABILITY****Rehabilitation of Muscle Creek Funding**

Muswellbrook Council has received \$73,776 from the NSW Environmental Trust's Restoration and Rehabilitation grant to restore the endangered Hunter River Redgum ecological community.

Muswellbrook Council's rehabilitation of Muscle Creek aims to restore the Hunter River Redgum endangered ecological community and involves measures such as the control and eradication of weeds, erosion control on steep banks and the establishment of native vegetation connectivity through native tree plantings. The project will also involve community engagement through awareness raising and volunteering events.

**Martindale Reserve Project**

Work has been completed on the Martindale Crown Land Reserve Project (see photos below).

Before



After

**Weed Control Along the Name Trail Denman**

The Sustainability Unit has undertaken woody weed control along the riparian zone from Turtle Street to Merino Street. This was to remove several Chinese Tallow trees, Pepper Trees, Broad Leaf Privet, some establishing Date Palms, and a couple of dead and or dying Paperbarks. The woodchip created has been left in place and will be spread shortly. The site will undergo a seasons of secondary weed control with a program of planting with shrubs and groundcover to occur in the following season.

**Environmental Weed Field guide**

The Sustainability Unit has prepared an environmental weed field guide for distribution to staff and the community. The guide was developed based on a need identified by the works team so that the less experienced staff would know what were considered as weeds along Muscle Creek as well as other natural areas within the shire. Consultation was undertaken with the Martindale Landcare Group as well



as the Regional Landcare Coordinator in preparing the list and the field guide design and layout was undertaken by Council's communications team.

### **National Tree Day Site – Highbrook Park**

The Sustainability Unit has partnered with the Muswellbrook Rugby Club (initiated by Kevin Doherty) to undertake a National Tree Day planting day on 27<sup>th</sup> September 2020. The trees will be planted alongside the path leading from the Ironbark Rd car park to the fields, adjacent to the existing plantings. The event is primarily for the club but will also be open to the public. The Council works team has offered to prepare the site for planting and council will be providing the plants.

### **Flying Fox Grant 2020**

Council was successful in securing a \$10,000 grant to remove problematic weedy Flying-fox feed trees. Broad-leaf Privet and Cocos Palms (amongst a couple of other species) that are utilised as feed trees by Flying-foxes.

Flying-foxes are quite noisy when feeding and when they take off from a feed tree they defecate, in the case of the Large-leaf Privet this stains clothes, footpaths and cars. The noise they generate has been the source of nearly all complaints to council due to health fears. In addition, the Cocos Palm fruit can lodge in a Flying-foxes jaw resulting in the death of the animal.

The details of the program will be developed in the following reporting period, fundamentally though council will be asking the community whether they have one of the identified problematic weedy feed trees and following an assessment by council their removal will be either subsidised or wholly covered by the program. Replacement native trees will be offered to those residents to limit net tree loss within the urban areas of Muswellbrook.

### **Waste Education and Engagement Planning**

Planning has begun for next financial year's waste education and engagement projects.

### **Museum in a Box**

Each term the Sustainability Unit hires a box from the Australian Museum. These boxes are a great way of engaging with the community about different environmental themes. This term, boxes have only been hired for a short period and have been shared with Muswellbrook Girl Guides, Muswellbrook High School, Muswellbrook Preschool and Goodstart Early Learning Centre.

### **Muswellbrook Connect**

The Sustainability Unit continues to engage with the community online. Staff are working with Muswellbrook Healthy and Well in coordinating social media posts under the heading Muswellbrook Connect. This involves coordinating the development of locally produced materials aimed at making recommendation to local community members on what they can do at home around living healthy and sustainable lives.

### **Sustainable Futures – Muswellbrook**

The popularity of the Sustainable Futures – Muswellbrook Facebook page continues to grow. It has now received 1,109 page likes and 1,150 page followers. This page is a great way for the Council to engage with the community around a range of sustainability topics.

### **Waste Trails**

The Sustainability Unit is working with Monash University on research aimed at determining what messages work around waste. The trials will soon begin.

### **World Environment Day**

World Environment Day was celebrated through Facebook and by encouraging residents to purchase compost bins. Twelve compost bins were sold during this promotion.

### **HJO – Don't be a Tosser project**

The Hunter Joint Organisations of Councils is running a Don't be a Tosser anti-litter project focused on sporting fields in Muswellbrook.

**Free Seedlings for the community**

The Penguin Garden Club continue to offer the community free seedlings through the fence at the Sustainability Hub in Wilkinson Ave. This project is supported by Muswellbrook Shire Council. This project has been very popular with over 500 seedlings given away and many new people interested in activities at the Sustainability Hub.

## 12.7 COMMUNITY SERVICES

<b>Attachments:</b>	Nil
<b>Responsible Officer:</b>	Fiona Plesman - General Manager
<b>Author:</b>	Kim Manwarring - Co-ordinator - Community Partnerships
<b>Community Plan Issue:</b>	<i>A Council that is well managed, efficient and properly resourced and that is responsive to its communities and stakeholders</i>
<b>Community Plan Goal:</b>	<i>Maintain a strong focus on financial discipline to enable Council to properly respond to the needs of the communities it serves.</i>
<b>Community Plan Strategy:</b>	<i>Appropriate matters are reported to Council in a timely manner in accordance with the Financial Control and Reporting Policy.</i>

### PURPOSE

To provide an update on activities in the Community Services section.

### OFFICER'S RECOMMENDATION

The information contained in this report be noted.

Moved: \_\_\_\_\_ Seconded: \_\_\_\_\_

### REPORT

#### COMMUNITY SERVICES

Preliminary work has commenced with internal staff to progress the concept of an Aboriginal Cultural Heritage Officer position that can be activated as required;

The workingwithindigenousaustralians.info website has been reviewed in consultation with the principal consultant and the inclusion of additional resources. The update has occurred alongside the development of a website sub domain that will incorporate the stories and material contained in the 'In Our Own Word' Aboriginal Oral History Project;

**Other activities as per the Community Builders Department of Communities and Justice Project and other project activities:**

- Maintenance of Upper Hunter Community Services Interagency e-list and distribution of network information;
- Governance support to Upper Hunter Youth Services Management Committee and Management Team around human resources and service development issues;
- Participation in the Local Drug Action team meetings to identify their next community education and awareness projects.

#### MUSWELLBROOK REGIONAL ARTS CENTRE

There are 6 exhibitions scheduled for this Financial Year:

**July/August Exhibitions are as follows:**

*Shapeshifters: Transformative Forms*

*Monovision: Black and White Photography from the Collections*

*Art Tracks III: Grey Skies Blackened Earth*

*Seasons of Drought and Rain: Early Links Upper Hunter*

*Reflection: Contemporary Portraiture from the Collections*

A closed invite-only opening has been scheduled for the coming exhibition and we hope to facilitate a full public opening in the next round of exhibitions.

In other activities; Discussions have commenced in with Bengalla Mining Company regarding sponsorship of the 2021 Muswellbrook Art Prize as well as a partnership with local Muswellbrook High School to exhibit HSC works in January 2021 in Exhibition: *Ignite 2021: Upper Hunter HSC Visual Arts*. Discussion have also commenced for the Artist in Residence Project for the coming financial year. The past 2020 Artist in Residence Todd Fuller is scheduled to exhibit his work in Exhibition 5 (August 2021).

## **TOURISM AND EVENTS**

### **Events and Festivals**

Due to the COVID-19 all Council run Events and Festivals has been cancelled or postponed for 2020.

The Events Team has commenced works on the implantation of an Events and Festivals Strategy. The Events and Festivals Strategy is currently in draft and the anticipated completion date is the end of August 2020. The strategy will see us better understand the needs and requirements for Events and Festivals within the Shire while providing a comprehensive document that would assist Muswellbrook Shire Council to better understand current trends and opportunities within the Events and Festival sector. Furthermore the Strategy will allow us to set goals, build partnerships within the Shire and provide a level of support to other Event and Festival organisers within the region.

The events team has set dates for the Citizenship Ceremonies, with social distancing as a consideration, our new venues will alternate between the Atherston Room and the Library Seminar Room, depending on the size of the ceremony and availability of venues.

Current Citizenship Ceremonies dates,

24 August 2020 – Confirmed

17 September 2020 – TBC

9 November 2020 – TBC

26 January 2021 – Confirmed

### **Visitor Information Centres**

The Muswellbrook Visitor Information Centre has commenced operation with COVID-19 Safe Guidelines in place. Current Muswellbrook Visitor Information Centre operating hours are,

Monday – CLOSED

Tuesday – 10:00am to 4:30pm

Wednesday – 10:00am to 4:30pm

Thursday – 10:00am to 4:30pm

Friday – 10:00am to 4:30pm

Saturday – CLOSED

Sunday – CLOSED

Furthermore current works relating to the Muswellbrook Visitor Information Centre includes signage updates scheduled to be installed by the end of July and a new town map in line with the Muswellbrook Shire branding has been scheduled for installation to the outer wall of the Muswellbrook Visitor Information Centre also due by the end of July.

The Denman Visitor Information Centre has ceased operation until further notice, but a pop-up information stand had been set up inside the Denman Library for visitor and locals to access a variety of information brochures, visitor guides and maps.

**13 REPORTS FROM COMMITTEES**

Nil

**14 NOTICES OF MOTION / RESCISSION**

Nil

**15 QUESTIONS WITH NOTICE**

Nil

**16 COUNCILLORS REPORTS****17 QUESTIONS FOR NEXT MEETING****18 ADJOURNMENT INTO CLOSED COUNCIL**

In accordance with the Local Government Act 1993, and the Local Government (General) Regulation 2005, business of a kind referred to in Section 10A(2) of the Act should be dealt with in a Confidential Session of the Council meeting closed to the press and public.

**19 CLOSED COUNCIL****RECOMMENDATION**

**That Council adjourn into Closed Session and members of the press and public be excluded from the meeting of the Closed Session, and access to the correspondence and reports relating to the items considered during the course of the Closed Session be withheld unless declassified by separate resolution. This action is taken in accordance with Section 10A(2) of the Local Government Act, 1993 as the items listed come within the following provisions:**

**19.1 TENDER NO 2019-2020-0426 CONCRETE AND MINOR ROAD WORKS**

Item 19.1 is classified CONFIDENTIAL under the provisions of Section 10A(2)(d)(i) of the local government act 1993, as it deals with commercial information of a confidential nature that would, if disclosed prejudice the commercial position of the person who supplied it, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.

**19.2 MEMORANDUM OF UNDERSTANDING WITH UPPER HUNTER SHIRE COUNCIL FOR ACCEPTANCE AND DISPOSAL OF MIXED WASTE**

Item 19.2 is classified CONFIDENTIAL under the provisions of Section 10A(2)(c) of the local government act 1993, as it deals with information that would, if disclosed, confer a commercial advantage on a person with whom the council is conducting (or proposes to conduct) business, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.

**19.3 REQUEST TO REMOVE WASTE MANAGEMENT CHARGE FROM ASSESSMENT**

Item 19.3 is classified CONFIDENTIAL under the provisions of Section 10A(2)(d)(i) of the local government act 1993, as it deals with commercial information of a confidential nature that would,

if disclosed prejudice the commercial position of the person who supplied it, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.

**19.4 MUSWELLBROOK INDOOR SPORTS CENTRE - MUSWELLBROOK YOUTH VENUE**

Item 19.4 is classified CONFIDENTIAL under the provisions of Section10A(2)(d)(i) of the local government act 1993, as it deals with commercial information of a confidential nature that would, if disclosed prejudice the commercial position of the person who supplied it, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.

**19.5 FUTURE FUND COMMITTEE MINUTES**

Item 19.5 is classified CONFIDENTIAL under the provisions of Section10A(2)(c) of the local government act 1993, as it deals with information that would, if disclosed, confer a commercial advantage on a person with whom the council is conducting (or proposes to conduct) business, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.

**19.6 EXECUTION OF REFINANCING OF \$13.3M FUTURE FUND LOAN**

Item 19.6 is classified CONFIDENTIAL under the provisions of Section10A(2)(c) of the local government act 1993, as it deals with information that would, if disclosed, confer a commercial advantage on a person with whom the council is conducting (or proposes to conduct) business, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.

**19.7 OPTIMISING THE RETURN ON COUNCIL'S CASH INVESTMENTS**

Item 19.7 is classified CONFIDENTIAL under the provisions of Section10A(2)(c) of the local government act 1993, as it deals with information that would, if disclosed, confer a commercial advantage on a person with whom the council is conducting (or proposes to conduct) business, and Council considers that discussion of the matter in an open meeting would be, on balance, contrary to the public interest.

Moved: \_\_\_\_\_ Seconded: \_\_\_\_\_

**20 RESUMPTION OF OPEN COUNCIL**

**21 CLOSURE**

**DATE OF NEXT MEETING: TUESDAY 25 AUGUST 2020**